

# CITY OF RIVERTON PARKING GUIDELINES

## OFF-STREET PARKING

### **1.0 - Purpose.**

The purpose of this section is to promote safe access, enhanced vehicle parking and maneuvering, more efficient use of land, enhanced urban form, accommodation of alternative modes of transportation, and better pedestrian movement. The provisions of this section are intended to:

1. Prevent and alleviate the congestion of public streets;
2. Encourage the incorporation of alternative modes of transportation by emphasizing pedestrian circulation, and establishing recommendations for bicycle parking;
3. Minimize the detrimental effects of vehicular use areas on adjacent properties;
4. Promote the health, safety and public welfare by establishing maximum requirements for off-street parking and loading areas, and;
5. Limit parking to encourage more compact, walkable developments.

### **2.0 - Applicability and location.**

#### *A. Scope of guidelines.*

1. Off-street parking space shall be provided for all new buildings and structures, for changes in use which increase the capacity, available gross floor area, intensity of use, or other unit of measurement specified herein, and for additions to existing structures.
2. The term "addition" as used in this section shall include any alteration intended to enlarge or increase capacity by adding or creating dwelling units, guestrooms, floor area or seats.
3. Any building completed, and in use at the time of passage of the ordinance from which this article is derived, shall not be required to furnish any additional parking spaces that may be required by this chapter. When the intensity of use of a structure is in any way increased, all restrictions of this article shall pertain.
4. Accessory off-street parking and loading facilities in existence on the effective date of this chapter, and located on the same lot as the building or use served, shall not hereafter be reduced below the requirements of this section for a similar new building or use. If such existing facilities are already below the required amount, they shall not be hereafter further reduced.

#### *B. Location of parking.*

##### *1. Generally.*

- a. Parking spaces required for dwellings shall be located on the same parcel, or site, as the main dwelling, and located either in a garage or on a paved driveway or parking area.
- b. New parking spaces created in residential districts after the effective date of these regulations shall not occupy any part of any required front yard except for the driveway. Parking on a nonpaved surface in the front yard shall be prohibited.
- c. Parking in Commercial and Mixed-Use Districts shall be located to the side or rear of the primary structure. One double-loaded row of surface parking may be allowed between the front of a commercial structure and the front setback provided 50 percent additional landscaping is provided pursuant to this chapter.
- d. Parking spaces shall be reserved for the sole use of the occupants and employees of the building and their visitors. However, churches, theaters, stadiums, auditoriums and other

similar places of assembly may make arrangements for joint use of parking space with other uses herein specified.

- C. *Off-site parking.* All off-street parking areas for any use shall be provided on the same lot as the use it serves. However, parking may be allowed on another lot where there are practical difficulties in the location of the parking area or if public safety or public convenience, or both, are better served by a remote location. Off-site parking shall be approved by the community development director, and shall comply with the following standards:
1. *Same ownership.* The parking area is located on land under the same ownership as the use it serves, or a recorded easement in perpetuity that has been established for the use of an off-site location for parking and filed with city clerk and county recorder.
  2. *Distance between off-site parking area and the proposed use.*
    - a. Off-site parking for multiple-family dwellings shall not be located more than 200 feet from any normally used entrance of the principal use served.
    - b. Off-site parking for nonresidential or mixed-uses shall not be located more than 300 feet from any normally used entrance of the principal use served.
    - c. The above distances shall be measured along the shortest legal, practical walking route. This route may include crossing a right-of-way provided it uses a legal crosswalk.
  3. *No undue hazard.* The off-site parking area shall be convenient to use without causing unreasonable:
    - a. Hazard to pedestrians,
    - b. Hazard to vehicular traffic,
    - c. Traffic congestion,
    - d. Interference with commercial activity or convenient access to other parking areas in the vicinity,
    - e. Detriment to the appropriate use of business lands in the vicinity, or
    - f. Detriment to any abutting residential neighborhood.

### **3.0 - Off-street parking standards.**

- A. *Computation of required off-street parking spaces.*
1. *Fractions.* When measurements of the number of required parking spaces result in fractions, the space standard shall be rounded upward to the next highest whole number.
  2. *Different use areas.* Except as provided for in this section, parking shall be calculated separately for each different use area in a building or on a site, including all accessory uses.
  3. *Combinations of uses.* If the community development director determines that a proposed use represents a combination of uses listed in Table 15.36.030-1, Off-Street Parking Standards, the parking space standards shall be those that would apply if the two (or more) uses were developed separately, unless the community development director determines that a lower standard would be adequate because of differences in peak operating hours pursuant to this chapter.
  4. *On-street parking.* Except as permitted as part of an approved Alternative Parking Plan, on-street parking on streets, shall not be used to satisfy the off-street parking standards of this article.
  5. *Parking based on seating.* When the standards use seating as a unit of measurement, all calculations shall be based on the occupant load of the areas used for seating.

