

# MCMINNVILLE SAFE STREETS TO SCHOOL

## BICYCLE AND PEDESTRIAN PLAN



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# ACKNOWLEDGMENTS

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**A special thanks to the citizens of McMinnville and  
Warren County whose valuable input was critical to this project.**

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## INTRODUCTION

A transportation network with multimodal options is essential to a community, providing economic, health, and quality of life benefits. Access to jobs, education, civic engagement, and other opportunities depends on affordable and convenient transportation options, especially for people who do not or choose not to have a vehicle. Bicycle and pedestrian infrastructure plays an important role in making transportation safe, affordable, and convenient for everyone.

### 1.1 PLAN NEED AND PURPOSE

Over the past decade, the City of McMinnville has undertaken several planning efforts to establish a vision and outline strategies for how the community wants to grow. Transportation systems are a key element in these plans, including the Warren County-McMinnville Comprehensive Plan and the Adventure Tourism Plan for McMinnville-Warren County. Both plans identify a need for improved pedestrian and bicycle facilities to improve the community's overall walkability and bikeability, specifically, between major destinations.

Two current projects, funded by the Tennessee Department of Transportation (TDOT), are helping to improve walking and bicycling in McMinnville. The first, a construction project funded by a Multimodal Access Grant, focuses on improving the Locust Street/N. Chancery Street/State Route 56 (SR 56) intersection for pedestrians. The second, funded by a Community Transportation Planning Grant (CTPG), is developing a bicycle and pedestrian plan for the area south of downtown, focusing on SR 55, 56, and 380.

This plan was developed through TDOT’s Rural Planning Initiative (RuPI), a new grant offered to distressed or at-risk communities located outside of a Metropolitan Planning Organization (MPO) boundary. The purpose of the plan is to document bicycle and pedestrian challenges and opportunities in the neighborhoods north of downtown. Additionally, the plan identifies a schedule and planning-level cost estimates for proposed improvements. Project recommendations build on the vision, goals, and objectives established by the community for the study area and are intended to be implemented as opportunities arise for funding.



*Dottie West Park*

### 1.2 PLAN DEVELOPMENT PROCESS

The plan development process began in January 2021 and was organized around a series of Steering Committee and community meetings. The Steering Committee included representatives from the City of McMinnville’s administrative staff, Community Development Department, Housing Authority, Parks and Recreation Department, and Board of Mayor and Aldermen, with external partners including Warren County Schools, Bike McMinnville, TDOT, the Upper Cumberland Development District (UCDD) and the Tennessee Department of Health’s Healthy Development Program. The plan will be brought before the City’s Board of Mayor and Aldermen in September 2021 for approval and guidance.

Two current projects, funded by the Tennessee Department of Transportation (TDOT), are helping to improve walking and bicycling in McMinnville. The first, a construction project funded by a Multimodal Access Grant, focuses on improving the Locust Street/N. Chancery Street/State Route 56 (SR 56) intersection for pedestrians. The second, funded by a Community Transportation Planning Grant (CTPG), is developing a bicycle and pedestrian plan for the area south of downtown, focusing on SR 55, 56, and 380.

# VISION

*McMinnville's inner city public schools and neighborhoods are seamlessly connected through a network of safe and comfortable streets, walkways, and bikeways that provide affordable, healthy transportation and recreation options.*

## **GOAL 1: CONNECTED AND ACCESSIBLE**

Bicycle and pedestrian networks provide connections to places people of all ages and abilities want to go.

### **OBJECTIVES:**

- 1.1: Strengthen walking, bicycling, and rolling connections between McMinnville's inner city public schools and neighborhoods.
- 1.2: Provide enhanced multimodal access along, across, and to the N. Chancery Street corridor, specifically near Bobby Ray Elementary School.
- 1.3: Reduce car dependence by providing viable and attractive non-motorized transportation options.
- 1.4: Provide consistent wayfinding signage for bicycle routes and other low-stress routes to McMinnville's major destinations.

## **GOAL 2: SAFE AND COMFORTABLE**

People feel safe and comfortable walking and bicycling.

