



**MONTROSE
PLANNING AND ZONING COMMISSION
AGENDA**

Wednesday, January 30, 2019 – 7:00 PM

**Montrose Community Center
200 Center Avenue South
Montrose, Minnesota 55363**

1. Call to Order
2. Oath of Office
 - A. Oath of Office
 - Mr. Justin Emery
3. Roll Call
4. Pledge of Allegiance
5. Approval of Agenda
6. Approval of Minutes
 - A. December 10, 2018 Planning and Zoning Commission Meeting Minutes
7. Appoint Officers
 - A. Chair
 - B. Vice-Chair
8. Public Hearing
 - A. The Preserve Housing Development
 - Planned Unit Development (P.U.D.) Rezoning
 - B. The Preserve Housing Development
 - Preliminary and Final Plat Presentation

9. Old Business

- A. The Preserve Housing Development
- Planned Unit Development (P.U.D.) Rezoning
 - Preliminary and Final Plat Presentation
 - Sale of City-Owned Property

10. New Business**11. Next Meeting**

- A. Wednesday, February 20, 2019 to be held at the Montrose Community Center - 7:00 p.m.

12. Adjournment

City of Montrose
 Planning and Zoning Commission Meeting
 Montrose Community Center
 200 Center Avenue South
 Wednesday, December 19, 2018
 7:00 P.M.

1. CALL TO ORDER

Pursuant to call and notice the Montrose Planning and Zoning Commission met in Regular Session on Wednesday, December 19, 2018 at 7:00 p.m.

Planning and Zoning Commission Chair, Ms. Sylvia Henry, called the meeting to order at 7:00 p.m.

2. ROLL CALL

Present: Commissioner Sylvia Henry
 Commissioner Tracy Gurneau
 Commissioner Barry Rhineberger
 Commissioner Mike Scanlon
 City Council Liaison Lloyd Johnson

Staff Present: Ms. Deborah Boelter, City Clerk-Treasurer
 Mr. Myles Campbell, City Planner

3. PLEDGE OF ALLEGIENCE

The Pledge of Allegiance was taken.

4. APPROVAL OF THE AGENDA

Commissioner Gurneau motioned to approve the Agenda. Commissioner Rhineberger seconded the motion. Motion carried 4-0.

5. APPROVAL OF THE MINUTES

A. November 14, 2018 Planning and Zoning Commission Meeting

Commissioner Rhineberger motioned to approve the Planning and Zoning Commission Meeting minutes of September 19, 2018. Commissioner Gurneau seconded the motion. Motion carried 4-0.

6. NEW BUSINESS

A. Temporary Semi-Trailer Truck Parking Ordinance

The City Planner, Mr. Myles Campbell, presented his *Memo* on semi-trailer truck parking in residential districts within the City of Montrose and alternatives for this type of parking within the City limits for the Planning and Zoning Commission Members to consider. The City's Zoning Ordinance does not allow for this type of parking in residential districts.

Mr. Campbell stated that Emery's Truck and Trailer Repair, 601 Nelson Boulevard, has availability for semi-trailer truck parking and they would be interested in providing this type of parking service. He continued by stating that City staff has explored the possibility of amending the City's Zoning Ordinance for the B-2, Highway Commercial; I-1, Light Industrial; and I-2, General Industrial Districts to allow for temporary semi-trailer truck parking as an allowed land use.

Mr. Campbell stated that automobile repair is currently permitted in both Industrial Districts; but, requires a Conditional Use Permit (C.U.P.) in the Highway Business District. The C.U.P. allows the City better control over potential business types and limit any detrimental impact on other commercial or residential properties surrounding them. In all of these Districts, truck parking would be held to screening standards as laid out in Section 1020-5 of the City's Zoning Ordinance.

Mr. Campbell stated that the drawback to requiring a C.U.P. would be that some existing businesses may already have a C.U.P. for automobile repair, and that a second Permit and accompanying fee could be seen as just another cost to operating a business in the City of Montrose. Automobile repair and truck parking will have very similar conditions for permitting, and it is more than likely that if a person is abiding by their initial C.U.P., they would be following conditions of the semi-trailer truck parking Permit.

Mr. Campbell stated that making temporary semi-trailer truck parking an accessory use when the principal use of a property is automobile repair means that an existing business owner will not have to reapply for a second C.U.P., and any new repair businesses that open in the City would be allowed to operate temporary semi-trailer truck parking as long as they were in compliance with the rest of the conditions for their business. This approach would reduce some of the exhaustive steps with getting these services in place; but, could give less flexibility and control on term to the City. Mr. Campbell continued by stating that for example, a business that only provided truck parking could not exist since it would need a principal repair use to be accessory to and the City would be reliant on the conditions for automobile repair to cover any nuisances created by truck parking.

Ms. Boelter stated that City staff has brought this issue to the Planning and Zoning Commission Members on the recommendation of the Wright County Sheriff's Office (Sheriff's Office). The City and the Sheriff's Office repeatedly receives concerns from residents that have semi-trailer trucks parking in their neighborhoods and it is a violation of the City's Zoning Code. City staff met with representatives from the Sheriff's Office and they recommended that the City identify a location where semi-trailer trucks may park their vehicles overnight. She continued by stating that representatives from the Sheriff's Office provided staff with different locations throughout Wright County where cities provide this type of service to truck drivers.

Ms. Boelter also stated that the City receives telephone calls from truck drivers who are thinking about moving to the City of Montrose and they inquire about whether or not they can park their semi-trailer trucks in residential areas. When staff informs them that they cannot park in residential areas, they ask for the name and telephone number of potential businesses where they can park their trucks. It is for the two (2) aforementioned reasons that City staff is working to identify a location in the City for semi-trailer truck parking.

Ms. Boelter stated that when she, Mayor Otto, Council Member Henry and the City Planner met with the owners of Emery's Truck and Trailer Repair to evaluate that the conditions of their C.U.P. were being met, they indicated that their property is equipped with plug-ins for semi-trailer trucks and that they would be interested in providing a parking service to truck drivers.

The Planning and Zoning Commission Members discussed the Ordinance regulating semi-trailer truck parking and shared their concerns about the following:

- Having a location to park semi-trailer trucks near a residential district can cause problems due to how loud diesel-powered engines run and/or the smell of their emissions.
- Would like to see some type of screening for properties that are adjacent to residential districts.
- Would like to restrict the time and/or number of hours that the semi-trailer trucks are allowed to run their engines.
- Recommended that City staff contact some of the businesses located in the Industrial Park to determine if they have space available for semi-trailer truck parking.

Commissioner Rhineberger stated that in the Commercial Districts he would like to see the parking service as a C.U.P.; because, the City may want to have the ability to set specific conditions for the businesses that are located near residential districts. He stated that he would like to see the permitting in Industrial Districts.

Mr. Robert Chantland, 350 Buffalo Avenue South, addressed the Planning and Zoning Commission and stated that Mr. Doug Brenny, 195 Nelson Boulevard, is interested in providing semi-trailer truck parking. Mr. Chantland stated that he would provide City staff with Mr. Brenny's contact information.