6. *Parking based on floor area.* Except as provided for in this section, when the standards use amount of square footage in buildings as a unit of measurement, all calculations shall be based on gross floor area minus ten percent except as may hereinafter be modified.
  7. *Parking based on occupants.* Except as provided for in this section, when the standards use the number of occupants as a unit of measurement, all calculations shall be based on the maximum fire-rated capacity.
  8. *Fleet parking.* For the purpose of calculating parking requirements, fleet vehicle parking spaces shall not count against either the minimum or maximum requirements.
  9. *Handicap parking.* One twelve (12) foot by nineteen (19) foot parking space shall be reserved and so designated for the handicapped. One handicap space shall be provided for the first fifty (50) parking spaces and each additional fifty (50) parking spaces thereafter.
- B. *Determination by community development director.*
1. Parking standards for uses not specifically listed in Table 15.36.030-1, Off-Street Parking Standards, shall be determined by the community development director based on the standards for the closest comparable use, or by reference to standard parking resources published by the National Parking Association, the American Planning Association or similar organization. Final construction permitting will be contingent upon approval of the community development director.
  2. The community development director may alternately require the submittal of a parking demand study that justifies estimates of parking demand based on the recommendations of the Institute of Transportation Engineers (ITE), and includes relevant data collected from uses or combinations of uses that are the same or comparable to the proposed use in terms of density, scale, bulk, area, type of activity and location.
- C. *Off-street parking spaces required.* Off-street parking spaces shall be provided in accordance with Table 3.0-1, Maximum Off-Street Parking Standards. Where this table does not specify a parking requirement, the Community Development Department will base determination of parking requirements on similar use as to that proposed.

**Table 3.0-1: Maximum Off-Street Parking Standards**  
 Dwelling Unit (du)      Gross Floor Area (GFA)

Use	Parking Requirements
<b>Residential</b>	
<b>Household Living</b>	
<b>Dwelling</b>	
Single-Family, Detached	2 per du (min.)
Single-Family, Attached (3 or more	2 per du (min.)
Single-Family, Duplex	2 per du (min.)
Single-Family, Patio Home	2 per du (min.)
Multi-Family	1 per studio; 1.5 per 1 br; 2.0 per 2 br; 2.5 per 3 br; 2.5 + 0.5 for each br more than 3; plus 1 per 5 units (min.)
Home-Based Occupation	As required for dwelling type
Live/Work	1 per du plus parking for work use (min.)

Loft	1 per du (min.)
Manufactured Home	2 per du (min.)
Modular Housing	2 per du (min.)
Mobile Home Community	Mobile Home Zone Requirements
<b>Group Living</b>	
Congregate Living Facility/Senior	1 per 2 beds plus 1 per 100 sf of assembly area
Group Home	1 per 4 beds plus 1 per 100 sf of assembly area
Nursing, Convalescent, and Rest Home	1 per 2 beds plus 1 per 100 sf of assembly area
Shelter Care Facility	1 per 4 beds
<b>Civic and Institutional</b>	
<b>Community Services</b>	
Civic, Social, and Fraternal	1 per 300 sf GFA
Community Center	1 per 200 sf GFA
Cultural Institutions, Public	1 per 250 sf GFA
Government Offices and Facilities	1 per 300 sf GFA of space used by the public + 1 per 600 sf GFA
Religious Assembly	1 space per 75 sf GFA in the main assembly space
<b>Day Care</b>	
Family Day Care Home	family daycare home
Group Day Care Home	group daycare home & child center
Child Care Center	group daycare home & child center
<b>Educational Facilities</b>	
School, Primary or Secondary, Public	Elementary Schools: 2 per classroom; Middle Schools: 2 per
School, Vocational-Technical and	1 per 300 sf of enclosed floor space
University or College	1 per 300 sf of enclosed floor space
<b>Health Care Facilities</b>	
Alcohol and Drug Abuse Treatment	1 per 1,000 sf GFA
Hospital	1 per 2 beds based on maximum capacity, plus 1 per 350 sf of
Medical and Dental Offices and Clinics	1 per 200 sf GFA
Urgent Care Facility	1 per 200 sf GFA treatment plus 1 per 300 sf GFA remaining
<b>Parks and Open Space</b>	