### **OBJECTIVES:**

- 2.1: Provide the right street design in the right place based on adjacent land uses and built environment characteristics to protect and enhance neighborhood livability.
- 2.2: Minimize conflict points between pedestrians and bicyclists and vehicular traffic.
- 2.3: Implement traffic calming measures, where needed, to increase safety and comfort of residential streets.
- 2.4: Provide amenities, such as lighting and street furniture, to increase safety and comfort.
- 2.5: Design networks so that people of all ages and abilities feel safe and comfortable to attract 'interested but concerned' pedestrians and bicyclists.

**20  
YEARS**

## **GOAL 3: ACTIVE AND HEALTHY**

Active, healthy lifestyles are promoted as a preventative community health measure and as part of a high quality of life.

### **OBJECTIVES:**

- 3.1: Improve access to parks, trails, ballfields, and other recreational assets.
- 3.2: Increase Milner Recreation Center membership by strengthening connectivity between neighborhoods, particularly those east of N. Chancery Street.
- 3.3: Increase the number of students walking and bicycling to the elementary and middle schools.
- 3.4: Provide supportive programming for pedestrians and bicyclists, such as Safe Routes to School activities and bicycle maintenance workshops.

## **GOAL 4: COST EFFECTIVE**

Improvements are designed, constructed, and maintained in a cost-effective manner.

### **OBJECTIVES:**

- 4.1: Utilize lower-cost strategies, where feasible, to expand the multimodal network, such as implementing improvements that utilize existing pavement width or right-of-way.
- 4.2: Partner with TDOT regarding any redesign and/or maintenance projects for state routes to ensure considerations for walking, bicycling, and rolling are incorporated.
- 4.3: Pursue state and federal grant opportunities to assist in the development of the bicycle and pedestrian system.

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# 2

*Downtown McMinnville*

## EXISTING CONDITIONS & NEEDS ASSESSMENT

This chapter provides an overview of the study area, including its context within the greater region and the existing conditions for walking and bicycling. A field review was completed in March 2021 to supplement the data collected using a variety of city and state data sources. High-level information relating to the study area’s context is also provided in this chapter, such as existing destinations and uses of parcels.