Commissioner Rhineberger stated having this type of semi-trailer truck parking available would give residents another option for parking their trucks in a non-residential district.

Commissioner Rhineberger stated that he would like to see the semi-trailer truck parking services as a C.U.P. instead of an accessory use; so, the City has better control to set conditions.

Mr. Campbell asked for clarification on what the Commission Members would like to see in each District. The Commission Members discussed and agreed that they would like the B-2, Highway Commercial Districts to be a C.U.P. and permitting in the Industrial Districts.

Commissioner Gurneau recommended that City staff consider the area around the Milhausen Auto's Car Wash located at 350 Emerson Avenue South. The Commissioners discussed and agreed that it would not be a good location for semi-trailer truck parking.

Commissioner Rhineberger asked for information on what weight, and type of truck traffic, the roads in the Industrial Districts were built to handle for parking purposes. Ms. Boelter stated that the City's Ordinance does not allow for semi-trailer trucks to park on the City's streets in the Industrial Districts.

City Council Liaison Johnson asked about the Regional Park as a place for semi-trailer truck parking. The Commissioners discussed and asked City staff to investigate it as another option.

Commissioner Henry asked about the City Hall parking lot as a location for semi-trailer truck parking. Ms. Boelter stated that the City Hall parking lot is going to be used again by the Minnesota Department of Transportation (MN DOT) during construction of the Trunk Highway 25 improvement project. Ms. Boelter continued by stating that it may be a better solution to find a permanent location that can be used all year.

Mr. Campbell stated that he and Mr. Grittmann will evaluate the Planning and Zoning Commission's discussion on the semi-trailer truck parking and present some options for the Commissioners to consider and discuss at a future meeting.

7. OLD BUSINESS

A. Planning and Zoning Ordinance Amendments

1. Park Dedication Requirement

Mr. Campbell stated that the City is looking to update its existing ordinance language surrounding the dedication of lands or cash contributions by developers towards parkland and/or park amenities. Staff researched ordinances from surrounding cities and townships and worked with the Planning and Zoning Commission Members to identify priorities and desired outcomes from revising the ordinances language.

The following key points were addressed by Commissioners:

- Cash Amounts should be referred to in the City's fee schedule to avoid annual revision of the larger ordinance.
- Commissioners would like to preserve the ability to ask for cash contributions in lieu of land donations as the City currently has a large amount of empty parkland.
- Commissioners appreciated the clear payment schedule found in other cities' ordinances; versus, negotiating this on a development basis.

Mr. Campbell stated that staff took these comments and incorporated them into draft ordinance language. Mr. Campbell gave a brief overview of the draft Ordinance.

Commissioner Rhineberger asked for clarification on E. Dedication Requirements. 3. e. Thirty-Six Month Scheduled Payment Plan. Mr. Campbell gave an overview and explanation of why this paragraph is included. Commissioner Rhineberger asked that it be written clearer. He continued by stating that the Ordinance gives the City Council the ability to determine what type of payment plan they would like to implement.

Commissioner Gurneau motioned to recommend that the City Council consider approval of the amendments to the Park Dedication Ordinance as presented. Commissioner Scanlon seconded the motion. Motion carried 4-0.

8. NEXT MEETING

- A. Thursday, January 3, 2019 – Town Hall Meeting – Montrose Community Center – 6:00 p.m.
- B. Wednesday, January 16, 2019 to be held at the Montrose Community Center – 7:00 p.m.

9. ADJOURNMENT

Commissioner Gurneau motioned to adjourn the Planning and Zoning Commission Meeting at 8:00 p.m. Commissioner Rhineberger seconded the motion. Motion carried 4-0.

Sylvia Henry
Chair
City of Montrose

ATTEST:

Deborah R. Boelter, CMC
City Clerk-Treasurer
City of Montrose



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PLANNING REPORT

TO: Deb Boelter
 FROM: Myles Campbell
 DATE: January 23, 2019
 RE: The Preserve Second Addition
 FILE NO: 18.02
 PROPERTY ADDRESS: N/A
 PID: 112-048-000010 through 112-048-017080

BACKGROUND

Paxmar Land Development LLC. has submitted an application for a PUD Amendment and Final Plat review for a residential development project located off of US Highway 12. This site was previously set to be developed in 2007 before financial issues caused the land to revert back to city ownership. The Preserve Second Addition is a replat of that initial plan, which by city ordinance requires the adoption of an amended PUD zoning district, and a new set of plat documents showing all modifications. These items, as well as the purchase agreement for the land itself have been brought to the planning commission for consideration and recommendation.

The site is currently zoned R-3, Medium Density Residential, and was rezoned to this designation in conjunction with the original development plans. Under the R-3 zoning district, Townhouse and Multifamily Housing are allowed uses; however, the PUD Amendment is in place in order to provide greater design flexibility than is allowed by the base zoning district. The site is adjacent to commercial zoning to the North; low density residential zoning to the South; and township lands to the East and West.

Attached for reference:

Exhibit A:	Original Plat Document
Exhibit B:	PUD Amendment Development Plan
Exhibit C:	Final Plat
Exhibit D:	Grading Plan
Exhibit E:	Landscape Plans
Exhibit F:	Building Elevations
Exhibit G:	Engineer's Comments

ISSUES ANALYSIS

Comprehensive Plan. The Comprehensive Plan for the City of Montrose identifies this area of the city as future high-density residential land with accompanying parklands to serve the population. While the project itself has a slightly decreased total unit count from the original plan, this project does create more than 140 new units at an approximate residential density of 9.8 units per developed acre. This fits within the prescribed range of 3-12 units per acre for Medium to High Density Residential from the Comprehensive Plan's Land Use chapter. In addition, infill development is mentioned as a city goal in numerous sections of the Comprehensive plan:

Land Use and Growth Goal #1 Support the compact, efficient and orderly growth of residential, commercial and industrial land uses.

Strategies:

1. Encourage infill on existing vacant lots and redevelopment of underutilized lots.

Housing Goals and Strategies

Infill Development. Proactively plan for housing needs including infill of available residential lots. New residential development within existing neighborhoods should be designed to be compatible in use and in scale with the surrounding neighborhood. When locating duplexes or multi-family development in a single-family residential district, the buildings should be designed with an adequate setback or buffer space, be of materials compatible with the surrounding area so as to blend into the neighborhood. City of Montrose Comprehensive Plan, 2017 Chapter 9 Page 16 Multi-family housing projects should be spread throughout the City rather than concentrated in one area.

Another aspect of the project which aligns with the comprehensive plan is the dedication of a large portion of open space to the city's existing parkland. Over the two outlots being dedicated, 8 acres of upland will be available for development into public parkland, and a trail system along the eastern edge of the development. This fits with the City's goal to protect natural areas while continuing to grow and develop.

Land Use and Growth Goal #2

Protect natural resources without restricting community growth.