Athletic Area	20 spaces per field minimum
Recreation Areas and Facilities	20 spaces per field minimum
<b>Transportation</b>	
Airport and Passenger Terminal	1 per 400 sf GFA passenger terminal area
Bus Garaging and Equipment	1 per 2,000 sf GFA for indoor and outdoor storage areas plus 1
Bus Terminal	1 per 200 sf GFA
<b>Commercial</b>	
<b>Animal Sales and Service</b>	
Pet Grooming	1 per 300 sf GFA
Pet Shop	1 per 200 sf GFA
Kennels	1 per 500 sf GFA
Shelter	1 per 500 sf GFA
Stables (commercial)	1 per 5 stalls
<b>Veterinary Services</b>	
Indoor Only	1 per 250 sf GFA
With Outdoor Facilities	Indoor plus 1 per 1,000 sf outdoor GFA
<b>Office</b>	
Business and Professional Office	1 per 300 sf GFA
<b>Recreation and Entertainment, Outdoor</b>	
Golf Course	4 per hole plus 2.5 per 1,000 sf GFA in clubhouse
Recreation and Entertainment,	30 per acre of site
<b>Recreation and Entertainment, Indoor</b>	
Amusement Parlor/Arcade	1 per 150 sf GFA
Recreation and Entertainment, Indoor	1 per 200 sf GFA
Theater	1 per 3 seats
<b>Commercial Services</b>	
Commercial Services, General	1 per 200 sf GFA
<b>Eating and Drinking</b>	
Brewery, Distillery or Winery	1 per 100 sf of floor area accessible to customers
Microbrewery	1 per 200 sf of floor including outside dining/drinking areas

Restaurant	1 per 60 sf of floor including outside dining/drinking areas
With Drive-Thru	Restaurant plus stacking spaces
<b>Financial Services</b>	
All Financial Services	1 per 300 sf of floor area plus stacking as necessary
Off-Premises Contacts (OPC)	1 per 250 sf GFA
<b>Retail Sales and Personal Services</b>	
<b>Retail</b>	
General Retail, No Drive-Thru, up to 100,000—399,999 sf GFA	1 per 250 sf GFA 1 per 300 sf GFA
With Drive-Thru	Stacking requirements
Home Supply and Lumberyard	1 per 250 sf GFA
Plant Nursery and Greenhouse	1 per 200 sf GFA
<b>Personal Services</b>	
Personal services	1 per 200 sf GFA
Dry Cleaner/Laundromat/Laundry	1 per 250 sf GFA plus stacking as necessary
<b>Lodging Establishment</b>	
Bed and Breakfast Inn	1 per guest room in addition to those required for principal
Campground and Vehicle Park	RV: 1 per RV parking space plus 1 per each 10 RV parking spaces
Hotel and Motel	1 per guest room up to 100 units, then 0.75 per unit over 100;
<b>Vehicles and Equipment</b>	
Car Wash	Stacking requirements
<b>Vehicles</b>	
Rental Services	1.5 per 1,000 sf GFA
Repair Services	1.5 per 1,000 sf GFA
Sales	1.5 per 1,000 sf GFA plus 0.7 per 1,000 sf open air display up to
Gasoline Service Stations	1 per 200 sf GFA, minimum 5 spaces including spaces located in
Accessory Car Wash	Stacking requirements
Truck Stop	1 per 200 sf of building area (excluding car wash area) and
<b>Industrial</b>	
<b>Industrial Service and Manufacturing Assembly</b>	

All uses	1 per 200 sf GFA office space + 0.5 per 1000 sf of manufacturing
<b>Energy Production and Natural Resource Extraction</b>	
All uses	0.8 per number of employees on peak shift
<b>Wholesale, Storage, and Distribution</b>	
All uses	1 per 200 sf GFA office space + 0.5 per 1000 sf of manufacturing
<b>Waste and Salvage</b>	
Recycling Collection Facility	0.8 per number of employees on peak shift

D. *Off-street parking standards for selected service and industrial uses.* Uses that reference this subsection in Table 3.0-1, Maximum Off-Street Parking Standards, shall provide no more than the number of spaces identified in Table 3.0-2, Maximum Off-Street Parking Standards for Selected Service and Industrial Uses:

**Table 3.0-2: Maximum Off-Street Parking Standards for Selected Service and Industrial Uses**

Use or Activity		Required Number of Spaces
Office or administrative area		1.0 space per 300 square feet
Indoor sales area		1.0 space per 200 square feet
Indoor storage, warehousing, assembly, vehicular service, or manufacturing area:	1—3,000 square feet of floor area	1.0 space per 250 square feet
	3,001—5,000 square feet of floor area	1.0 space per 500 square feet
	5,001—10,000 square feet of floor area	1.0 space per 750 square feet
	10,001 or more square feet of floor area	1.0 space per 1,250 square feet
Outdoor sales, display, or storage area (3,000 square feet or less)		1.0 space per 750 square feet
Outdoor sales, display, or storage area (more than 3,000 square feet)		1.0 space per 1,000 square feet
NOTE: The total number of required spaces is cumulative based on the variety of different functions present in a single use.		