### 2.1 PLANNING AREA

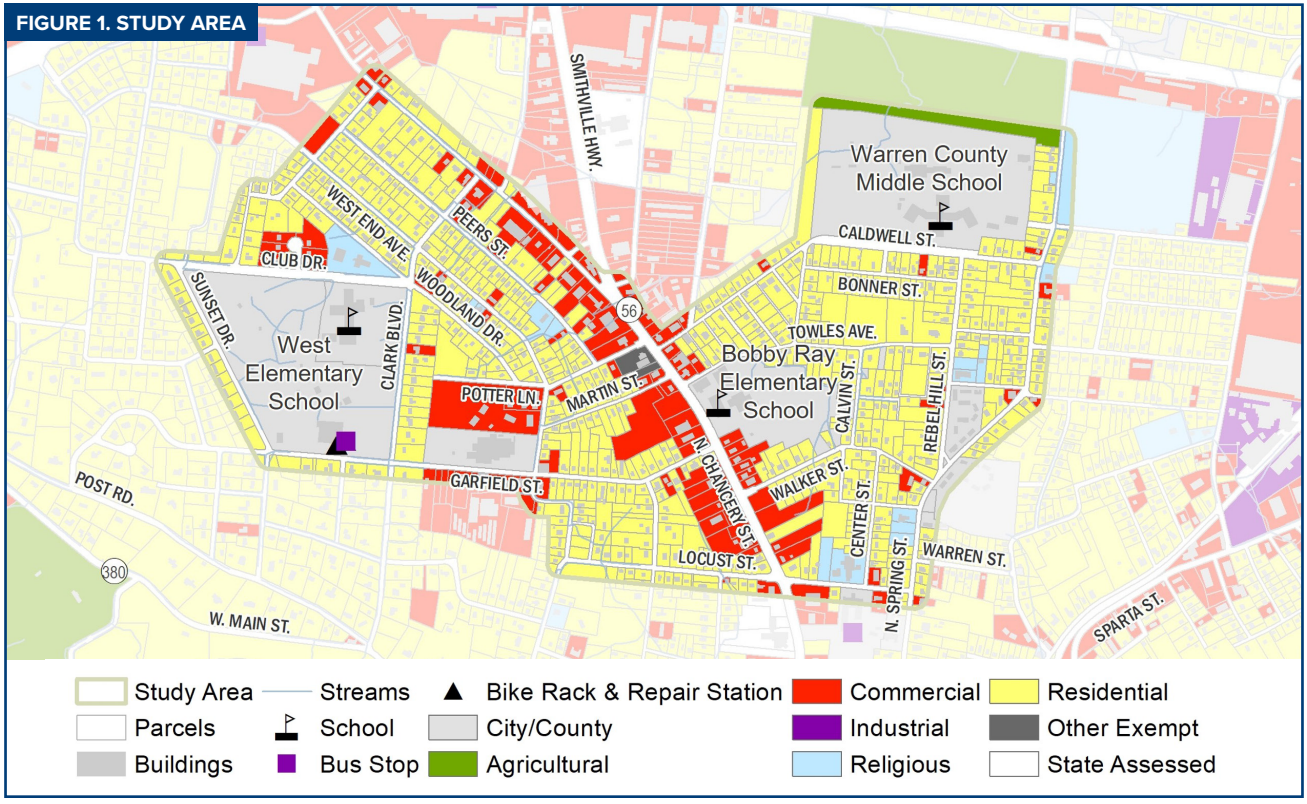
McMinnville is the largest city in and county seat of Warren County with a population of approximately 13,800, according to the U.S. Census Bureau. Major routes providing important connections to the community include U.S. Highway 70S, SR 56, SR 8, and SR 55. Nearby outdoor attractions, such as Cumberland Caverns and Rock Island State Park, are generally located to the north and east of McMinnville. Major attractions within the community include the historic Downtown, Riverfront Park, Dottie West Park, and Pepper Branch Park.

The plan’s study area (Figure 1) includes the N. Chancery Street/SR 56 commercial corridor, three schools – Bobby Ray Memorial Elementary, Warren County Middle School, and West Elementary School – and the community’s largest park, Dottie West Park. The Middle School also houses the High School’s football stadium, which generates significant vehicular and foot traffic on event nights. Dottie West Park includes a number of important community destinations, including one of McMinnville’s greenways, the public pool, a skate park, tennis courts, ball fields, and the recently renovated Milner Recreational Center.



The majority of the land uses in the study area are residential, commercial, or public uses. Commercial is largely concentrated along the N. Chancery Street/SR 56 corridor in a north-south manner, with a small neighborhood node located at the intersection of Garfield Street and West End Avenue. The parcels identified as public use – schools, recreational amenities, and subsidized housing – also generate and attract pedestrians and bicyclists. Trips between these destinations and to/from N. Chancery Street/SR 56 create east-west demand through the study area. Over three-fourths of the parcels are identified as residential and include some of McMinnville’s densest neighborhoods. Beyond recreational walking and bicycling trips, these neighborhoods also generate trips for accessing key community destinations given their proximity.

**FIGURE 1. STUDY AREA**



The City of McMinnville received two planning grants to develop bicycle and pedestrian plans concurrently in two separate study areas (shown below).



**Safe Streets to School Bicycle and Pedestrian Plan**

**SR 55/56/380 Bicycle and Pedestrian Plan**

Study Area	Roadways		
McMinnville City Limits	School		
River	Park and Recreation Facilities		