Strategies:

1. Protect gateways into the community with higher design standards, landscaping and architectural guidelines.

- 2. Protect natural resources such as tree lines, wetlands, creeks, etc. through appropriately located new parks, buffers and open space areas.

Overall, applicant's proposal fits with the goals of the Comprehensive Plan.

Purchase Agreement. While the commission does not need to directly vote on the purchase agreement, part of the findings for the PUD requires that it be found to be in line with the goals and policies of the city's Comprehensive Plan. For sale are all lots from PID 112-048-000010 through 112-048-017080, located in the southwestern portion of the city. These lands total 54.22 gross acres of land. The city is selling the property at a reduced amount, covering the costs the city has accrued since it took ownership of the property as well as any fees incurred through staff hours and the processing of this development application. Given the previously mentioned goals surrounding infill development and new denser housing options, as well as the stated goal to be more proactive in the approach the city takes towards new growth and long-range planning, staff finds the purchase agreement and sale of land to be consistent with the plan.

Zoning. The site is currently zoned R-3, Medium Density Residential, under which Townhouse and Multifamily Housing are allowed uses. For increased design flexibility, the site will need to be rezoned to a PUD district.

Setbacks. The following table illustrates the minimum setbacks being proposed through the PUD Amendment, as well as the setbacks for the R-3 zoning district and those from the original PUD.

	R-3	Original PUD	Proposed PUD
Front Yard	25'	25'	20'
Side Yard	10'	10'	6'
Side Yard (abutting ROW)	25'	25'	Varies, 20' minimum
Rear Yard	20'	6'	15'

The proposed setbacks for the PUD amendment fall well short of the R-3 district requirements. This is a major reason for why a PUD is necessary for the development to occur in the first place. With the current proposed setbacks, staff found that a small portion of lots did not meet the 20' ROW setback in their side yard. These lots/blocks will need to be adjusted in order to bring the site plan into compliance with the PUD amendment and the city's ordinance requirements.

Staff does have some concerns over the proposed 20' setback from right-of-way in regard to the front yard of these lots. Particularly along Willow Avenue itself, which will be a through street in the future, and along the block faces with sidewalk or trails present. A 20' setback will cause issues with cars backing out onto Willow with reduced visibility due to parked cars and the lack of pull out space available before being in the main drive lane. It also creates issues for sidewalks, both with parked cars potentially extending into the boulevard and the lack of space for snow storage during the winter. Additional space for snow storage will be

necessary to maintain clear walking paths, and staff is concerned that a 20' setback will not be able to provide this space.

Staff recommends that at least in portions of the development a front yard setback of 25' be required, in order to maintain safety along Willow Avenue in regard to vehicles backing out, and to allow for more effective snow storage and clear walking paths. Staff believes that 20' is a sufficient setback for side yards abutting right of way. While it is left up to Commissioner's discretion, requiring a set 25' front yard setback for all lots in the development would be beneficial in regard to the work of the city's building inspector. Having varying front setback requirements could create difficulties when processing the building applications throughout the development, versus having a single standard to enforce throughout the PUD district.

Staff believes a 25' setback could be achieved throughout the development by extending lots back further, with little modification necessary given the physical environment. This would result in a slight reduction to the overall dedicated parkland in some areas, however an alternative here would be to reduce rear yard setback to 10' and add this extra 5 feet to the front setback. For most of the lots that are already at the minimum 15' rear yard setback there is not a usable rear yard space in terms of patios or sheds, so the loss of the rear yard would be mostly inconsequential. Whereas to staff, an increase to 25' front yard setbacks represents a great increase to the longer-term livability and quality of the neighborhood.

Staff finds the side yard setback of 6' acceptable given the new style of townhomes being proposed and the existing conditions of the site. Side yard grading and drainage shall be reviewed and approved by the city engineer.

Lot Area, Width, and Depth. Lots within the site have varying areas, depths and widths depending on the style of housing structure. The site will contain both single family townhouses occupying a single base lot, as well as duplex style 2-family homes which are located on two lots, with a zero-lot line setback at the dividing interior wall. Listed below are the minimum widths, depths and areas found within the entire development, but there is significant variation beyond these figures.

	R-3	Single Family PUD	2-Family PUD
Lot Width	100'	Varies, 41' minimum	Varies, 66.6' minimum
Lot Depth	N/A	Varies, 88' minimum	Varies, 86' minimum
Lot Area	7,500 sq. ft. per dwelling unit	Varies, 3,608 sq. ft. minimum	Varies, 5,866 sq. ft. minimum

While the R-3 zoning district does not itself have lot dimension requirements for single family homes, the applicant is proposing lots with significantly reduced widths and overall areas than is required for lots in the R-3 zoning district. They are still viable through PUD amendment, and the lot areas themselves represent an increase over the previously

approved old plan, which had an average lot area of just over 1,000 sq. ft. per unit. The city understands that this replat represents a major decrease in the overall number of lots and density of lots to be more in line with the Comprehensive Plan, and staff endorses the proposed lot sizes for these reasons.

Easements. All lots within the subdivision have drainage and utility easements in place, 10' for the front and rear yards and 5' for the side yards, each being measured from the lot lines. In addition, all outlots to be dedicated to the city as part of the parkland dedication have full easement coverage for drainage and utility.

Maximum Building Height. The maximum building height in the R-3 district is 45 feet or 3 stories. All proposed building styles meet this requirement.

Parking. By code, Townhomes are required to provide at least 2.5 parking stall per unit. The following table shows the anticipated off-street parking availability given the proposed unit count and garages. Proposal more than meets off-street parking requirements.

	Garage/Driveway Capacity per unit	Total spaces	Total Provided	Total Required
Two Family Townhome	4 per unit (8 total)	416		
St Harrison Single Family	5	100	616	372.5
St Henry Single Family	4	52		
St Anna Single Family	4	48		

Off-street parking was previously being underprovided in the previous iteration of this project, so it is good that this has been remedied in the new iteration. However, the switch from multifamily buildings with shared parking lots to individual townhomes with attached garages has created a different parking issue.

Due to the tight side setbacks, curb cuts for driveway access leave very little room for on street parking. Through most of the development, around 30 feet or slightly less room is left in between driveways, leaving room for one car, or two small sedans uncomfortably. This may create issues in the future in regard to visitor parking. A partial measure to address this would be to realign the drives for the St Henry style lots when side by side. Due to the angled cut in of the drive, realignment would eliminate 8 feet from either side of the drives but increase the interior spaces to 46 continuous feet for parking. This would not so much impact the overall number of street parking spaces, but instead would provide more ample space for multiple cars to park in a single area and improve the overall street parking throughout the development.

Landscaping. The Applicant has provided a full landscape plan for the project. In review of the applicant's plan, we recommended the following landscape conditions:

1. All turf areas that are disturbed by site grading and building construction must be restored by either sodding or seeding.
2. Trees screening the North portion of the development from the highway should be planted in such a way to maximize their potential screening of the roadway when taking into account sightlines from buildings.
3. The land on which screening trees are planted to the north of the development should be made into a separate outlot, owned and managed by the Home Owners Association, who will be liable in cases where replacement and maintenance are necessary.
 - a. This outlot will be common area for the association and will be held to the equal and undivided interest of all residents to ensure its upkeep.