- E. *Uses with variable parking demand.* Uses that reference this subsection in Table 3.0-1, Maximum Off-Street Parking Standards, have widely varying parking demand characteristics making it difficult to establish a single off-street parking standard. Upon receiving a development application for a use subject to this subsection, the community development director shall apply the off-street parking standard specified for the listed use that is deemed most similar to the proposed use, or establish an off-street parking standard on the basis of a parking study prepared by the applicant. Such a study shall include estimates of parking demand based on recommendations of the Institute of Transportation Engineers (ITE), or other acceptable estimates as approved by the community development director, and should include other reliable data collected from uses or combinations of uses that are the same as, or comparable with, the proposed use. Comparability will be determined by density, scale, bulk, area, type of activity and location. The study shall document the source of data used to develop the recommendations.
- F. *Maximum parking spaces allowed.*
1. *Applicability.* Off-street vehicle parking spaces shall not be provided in an amount that is more than the amount specified in Table 3.0-1 unless the maximum parking requirement is waived or mitigated as provided in this section.
  2. *Maximum parking calculation exceptions.* For the purpose of calculating parking requirements, the following types of parking spaces shall not count against the maximum parking requirement:
    - a. Accessible parking;
    - b. Vanpool and carpool parking;
    - c. Spaces with electrical vehicle charging stations, up to a maximum of two per 50 parking spaces;
    - d. On-street parking adjacent to the lot or lots on which the parking located; and
    - e. Structured parking, underground parking, and parking within, above or beneath the building(s) it serves.
- G. *Maximum parking waiver.*
1. *Parking demand study.* Requests to exceed the maximum parking requirement shall be accompanied by a parking demand study demonstrating how the maximum number of parking spaces specified in Table 15.36.030-1 is insufficient for the proposed development.
  2. *Review criteria.* A waiver to the maximum parking requirement may be allowed by the community development director in situations that meet the following criteria:
    - a. The proposed development has unique or unusual characteristics such as high sales volume per floor area or low turnover, that create a parking demand that exceeds the maximum ratio and that typically does not apply to comparable uses;
    - b. The parking demand cannot be accommodated by on-street parking, shared parking with nearby uses, or by increasing the supply of spaces that are exempt from the maximum ratio;
    - c. The request is the minimum necessary variation from the standards; or
    - d. If application of the maximum parking standard would result in fewer than six parking spaces, the development shall be allowed six parking spaces.
- H. *Design requirements for excess parking.* Parking that is provided in excess of the maximum parking requirement shall be required to include increased internal landscaping and incorporate pervious pavement as described below.
1. *Pervious surfaces.* Where parking spaces in excess of the maximum specified in Table 3.0-1 are constructed, an area equal to the total area required for the number of spaces that exceed the

maximum parking requirement shall be constructed of pervious surfaces as approved by the community development director.

2. *Additional trees and landscaping.*

- a. Applicants that request parking that exceeds the number of spaces required by Table 3.0-1 shall provide additional parking lot trees and landscaping as follows:

**Table 3.0-3: Additional Landscaping Requirement for Overparking**

<b>Amount of Excess Parking Requested Over Maximum (percent)</b>	<b>Additional Landscaping (percent)</b>
5	5
6—10	10
11—15	15
16—20	20

- b. Any required additional landscaping shall be distributed throughout the site as determined by the community development director, and in accordance with Table 3.0-3. The additional landscaping shall be integrated with the parking lot and site design.
- c. Where the provision of additional landscaping is restricted for infill and redevelopment projects due to site constraints, the applicant may provide sidewalk amenities or streetscape features as determined by the community development director. Acceptable amenities shall have a value equal to or greater than the price of the additional landscaping features required in Table 3.0-3, and may include but are not limited to:
- i. Sidewalk planters between the parking area and building, or the parking area and the street;
  - ii. Public art including, but not limited to, sculptures, fountains, clocks or murals; or
  - iii. Decorative fencing (such as wrought iron) around the perimeter of the parking area provided with seasonal plantings.

i. *Exceptions and adjustments.*

1. *Change in permitted uses.* A permitted use that does not meet the parking requirements of this section may be converted to another permitted use without full compliance with the required number of parking spaces provided:
  - a. The applicant provides the maximum amount of parking spaces possible without being required to remove, or partially remove a structure.
  - b. If a structure or a portion of a structure is voluntarily removed, the resulting area shall be used to provide the additional parking spaces necessary towards fulfilling the requirements of Table 3.0-1.
2. *Small use exception.* Any individual use in a space that is 2,000 square feet or smaller shall be exempt from the parking requirement of Table 3.0-1 except for uses in the eating and drinking use category.

3. *Shared parking.*

- a. Shared parking spaces shall be located within 500 linear feet of the primary entrance of all uses served as measured along the shortest legal, practical walking route. This route may include crossing a right-of-way provided it uses a legal crosswalk. Such distance shall not apply if a remote parking shuttle bus service is provided. Shared parking spaces shall not be separated from the use they serve by an arterial or collector street. In addition, adequate and safe pedestrian access must be provided from, and to the shared parking areas.