## 2.2 TRANSPORTATION SYSTEM

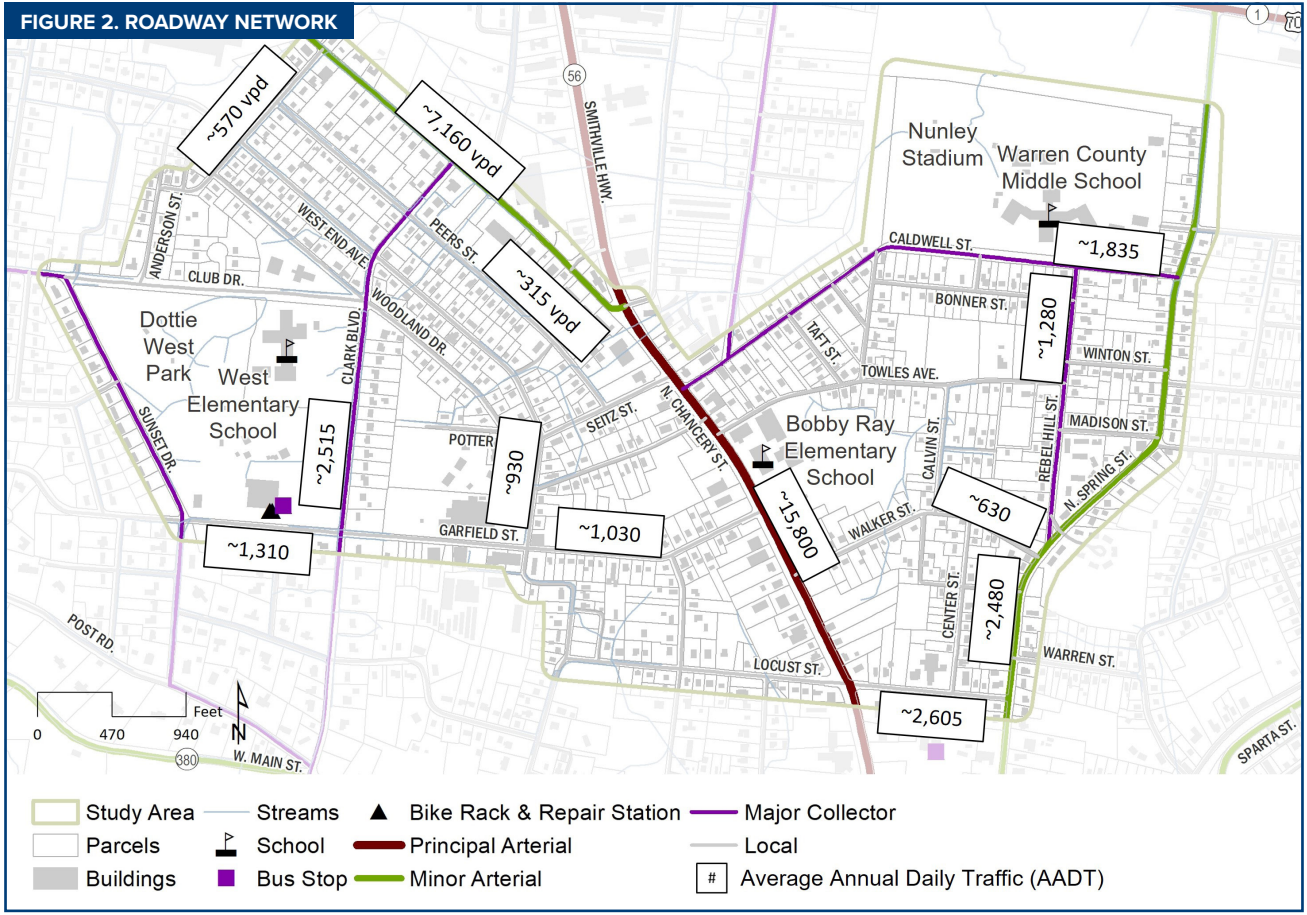
### ROADWAY NETWORK

According to TDOT’s traffic count station data, the highest traffic volumes in the study area are concentrated along the N. Chancery Street/SR 56 corridor (Figure 2). Near Bobby Ray Elementary School, the count station reported an annual average daily traffic (AADT) of approximately 15,800 in 2018. N. Chancery Street, providing a key connection to the Walmart commercial area, carries approximately 7,200 vehicles per day (vpd), while Caldwell Street, Clark Boulevard, Locust Street, and N. Spring Street carry 2,000-3,000 vpd. These important north-south and east-west arterials and collectors are intended to carry higher traffic volumes; however, given the location of critical pedestrian and bicycle destinations along these roadways, special attention should be paid to the quality of multimodal infrastructure provided.