Grading and Drainage Plan. The applicant has submitted a grading and drainage plan that directs hard surface stormwater to three separate stormwater basins. This plan is subject to review and approval by the city engineer.

Pedestrian Access. A public sidewalk or trail is provided on at least one side of the street throughout the development, as city code dictates. Sidewalk width is the required five foot minimum, while the trail that loops around the eastern portion of the development is 10 feet wide. No connection of the sidewalk across Highway 12 is being proposed due to the street being an arterial road and there being concerns over pedestrian safety.

Park Dedication. As per the original plan, the developer is dedicating well above the 10% of gross land required by the city code. While only required to dedicate 5.42 acres of land, a total of 8 acres of upland will be dedicated to the city. In addition, a bituminous paved trail will be constructed at cost of the developer circling the eastern portion of the development and connecting to the Southern public park shown on the plan documents. Originally, a second trail connection was to be provided on the western portion of the project, but city staff requested that this trail not be installed and instead the value of the materials be donated to the City's park and recreation fund.

RECOMMENDATION

Based on our review of the plans submitted on behalf of Paxmar Land Development LLC, dated December 28, 2018, we recommend approval of said plans with the following conditions:

1. An amended front yard setback of 25' be applied throughout the PUD District, or at the least on those properties fronting Willow Avenue or which abut a public sidewalk.
2. All lots be brought into compliance with a 20' side yard setback when abutting right of way.
3. Reposition driveways and curb cuts to provide more continuous sections of on-street parking where possible.
4. Calculate the estimated cash donation for the value of the previously approved bituminous trail that is no longer being installed.
5. The City Engineer reviews and approves the site drainage plan.

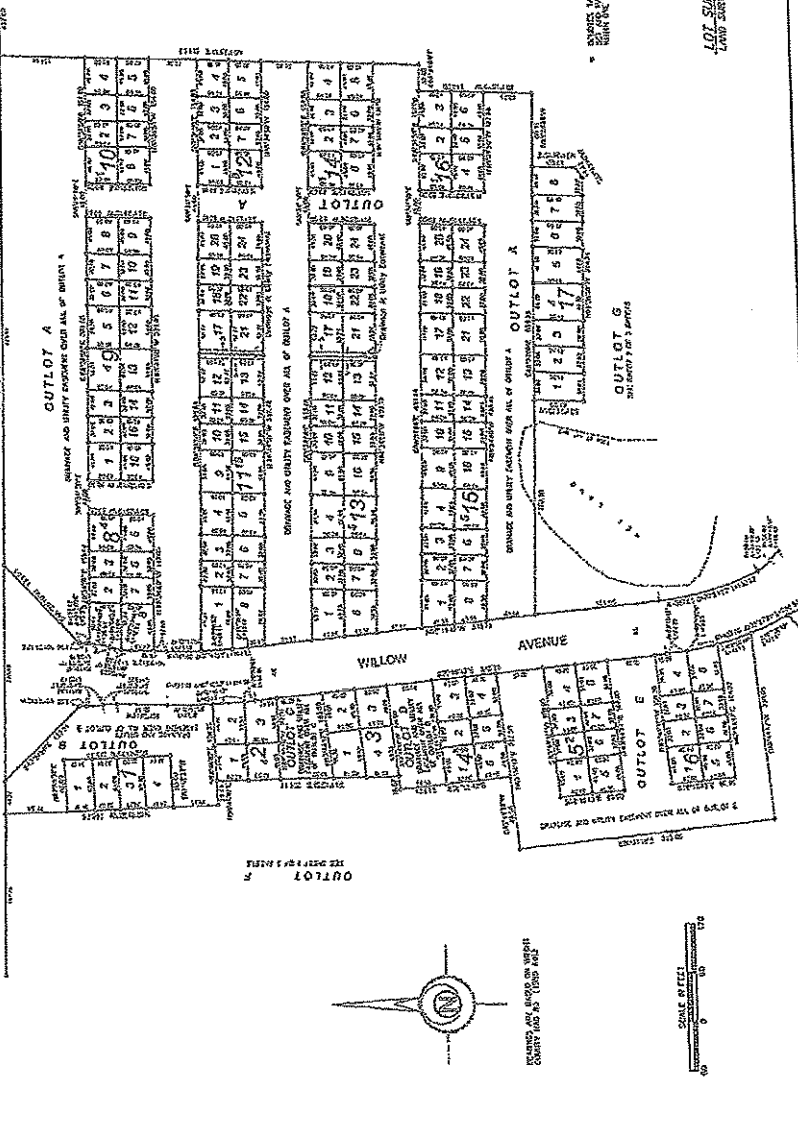
6. The City Fire Chief reviews approves the fire connections to the site.