J. *Bicycle parking recommendations.*

- 1. *Number of spaces.* Bicycle parking may be provided as follows unless otherwise approved by the community development director due to safe accessibility:

**Table 3.0-4: Recommended Bicycle Parking**

Use	Bicycle Parking Spaces [1]
Multiple-family	2 or 1 per 20 units
Group home	1 per 4 bedrooms
Office	2 or 1 per 40,000 sq. ft. (net area)
Commercial sales and service	2 or 1 per 5,000 sq. ft. (net area)
Community use (non-utility)	2 or 1 per 10,000 sq. ft. (net area)
Schools	2 per classroom
Notes: [1] Whichever measurement results in the higher number of spaces.	

- 2. *Design and location.* Bicycle parking facilities should include a rack or storage facility (e.g., locker) that enables bicycles to be secured. Where racks are used, they shall meet the following standards:

- a. The bicycle frame and one wheel can be locked to the rack with a high-security, U-shaped shackle lock if both wheels are left on the bicycle;
- b. A bicycle six feet long can be securely held with its frame supported so that the bicycle cannot be pushed or fall in a manner that will damage the wheels or components;
- c. The rack must be securely anchored;
- d. Bicycle racks and storage facilities shall be accessible without moving another bicycle;
- e. Bicycle racks and storage facilities shall be located in convenient, visible, well-lit areas with easy access and near main entrances of all commercial, residential and institutional buildings. Such locations shall be clearly noted with signage;
- f. The racks and storage facilities shall be located so they do not interfere with pedestrian traffic, and shall be protected from potential damage by motor vehicles; and
- g. Bicycle parking shall not be within any required landscape area, nor interfere with any pedestrian pathway.

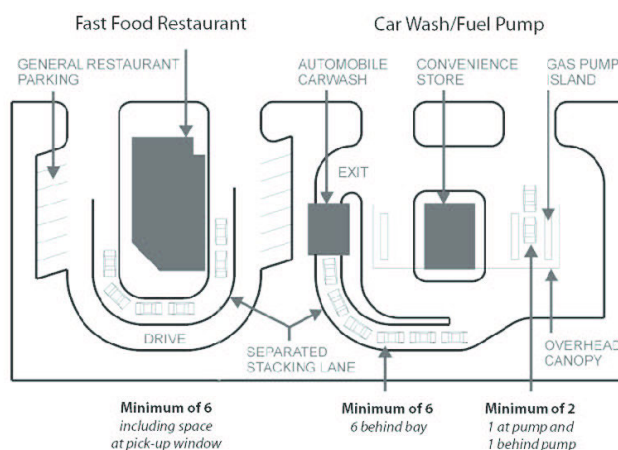
- K. *Loading areas.* A permanently maintained usable off-street loading space shall be provided on the same parcel when structures for the following uses are erected, established or altered:

1. Hospitals or similar institutions with a gross floor area (GFA) of 10,000 square feet or more shall provide one space, exclusive of ambulance space.
  2. Hotels and office buildings with a GFA of 10,000 square feet or more shall provide one space.
  3. Undertakers and funeral parlors shall provide one space for each 5,000 square feet of GFA.
  4. All commercial and industrial uses with a GFA of 5,000 square feet or more shall provide one space for the first 25,000 square feet, and one additional space for each 50,000 square feet GFA thereafter, or any portion thereof.
- L. *Vehicle stacking areas.* The following standards shall apply to businesses that contain a drive-through establishment (e.g., restaurant or financial institution), and are a stand-alone use (e.g., automatic teller machine):
1. *General standards.*
    - a. Audible electronic devices such as loudspeakers, automobile service order devices and similar instruments shall not be located within 250 feet of any residential dwelling unit.
    - b. No service shall be rendered, deliveries made or sales conducted within the required front yard or abutting street side yard; customers served in vehicles shall be parked to the sides or rear of the principal building.
    - c. All drive-through areas, including but not limited to, menu boards, stacking lanes, trash receptacles, loudspeakers, drive-up windows and other objects associated with the drive-through area shall be located in the side or rear yard of a property to the maximum extent feasible, and shall not cross, interfere with or impede any public right-of-way.
  2. *Stacking space and lane requirements.* Stacking lanes shall comply with the following design standards:
    - a. Drive-through stacking lanes shall have a minimum width of eight feet, and length of 20 feet.
    - b. The first position in a drive-through station, located at the window, is counted as a stacking space. No stacking spaces may be counted as parking spaces unless specified in Table 15.36.030-1.
    - c. When stacking lanes are separated from other stacking lanes, bypass lanes or from other site areas, the separation shall be by means of a raised concrete median, concrete curb, or landscaping.
    - d. Stacking lanes shall be set back 25 feet from rights-of-way.
    - e. All stacking lanes must be clearly identified, through the use of means such as striping, landscaping and signs.
    - f. The number of required stacking spaces shall be as provided for in Table 3.0-5, Stacking Space Requirements. See Figure 3.0.A for an illustration of stacking lanes:

**Table 3.0-5: Stacking Space Requirements**

<i>Activity</i>	<i>Minimum Stacking Spaces (per lane)</i>	<i>Measured From:</i>
Retail Sales, Bank, Financial Institution, or Automated Teller Machine (ATM)	3	Teller or Window
Restaurant	6	Pick-Up Window

Full-Service Vehicle Washing Establishment	6	Outside of Washing Bay
Self-Service or Automated Vehicle Washing Establishment	2	Outside of Washing Bay
Fuel or Gasoline Pump Island	2	Pump Island
Other	As determined by the community development director	



**Fig. 3.0-A: Stacking Lanes**

**M. Access.**

1. All driveway entrances, including stacking lane entrances, must be at least 50 feet from an intersection. The distance is measured along the property line from the junction of the two street lot lines to the nearest edge of the entrance. The community development director may determine that a right in, right out entrance is permitted closer than 50 feet from an intersection.
2. The entrance to a stacking lane shall be located so as to avoid conflicting with the street access.
3. Access driveways on a corner site shall be located as far as possible from the abutting street intersection.
4. Pedestrian/barrier-free access to building entrances shall provide direct pedestrian links to main parking areas and public sidewalks that do not pass through a stacking lane.

**4.0 - Off-street parking design standards.**

- A. *Stall size and driveways.* The required minimum area for off-street parking spaces and the required minimum area for driveways or other access serving off-street parking spaces shall be as follows:
1. The first ten spaces and 80 percent of the spaces over and above the first ten must be nine-foot by 19-foot "full-size" spaces.
  2. Twenty percent of the required off-street parking spaces over and above the first ten required spaces may be provided in eight-foot by 16-foot "compact" spaces.
  3. All compact spaces must be permanently marked for "compacts only."

4. The required minimum area for compact and full-size off-street parking spaces, and the required minimum area for driveways and other access serving off-street parking spaces are shown in the charts which are included in this chapter.

B. *Design and materials.*

1. *Mixed-use and nonresidential development.*

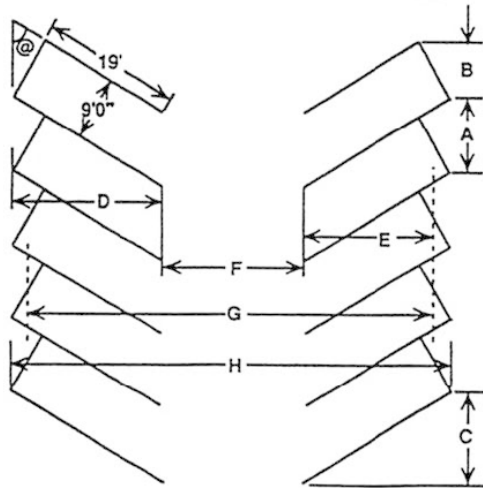
- a. Except as provided in this chapter, off-street parking spaces shall be surfaced with either:
  - i. A minimum of two inches of asphalt over a four-inch gravel base;
  - ii. Five inches of concrete; or
  - iii. Interlocking concrete paver stones of strength equal to five inches of concrete.
- b. All parking areas shall provide concrete curb and gutter that meets the community development directoring specifications.
- c. All surfacing shall be over an approved compacted subgrade and shall be graded so as to eliminate drainage problems.
- d. Parking or storage areas used exclusively for heavy construction equipment, such as track-loaders, cranes, bulldozers, road graders, track-hoes and other similar large heavy construction machinery or machinery with steel-track propulsion, may be surfaced with a compacted granular material or gravel of sufficient depth to prevent mud or silt from leaving the site. These parking areas shall be enclosed with a solid fence, no less than six feet in height.
- e. Each space shall be equipped with wheel guards when necessary to prevent vehicles from extending beyond the boundary of the space, and from coming in contact with other vehicles, walls, fences or plantings. In addition to wheel guards, any abrupt change in elevation, at a height of 18 inches or greater, shall require the provision and installation of guard rails in compliance with this chapter.

2. *Residential development.* Parking areas as required for dwellings shall be surfaced with either:

- a. A minimum of two inches of asphalt;
- b. Four inches of concrete; or
- c. All surfacing shall be over an approved compacted subgrade and shall be graded so as to eliminate drainage problems.