Opportunities to safely cross roadways are crucial to a pedestrian’s or bicyclist’s level of comfort. The greater the traffic volumes, speeds, and number of lanes, the less comfortable one generally feels. The largest barrier in the study area regarding crossing opportunities is N. Chancery Street/SR 56. Despite being five lanes, there are only two signalized crossing opportunities – one at N. Chancery Street and one at Locust Street. The distance between these two traffic signals is a half-mile, creating a barrier for those seeking to cross, especially Bobby Ray Elementary students, which is located in the middle of the corridor. While there is a mid-block crosswalk striped to the north of Garfield Street, there is no signalization or median refuge island to assist people crossing the street.



**FIGURE 2. ROADWAY NETWORK**



**BICYCLE AND PEDESTRIAN NETWORKS**

There are approximately 6.4 miles of existing sidewalk and 0.7 miles of greenway in the study area. The study area’s arterials (i.e., N. Chancery Street/SR 56, N. Chancery Street, and N. Spring Street) largely have sidewalks on at least one side providing connections to commercial destinations and neighborhoods (Figure 3). Major gaps interrupting important connections include Garfield Street, Towles Avenue, and Clark Boulevard. The greenway is contained within Dottie West Park and acts as a recreation and exercise destination for the neighborhood and community.

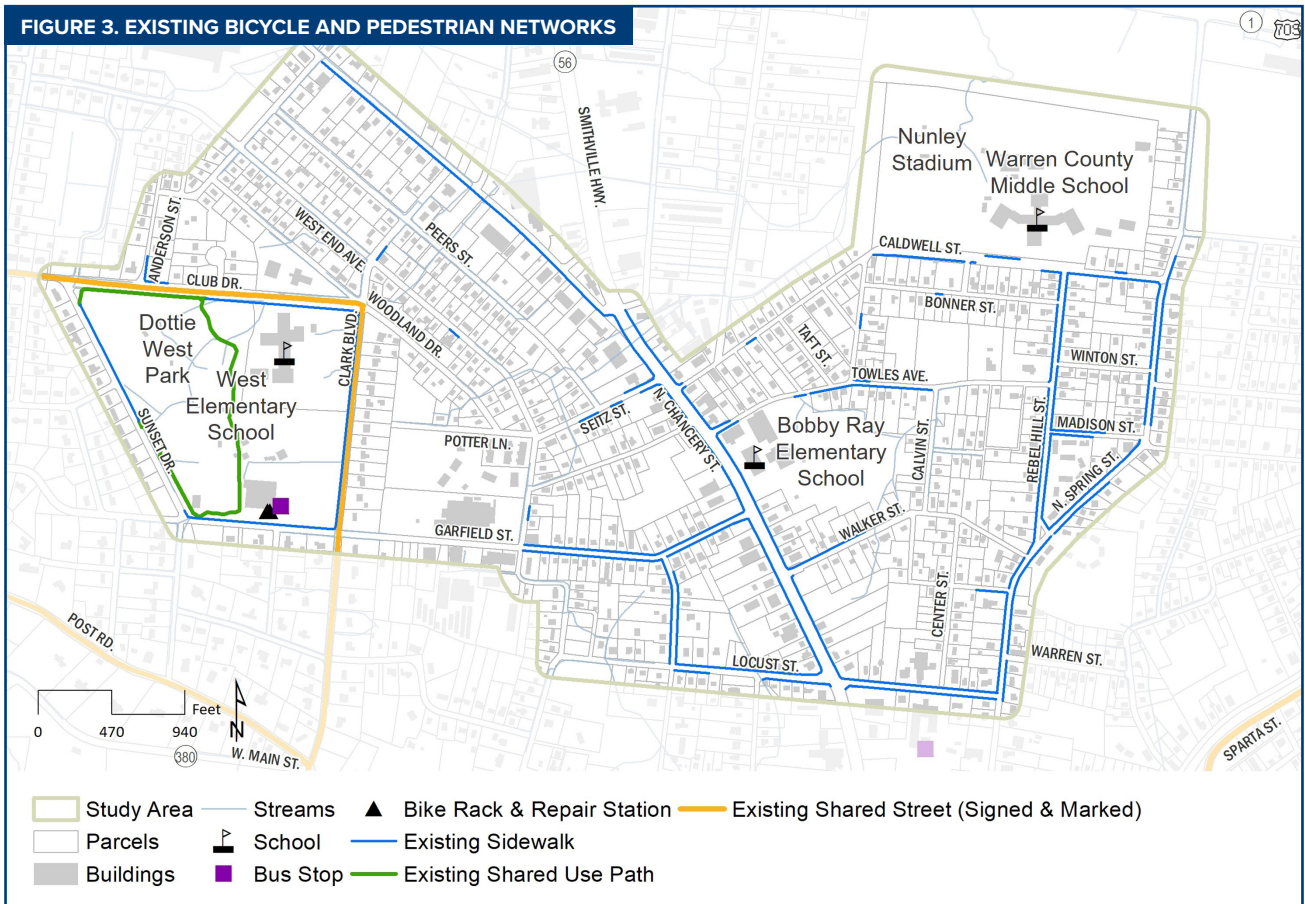


A signed bicycle route (shared street) runs through the study area along Club Drive and Clark Boulevard. The loop route provides a connection between residential areas to the west of the study area and Dottie West Park, West Elementary School, and Milner Recreation Center. While unsigned, a spur route for a state bicycle route (Kentucky to Alabama East) runs to the southeast of the study area along Sparta Street/SR 380. This route provides the most direct bicycling connection between downtown McMinnville and Rock Island State Park.

McMinnville has an active bicycling community, which includes the advocacy committee, Bike McMinnville. This group hosts community bike rides, plans for future improvements, pursues grant applications, and hosts bike rodeos for children. Using a Healthy Building Grant from the Tennessee Department of Health, the City installed several bicycle racks and repair stations at major destinations within the community, including the Milner Recreation Center.