THE PRESERVE OF MONTROSE

STATE HIGHWAY NO. 12

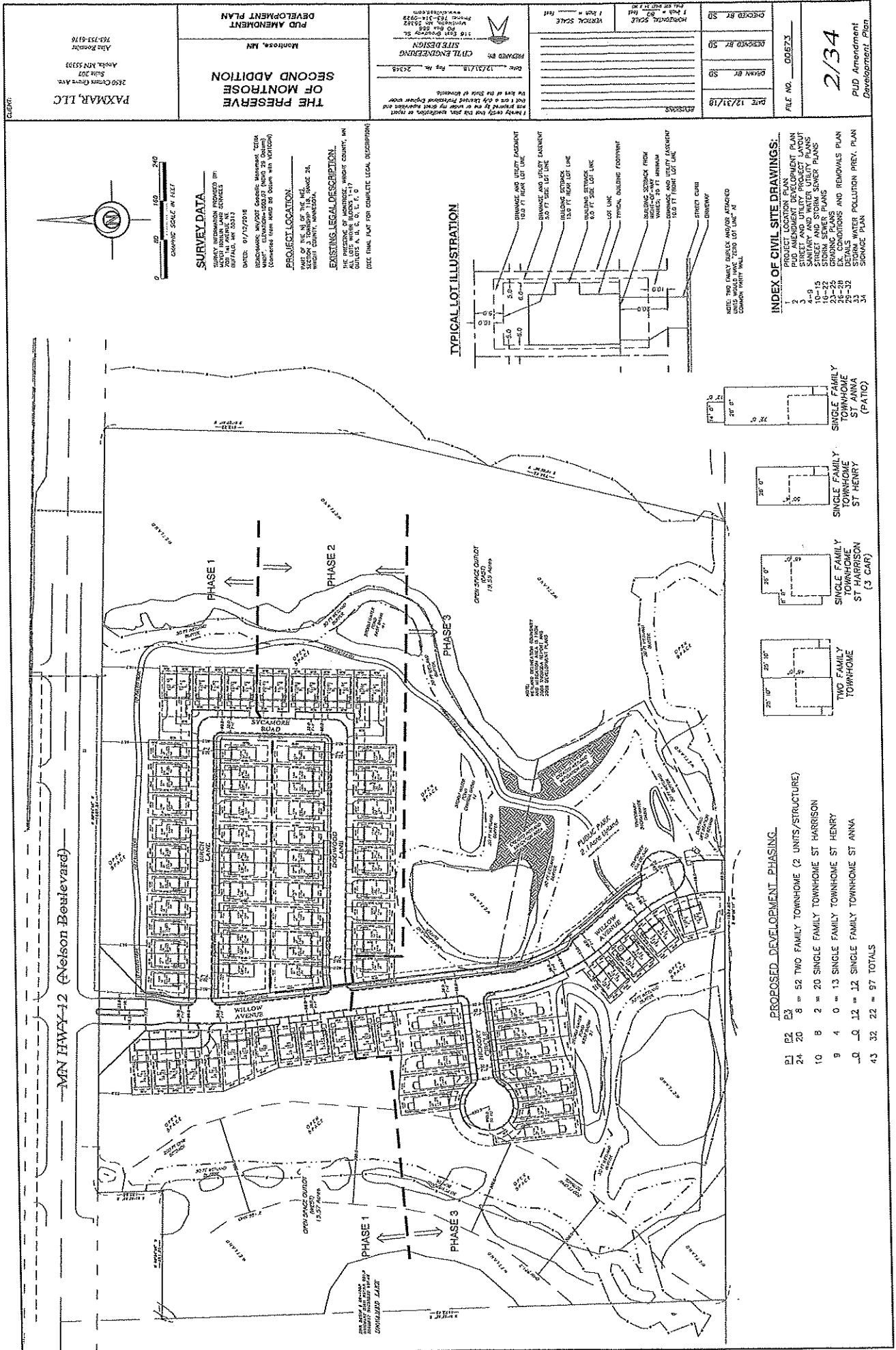
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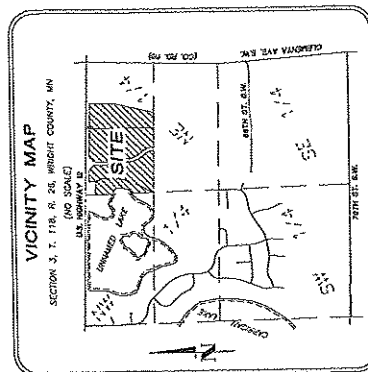