C. *Off-street parking design and dimensional tables.* Internal stall and driveway layout shall conform to the following standards:

1. *Full-sized cars.*



**Fig. 4.0-A: Layout and Dimensions for Full-Size Car**

**Table 4.0-1: Full-Size Car, 9 feet by 19 feet**

	A	B	C	D	E	F	G	H
At Parking Angle (degrees)	Curb Length per Car (feet)	Curb Length Short Dimension (feet)	Curb Length Long Dimension (feet)	Stall to Back Rows (feet)	Stall to Curb (feet)	Aisle Width (feet)	Back to Back Rows (feet)	Total Width (feet)
0	23.0'	0.0'	23.0'	9.0'	9.0'	12.0'	30.0'	30.0'
20 [1]	26.3'	3.1'	17.9'	15.0'	10.8'	11.0'	32.5'	41.0'
30 [1]	13.0'	4.5'	16.5'	17.3'	13.4'	11.0'	37.8'	45.6'
40 [1]	14.0'	5.8'	14.6'	19.1'	15.7'	12.0'	43.3'	50.2'
45	12.7'	6.4'	13.4'	19.8'	16.6'	13.0'	46.2'	52.6'
50	11.7'	6.9'	12.2'	20.4'	17.5'	12.0'	47.0'	52.8'
60	10.4'	7.8'	9.5'	21.0'	18.8'	18.0'	55.5'	60.0'
70	9.6'	8.5'	6.5'	21.0'	19.5'	19.0'	57.9'	61.0'
	9.0'	9.0'	0.0'	19.0'	19.0'	23.0'	61.0'	61.0'

Notes: [1] Must be approved by community development director

(2) Compact cars.

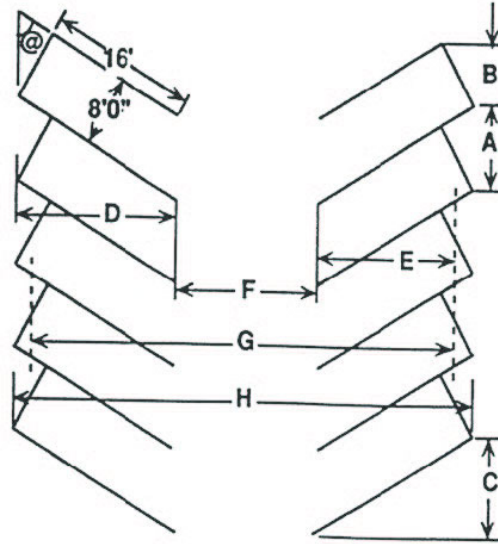


Fig. 4.0-B: Layout and Dimensions for Compact Car

Table 4.0-2: Compact Car, (8 feet by 16 feet)

	A	B	C	D	E	F	G	H
At Parking Angle (degrees)	Curb Length per Car (feet)	Curb Length Short Dimension (feet)	Curb Length Long Dimension (feet)	Stall to Back Rows (feet)	Stall to Curb (feet)	Aisle Width (feet)	Back to Back Rows (feet)	Total Width (feet)
	21.0'	21.0'	21.0'	8.0'	8.0'	11.0'	27.0'	27.0'
20 [1]	24.0'	3.0'	37.0'	13.0'	9.4'	10.0'	28.8'	36.0'
30 [1]	16.0'	4.8'	26.0'	15.0'	11.5'	10.0'	33.0'	40.0'
40 [1]	12.5'	5.1'	18.8'	16.4'	12.2'	11.0'	37.6'	43.8'
45	11.5'	5.8'	11.5'	17.0'	14.0'	12.0'	40.0'	46.0'
50	10.3'	6.1'	14.8'	17.6'	14.8'	13.0'	40.6'	46.2'
60	9.5'	7.0'	10.0'	18.0'	16.0'	17.0'	49.0'	52.0'
70	9.0'	8.0'	6.6'	18.0'	16.5'	18.0'	51.0'	54.0'
90	8.0'	8.0'	0.0'	16.0'	16.0'	21.0'	53.0'	53.0'

Notes:

[1] Must be approved by community development director

*D. Loading area dimensions, design, and materials.*

1. Each off-street loading space shall be at least 53 feet in length and ten feet in width, and shall be unobstructed from the surface up to a height of at least 15 feet.
2. Off-street loading spaces shall be surfaced with either:
  - a. A minimum of two inches of asphalt over a four-inch gravel base; or
  - b. Six inches of concrete.
3. All surfacing shall be over an approved compacted subgrade, and shall be graded so as to eliminate drainage problems. Each space shall be equipped with wheel guards when, in the opinion of the community development director, they are necessary to prevent vehicles from extending beyond the boundary of this space and from coming into contact with other vehicles, walls, fences or plantings. In addition to wheel guards, any abrupt change in elevation, at a height of 18 inches or greater, shall require the provision and installation of guard rails in compliance with this chapter.
4. Lighting provided for off-street loading spaces shall be so arranged so as to minimize illumination onto adjoining residential property, and so as to prevent glare directed at vehicles on streets and alleys.
5. Off-street loading spaces shall be located in such a way that no part of the vehicle will occupy an adjacent street or sidewalk, when the spaces are being used to load or unload a vehicle.