Downtown McMinnville



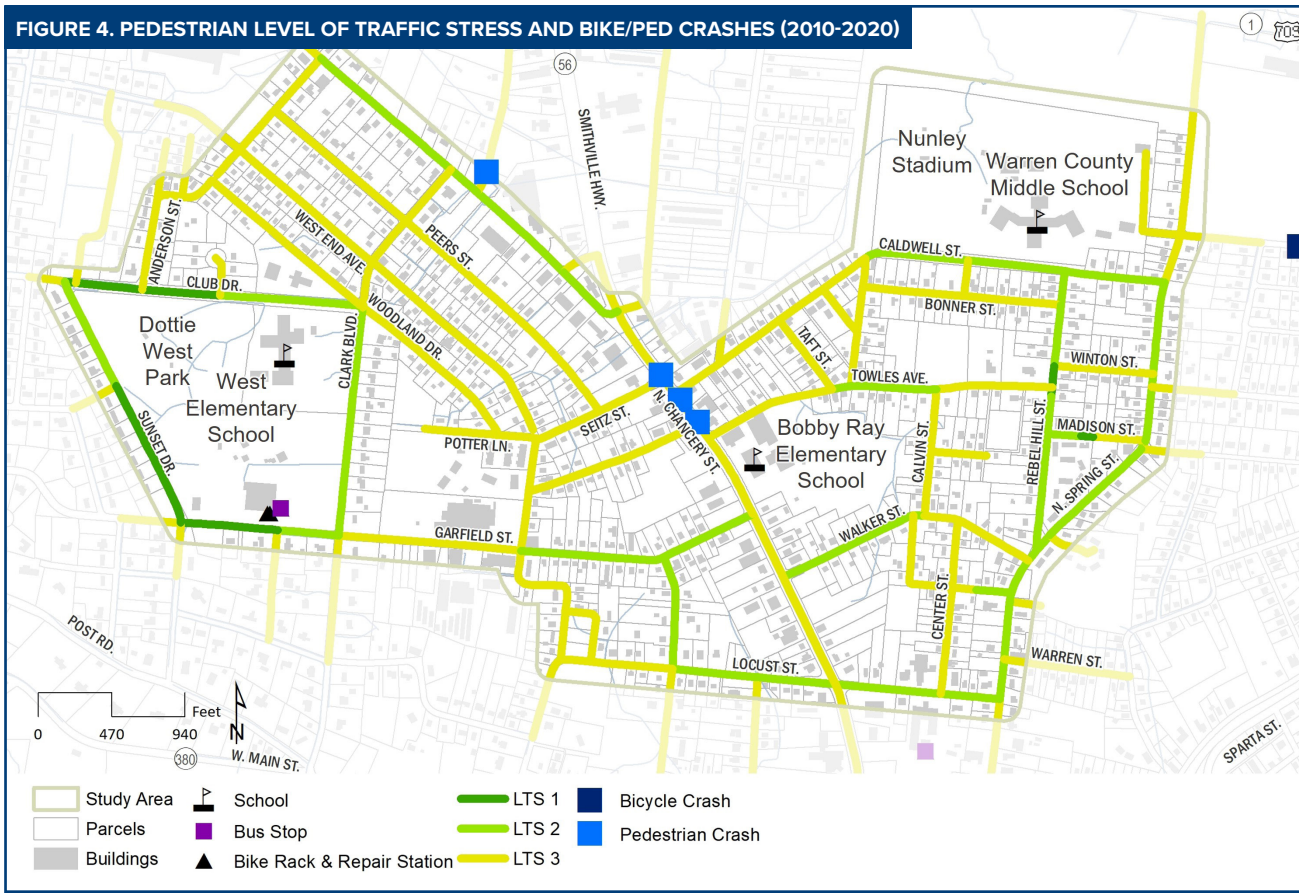
## SAFETY ANALYSIS

Crash data for the past five years (2016-2020) was analyzed for the study area. TDOT's Enhanced Tennessee Roadway Information Management System (E-TRIMS) was used to access crash information, which includes general location, conditions, and type. Overall, there were 234 vehicular crashes resulting in 53 injuries and zero fatalities. Rear-end crashes along N. Chancery Street/SR 56 were the most common, making up 35% of the overall crashes. Ten years of crash data (2010-2020) revealed three pedestrian crashes within the study area which were all located along N. Chancery Street/SR 56 near the Caldwell Street intersection (Figure 4). Two crashes – one pedestrian and one bicycle – were recorded just outside of the study area on Northgate Drive and Mountain Street, respectively.

Figure 4 also includes the pedestrian Level of Traffic Stress (LTS) for the study area, which scores roadways based on the level of comfort for walking. LTS 1 is considered the most comfortable for people of all ages and abilities, while LTS 4 is considered the most uncomfortable. This methodology takes into account the presence of sidewalks, vehicle speeds, and number of travel lanes. While there are no roadways with an LTS 4, major arterials and collectors act as barriers for pedestrians in the study area, including N. Chancery Street/SR 56, N. Spring Street, and Clark Boulevard. The presence of sidewalks improves the level of stress pedestrians encounter along segments of these roadways. Local streets without sidewalks have an LTS 3 because of the posted speed limit of 30 mph.



**FIGURE 4. PEDESTRIAN LEVEL OF TRAFFIC STRESS AND BIKE/PED CRASHES (2010-2020)**



### 2.3 NEEDS ASSESSMENT

The study area has a high potential for generating walking and bicycling trips, and by observation, people are already walking and bicycling for a variety of reasons. The mix of residential and commercial uses, number of community destinations, and urban form contribute to the general walkability and bikeability of the study area. At a high level, the existing sidewalk network provides north-south connectivity to the most critical resources, specifically, commercial destinations and Downtown. Several key collector roadways and connections, however, lack continuous, ADA-compliant sidewalk connections, such as Garfield Street. Local residential streets, largely without sidewalks, provide low-stress connections for both pedestrians and bicyclists given the low volumes and speeds; however, some roads (such as Rebel Hill Street) might warrant traffic calming measures to ensure vehicle speeds stay at a comfortable level.