LOT SURVEYS COMPANY, INC.
LAND SURVEYORS

SHEET 3 OF 3 SHEETS

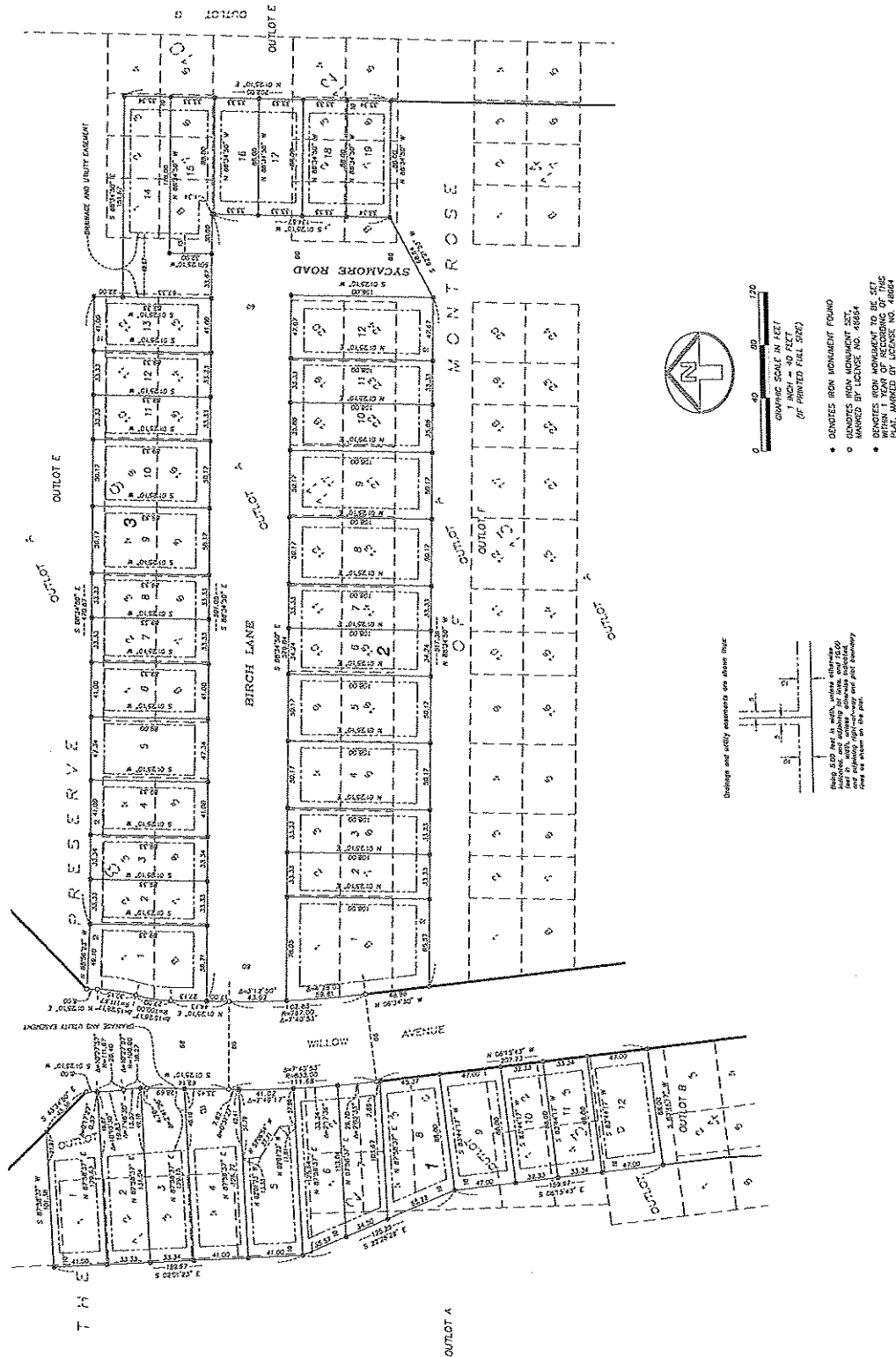


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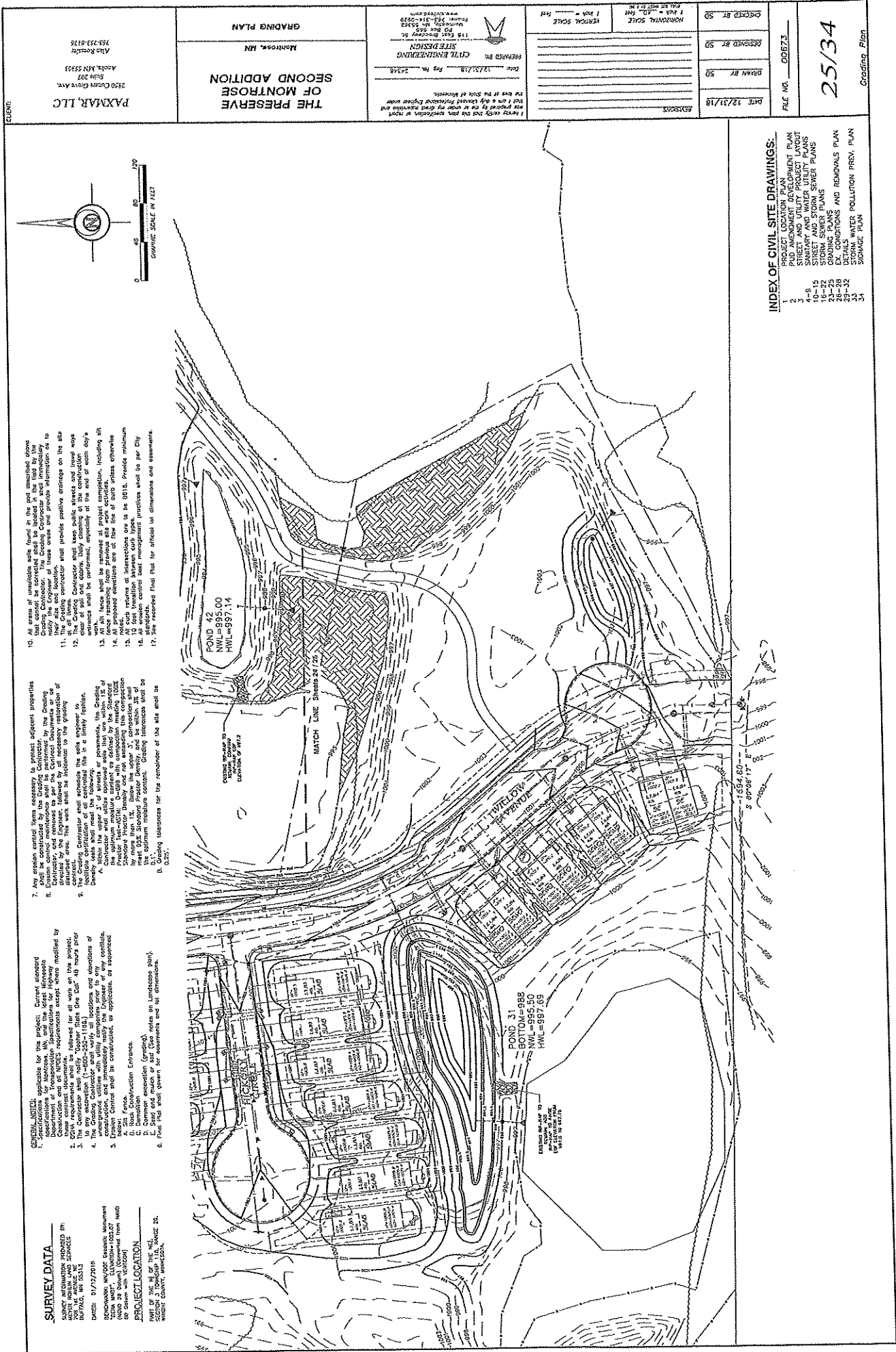


THE PRESERVE OF MONTROSE 2ND ADDITION

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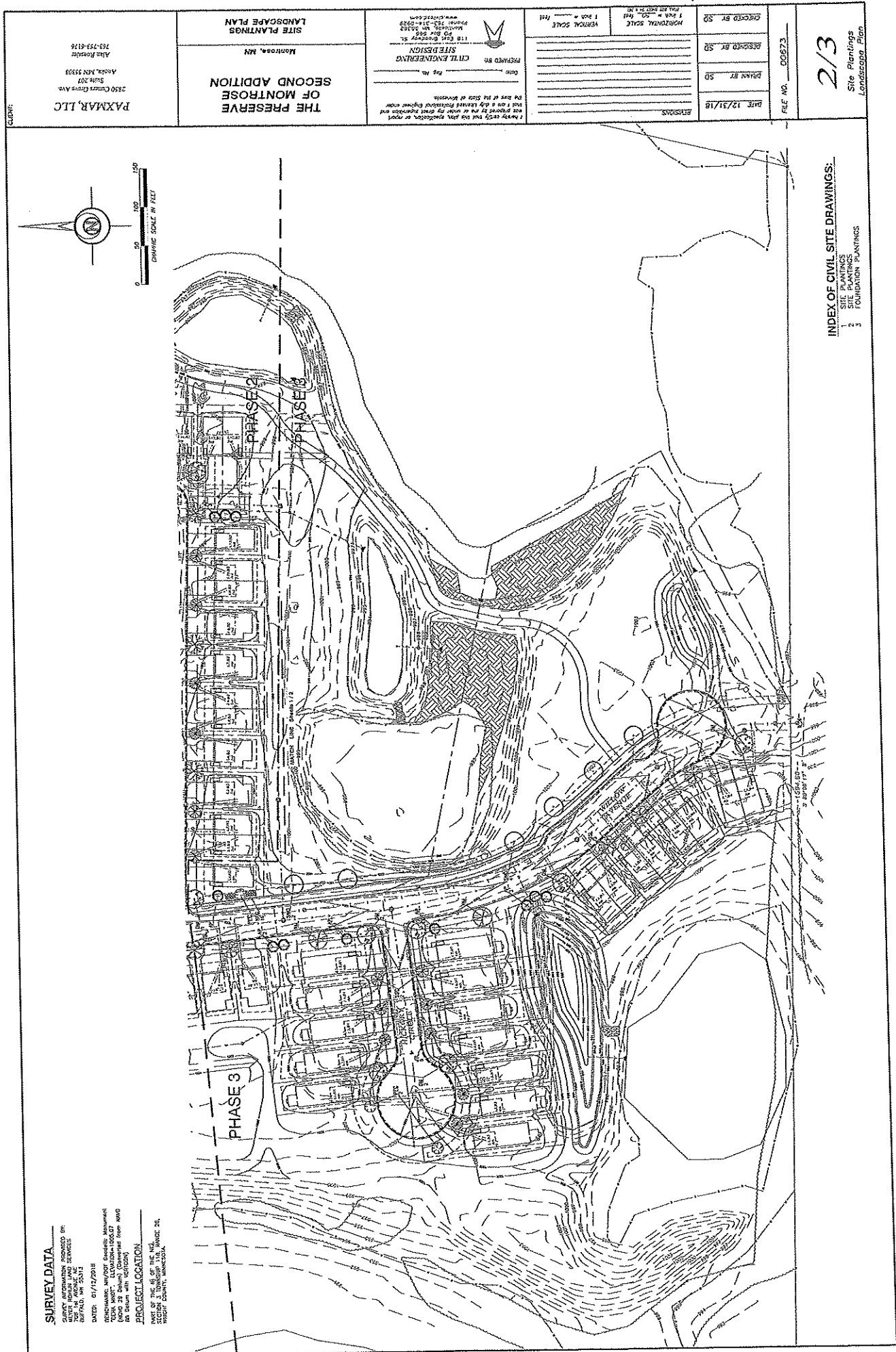