*E. Completion.*

1. All parking lot surfacing and striping shall be completed in accordance with this section prior to issuance of the certificate of occupancy. If seasonal limitations prevent completion and, if security as described in this subsection is provided, a temporary certificate of occupancy may be issued by the community development director.
2. Security in the form of cash, a performance bond, cashier's check or irrevocable letter of credit acceptable to the city equal to the cost of the parking lot surfacing and striping shall be provided by the permittee prior to issuance of the certificate of occupancy. Upon completion of the parking lot surfacing and striping, and with final approval by the community development director, the security will be returned to the permittee. In addition, the permittee shall be required to provide a construction easement, in a form acceptable to the community development director, prior to issuance of certificate of occupancy. Should the permittee fail to complete, by the date required upon the temporary certificate of occupancy, the parking lot surfacing and striping as required by the plan submitted and approved, the city may use said security to complete the parking lot surfacing and striping as required by the plan or revoke the temporary certificate of occupancy, or both. Any excess funds from the security not used to complete the parking lot surfacing and striping shall be returned to the permittee.

*F. Failure to comply.*

1. It shall be unlawful for any person, firm or corporation to erect, construct, alter, extend, repair, remove, demolish, resurface or restripe any off-street parking lot or facility regulated by this chapter, or cause same to be done in a manner that is in conflict with, or in violation of any of the provisions of this chapter, or without first obtaining a permit from the planning and development department.

**5.0 - Parking lot landscaping and lighting.**

- A. *Lighting*. Lighting provided for off-street parking spaces shall be so arranged so as to minimize illumination onto adjoining residential property, and so as to prevent glare directed at vehicles on streets and alleys.
- B. *Parking lot landscaping*. The following landscaping requirements shall be met for all off-street surface parking lots.
1. *Perimeter landscaping*.
    - a. Perimeter parking lot landscaping a minimum width of ten feet shall be required for all parking lots having more than three spaces where the parking lot is adjacent to a public street, or a nonretail or industrial use such as a residential area, institutional use (e.g., hospital) or office.
    - b. The parking lot perimeter landscaping shall include one canopy tree per 50 lineal feet of landscape area. Where the calculation of the number of trees results in a fraction, the number shall be rounded up to the nearest whole number. The remainder of the landscape area shall be grass, ground cover, or shrubs. No more than 20 percent of the landscape area shall be mulch, decorative rocks, or other nonliving materials. The use of concrete is not permitted in the landscape area.
    - c. Where lots are being developed in a Mixed-Use District, the parking lot perimeter landscaping requirement may be reduced along an interior lot line, at the discretion of the community development director, provided that interior parking lot landscaping applies to both parking lots.
  2. *Curbs*. Landscaped areas within parking lots, or along the perimeter of the property, must be protected from vehicular traffic through the use of permanent barriers. Barriers can be made of concrete curbs, large stones, bollards, or other rigid, permanent systems, not including asphalt curbs. If the barriers are continuous, adequate breaks, as determined by the community development director, shall be designed to allow runoff inflows to enter the landscape area while still providing adequate protection from traffic.
  3. *Internal landscaping design*. Landscape islands shall be installed for every row exceeding 15 parking spaces, either within the parking row or at the end of the parking row, and shall be designed as follows:
    - a. Be a minimum of eight feet in width.
    - b. Be at least 200 square feet.
    - c. Allow for runoff capture using curb cuts and approved bioswale designs.
    - d. Have a minimum of four, five-gallon deciduous shrubs and one canopy tree that is a minimum of 2½-inch caliper per 200 square feet.
    - e. Incorporate perennials and grasses for seasonal color.
    - f. Contain a minimum of 50 percent living landscaping material, with a maximum of 50 percent nonliving landscaping material. Approved sidewalks are not counted toward the nonliving landscape material percentage.
    - g. Internal landscape strips shall be installed between the parking rows of every other double row of parking when parking rows exceed 50 parking spaces, and shall meet the following standards:
      - i. Be a minimum of ten feet in width, and shall extend the length of the parking row.
      - ii. Include a four-foot wide sidewalk, and a six-foot wide planting strip.
      - iii. Meet planting requirements for interior landscape islands, as outlined above.  
Allow for runoff capture using curb cuts and approved bioswale designs.

*Sight distance triangle.* A site distance triangle within the off-street parking area must be established at street intersections by maintaining a maximum height for shrubs and ground cover of 48 inches. No obstructions are permitted in the site distance triangle including tree branches that must be trimmed within eight feet of the ground. All applicable sight distance requirements must be met for parking lot internal circulation and access points to the public right-of-way.