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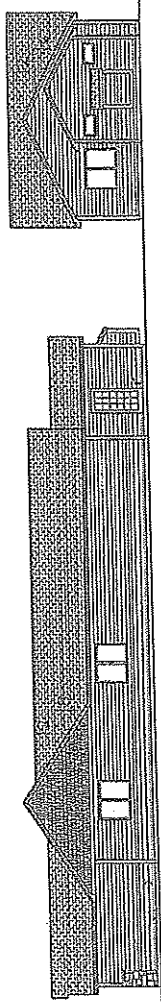


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736-738	STORM SEWER PLANS
739-741	STORM SEWER PLANS
742-744	STORM

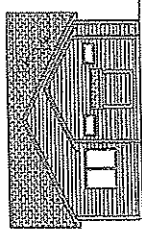


26' Paris Home

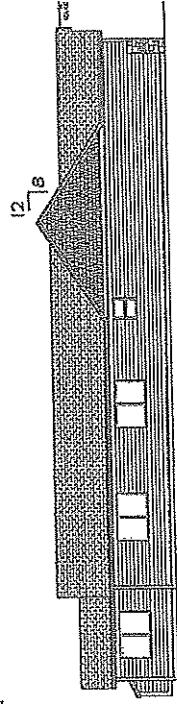


RIGHT ELEVATION
SCALE: 1/8" = 1'-0"

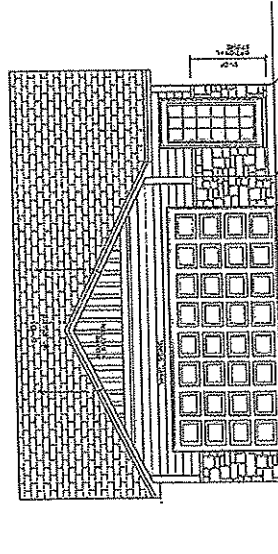
PROVIDE PROPER DRAINAGE
AWAY FROM BUILDING.



REAR ELEVATION
SCALE: 1/8" = 1'-0"



LEFT ELEVATION
SCALE: 1/8" = 1'-0"



DELUXE
FRONT ELEVATION
SCALE: 1/4" = 1'-0"

FLASHING NOTE:
ALL OPENINGS TO INTERIOR MUST
BE FLASHED TO EXTERIOR. FLASHING
IS REQUIRED WHERE ALL
ROOF AND VERTICAL PENETRATIONS
OR WHERE SOME PENETRATIONS CHANGES.

ATTN: VENTILATION:
ALL ROOF VENTILATION MUST BE
PROPERLY FLASHED TO EXTERIOR.
ROOF AND VERTICAL PENETRATIONS
OR WHERE SOME PENETRATIONS CHANGES.

MUST POST ADDRESS ON CONSTRUCTION
SITE VISIBLE FROM ROAD.
EVERY PERSON PERFORMING WORK ON WHICH
THIS PERMIT IS ISSUED SHALL COMPLY
WITH THE CODE.
NO OCCUPANCY OR USE ALLOWED UNTIL FINAL
INSPECTION AND APPROVAL.
STORM WATER RUN-OFF SHALL BE CHANNELLED
AWAY FROM THE BUILDING. NO RUN-OFF SHALL BE
DIRECTED ONTO ADJACENT PROPERTY.
PROPERTY LINES MUST BE SET IN PROPER
STANDARD SURVEY FIRST BE PROVIDED.
CLEAN ATTEMPT ON ALL MATERIAL
PACKED ONTO A FROM THIS SITE.

BUYER NOTICE:
DUE TO THE VARIOUS SITE CONDITIONS
OF INDIVIDUAL LOTS, THE GRADE LINES
REFLECTED BY THIS PRINT ARE
FOR ILLUSTRATIVE PURPOSES ONLY.
AND DO NOT REPRESENT ACTUAL
FINISH GRADE OR ELEVATIONS.

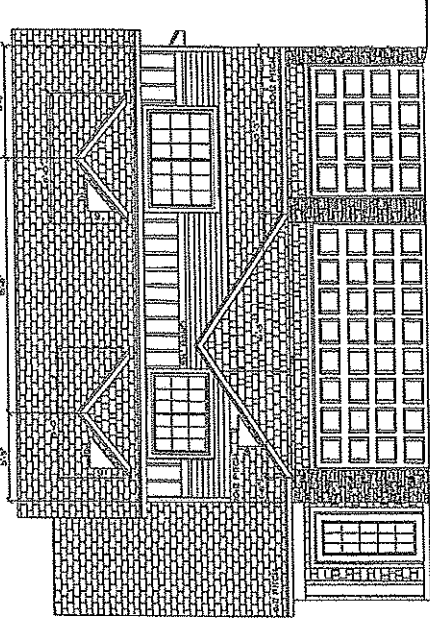
HOUSE INDEMNITY:
THE APPROVED ADDRESS SHALL BE
THE PROPERTY OF THE HOME
PROPERTY. IF ANY ADDRESS IS
USED, CONTACT TO CITY DEPARTMENT.

USE DIRT PROTECTION:
ALL NEW CONSTRUCTION SHALL
BE PROTECTED BY A DIRT MATT
DURING THE CONSTRUCTION PERIOD.
USE 6TH ALL DAYS COMBINED TOGETHER.

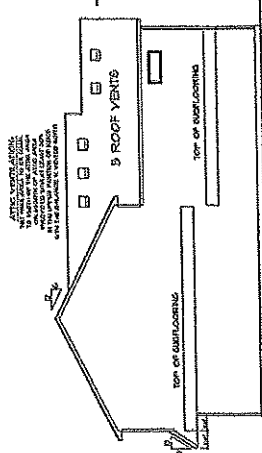
<p>PROJECT NOTES: ST ANNA C2211 PROJECT ADDRESS</p>	<p>DELUXE ELEVATIONS SCALE AS SHOWN</p>	<p>PLAN NOTE: SHEET # 1 OF 9</p>
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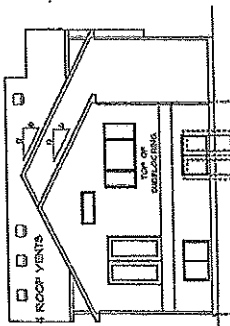
ST. HARRISON
(STANDARD ELEV.)



1 FRONT ELEVATION
AS SCALE: 1/4" = 1'-0"



2 RIGHT ELEVATION
AS SCALE: 1/8" = 1'-0"



3 REAR ELEVATION
AS SCALE: 1/8" = 1'-0"

4 LEFT ELEVATION
AS SCALE: 1/4" = 1'-0"

- NOTE:
- ☐ THIS PLAN IS FOR CLIMATE ZONE 4
 - ☐ THIS PLAN IS FOR CLIMATE ZONE 1

FLASHING NOTE:
ALL FLASHING TO EXTERIOR MUST BE INSTALLED PER MANUFACTURER'S INSTRUCTIONS. ALL ROOF FLASHING MUST BE INSTALLED PER MANUFACTURER'S INSTRUCTIONS. ALL FLASHING MUST BE INSTALLED PER MANUFACTURER'S INSTRUCTIONS.

1'-0" OVERHANGS & EAVES
1'-0" OVERHANGS & GABLE ENDS
ARCHITECTURAL ASPH SHINGLES
ALUMINUM SOFFIT & FASCIA
6" FRIEZE BOARD IN ALL FRONT GABLES
VINYL LAP SIDING (ALL SIDES UNLESS NOTED OTHERWISE)
VINYL BOARD & BATTEN / SHAKES (PER ELEVATION)
4" CORNER TRIM (ALL ELEVATIONS)
4" WINDOW & DOOR TRIM (FRONT ELEVATION ONLY)
STONE PER SELECTION
GRADE LINES ON PLAN DO NOT REFLECT ACTUAL GRADE

PROJECT: ST. HARRISON	DESIGNER: [Signature]	DATE: 10/10/2023
REVISION: (2)	APPROVED BY: [Signature]	DATE: 10/10/2023
PROJECT: ST. HARRISON	DESIGNER: [Signature]	DATE: 10/10/2023
REVISION: (2)	APPROVED BY: [Signature]	DATE: 10/10/2023
PROJECT: ST. HARRISON	DESIGNER: [Signature]	DATE: 10/10/2023
REVISION: (2)	APPROVED BY: [Signature]	DATE: 10/10/2023

CONTRACTOR: [Signature]
CONSTRUCTION PERMITTED ONLY
THIS PLAN IS FOR CLIMATE ZONE 4
THIS PLAN IS FOR CLIMATE ZONE 1

Technical Memo



To: Alan Roessler, Paxmar LLC
Scott Dalhke, P.E. Civil Engineering Site Design

From: Shawn Louwagie, P.E., Montrose City Engineer

Copy: Deb Boelter, Montrose City Clerk
Wayne McCormick, Montrose Public Works Director
Myles Campbell, Montrose City Planner

Date: January 23, 2019

Subject: The Preserve of Montrose 2nd Addition - Plan Review Comments

Site plans dated 12/31/18 have been received for the proposed The Preserve. The following comments are generated from the review of the submitted plans.

Exhibits: The Preserve of Montrose 2nd Addition Development Plans

General

1. Provide a comment response letter addressing each comment below.
2. Provide redundant erosion control protection along wetlands per MPCA requirements.
3. Coordinate small utility crossings within conduit prior to constructing the streets.
4. L4, B3: L2, B1: L3, B1: L9, B2: L5, B3 sanitary and/or sanitary service lines cross the neighboring property. Utility services should be installed directly in front of the property to reduce the length of service and number of bends.
5. Bituminous trail placed around the perimeter of the property shall avoid existing and proposed utility structures.
6. Landscape plan shall avoid placing trees over sanitary, storm, and water utility mains and services. Landscaping shall not be placed in common drainage swales.
7. Coordinate with Xcel to develop the street lighting layout.

Plat

8. Future easements widths for utilities installed through Outlot F to be reviewed with the next addition plat. Typical minimum easement width over watermain is 20' centered over the utility.

Streets

9. Provide crosswalk striping at pedestrian ramps.

Wenck Associates, Inc. | 1800 Pioneer Creek Center | P.O. Box 249 | Maple Plain, MN 55359-0249
Toll Free 800-472-2232 | Main 763-479-4200 | Email: wenckmp@wenck.com | Web: www.wenck.com

10. Include Ped Ramp details and install pedestrian ramps/sidewalks per ADA standards.
11. Provide parallel parking striping on the parking bump out near the at south end of Willow Ave.

Sanitary Sewer

12. Residents own sanitary services to the main (including wye). Therefore, each residence must have its own sanitary service. Lot 4, Block 4 and Lot 6, Block 4 shall not be allowed to have a service wye into the neighboring service line.
13. Provide a detail for the proposed method of bulkheading the abandoned sanitary structure openings.

Watermain

14. Watermain connection to the line on the north side of Hwy 12 to be completed using fittings and gate valve in lieu of wet tap for WM north of US 12.
15. Verify WM layout at intersection of Sycamore and Dogwood. Consider lining the proposed watermain to the north and south and relocating proposed hydrant to avoid crossing service lines.
16. Verify operation of all remaining curbstops and inspect standpipes for corrosion. Replace stand pipes and/or curbstops that are deemed unacceptable by Engineer.
17. Ensure curbstops are located out of trails, sidewalks, and driveways. If they are in pavement, place casting 1914A or approved equal over curbstop.
18. Hydrants 2 and 6 have leads that cross sanitary/water service lines. Adjust the proposed locations to avoid crossing services. Rotate the hydrants to face the street.
19. Rotate hydrant 3 so that it faces the street.
20. Proposed watermain adjustments on the south of the private stub street in the NW of the development should be adjusted to remove the tee/existing bends and installing one 90-degree bend.
21. Verify existing extensions in curb stops and gate valves. Extensions to be installed in valves which do not currently have them. Top of extensions shall be within 18" of the surface.

Storm Sewer

22. There appears to be potential conflicts over some sanitary manholes that are to remain abandoned in place. May need to remove instead of sand fill in order to have room for storm sewer. Line A1, Line C, etc.

23. Insulate WM crossings of 18" or less.
24. Wetland Buffer signs to be placed at all points of deflection and no greater than 200' apart. Staff to provide a proposed figure for placement.
25. Place rip-rap at flared ends
26. Include trash guards on flared ends.
27. Drainage areas appear to be incorrect for areas 14, 15, 58, and 59. Remainder of drainage areas were not checked, please verify that all areas used for calculations are correct. Update and resubmit calculations with correct drainage areas. Add labels to area map that currently don't have one.
28. Provide justification for time of concentration at 15 minutes used in storm sewer pie sizing.
29. Provide additional summary of CB-10 operation and interaction of ponds 41 and 42.
30. Further review of stormwater design submitted is still in progress by Wenck staff. Wenck will need re-submitted calculations to continue review of pipe sizing once drainage areas are updated.

Site Grading

31. Southern drainage swale south of the houses on Dogwood Lane is shown at a proposed slope of 0.38%. Minimum slope in green spaces should be 2% for drainage. Please provide spot elevations.
32. Minimum slope for rear yard drainage between Birch Lane and Dogwood Lane shall be 2%. Areas are shown below 2% slope. Please provide spot elevations.
33. Some areas show 1' contours and some show 2' in the same area. Show 1' contours in all yard areas. Provide consistent proposed contour intervals.
34. Connect silt fence on south end of site.
35. Install silt fence along north edge of site downstream of jacking pit.
36. Indicate location and flow direction of all overland emergency overflows (EOF) and their corresponding elevations.

End of Comments