The need for additional safe crossings across N. Chancery Street/SR 56 is evident, given the lack of signalized intersections and/or highly visible crosswalks with substantial enhancements. All three pedestrian crashes that occurred within the past 10 years have occurred along N. Chancery Street/ SR 56 in the vicinity of the Caldwell Street intersection.



# 3

*McMinnville-Warren County Chamber of Commerce*

## PUBLIC AND STAKEHOLDER ENGAGEMENT

### 3.1 ENGAGEMENT GOALS AND OVERVIEW

The Steering Committee served as a sounding board for project milestones, including identifying the best strategies to engage the public and reviewing draft deliverables. The committee met three times over the course of the plan’s development, some of which were consolidated with the ongoing CTPG for the study area south of Downtown.

Public engagement occurred using several platforms, including virtual and in-person meetings. Overall, approximately 325 people participated in the process. Additional detail is provided in Appendix A. The three primary engagement opportunities included:

- Virtual and in-person surveys – identifying needs;
- Virtual public workshop #1 – discussing needs and the draft vision and goals; and
- In-person public workshop #2 – refining draft recommendations.

### 3.2 KEY TAKEAWAYS

Key takeaways from the public and stakeholder engagement follow.

- The lack of bicycle and pedestrian infrastructure and lack of safe crossings are currently the top two major barriers.
- The Dottie West Park/West Elementary School/Milner Recreation Complex and Downtown are the top two destinations within or adjacent to the study area.
- New greenways and trails were prioritized as the top infrastructure need over maintaining and connecting the existing sidewalk network.
- Locations captured in the online map largely identified places where people currently walk and bicycle, including many low-volume neighborhood streets, such as Seitz Street and Martin Street.
- Opportunities were identified for increased east-west bicycling connectivity between the schools within the study area, as well as north-south connectivity between the study area and other portions of the community.
- The top two intersections along N. Chancery St./SR 56 needing improvements for pedestrians and bicyclists include the ones at N. Chancery Street and Garfield Street. Improving connectivity for those currently walking and bicycling to community resources and the schools were noted as being the impetus behind the need.



# 4

Garfield St.

## RECOMMENDATIONS

Building on the key takeaways from the public and engagement process and the technical analysis, the recommended pedestrian and bicycle improvements include sidewalks, shared use paths, buffered bike lanes, and shared streets that provide safe and comfortable connections to major destinations such as Warren County Middle School, Bobby Ray Elementary School, West Elementary School, and the Milner Recreation Center. Proposed improvements were developed to complement recommendations from the Warren County-McMinnville Comprehensive Plan and the Adventure Tourism Plan for McMinnville-Warren County.

### 4.1 PROPOSED PEDESTRIAN AND BICYCLE NETWORKS

Pedestrian recommendations (Figure 5) focus on closing gaps in the sidewalk network, particularly east of N. Chancery Street/SR 56. A shared-use path provides the major east-west connection from Warren County Middle School and Bobby Ray Elementary School to the Milner Recreation Center for both pedestrians and bicyclists. The recommended bicycle improvements (Figure 6) aim to provide the “right street in the right place.” Buffered bike lanes, for example, are recommended for roadways with the highest traffic volumes — N. Chancery Street/SR 56 and N. Spring Street — to provide additional protection for bicyclists, where feasible. Shared streets, with signs and pavement markings, are appropriate for low-volume roadways, such as Locust Street and Garfield Street. Figures 7-10 show the typical street cross-sections for each facility type within the study area.

For N. Chancery Street/SR 56, it is worth noting that plan participants preferred exploring a potential road diet to include buffered bike lanes and address vehicular access issues along the corridor. If the road diet is deemed infeasible through a future detailed corridor analysis, plan participants expressed interest in a shared-use path on the west side of the street as an alternative.

FIGURE 5. PEDESTRIAN NETWORK RECOMMENDATIONS

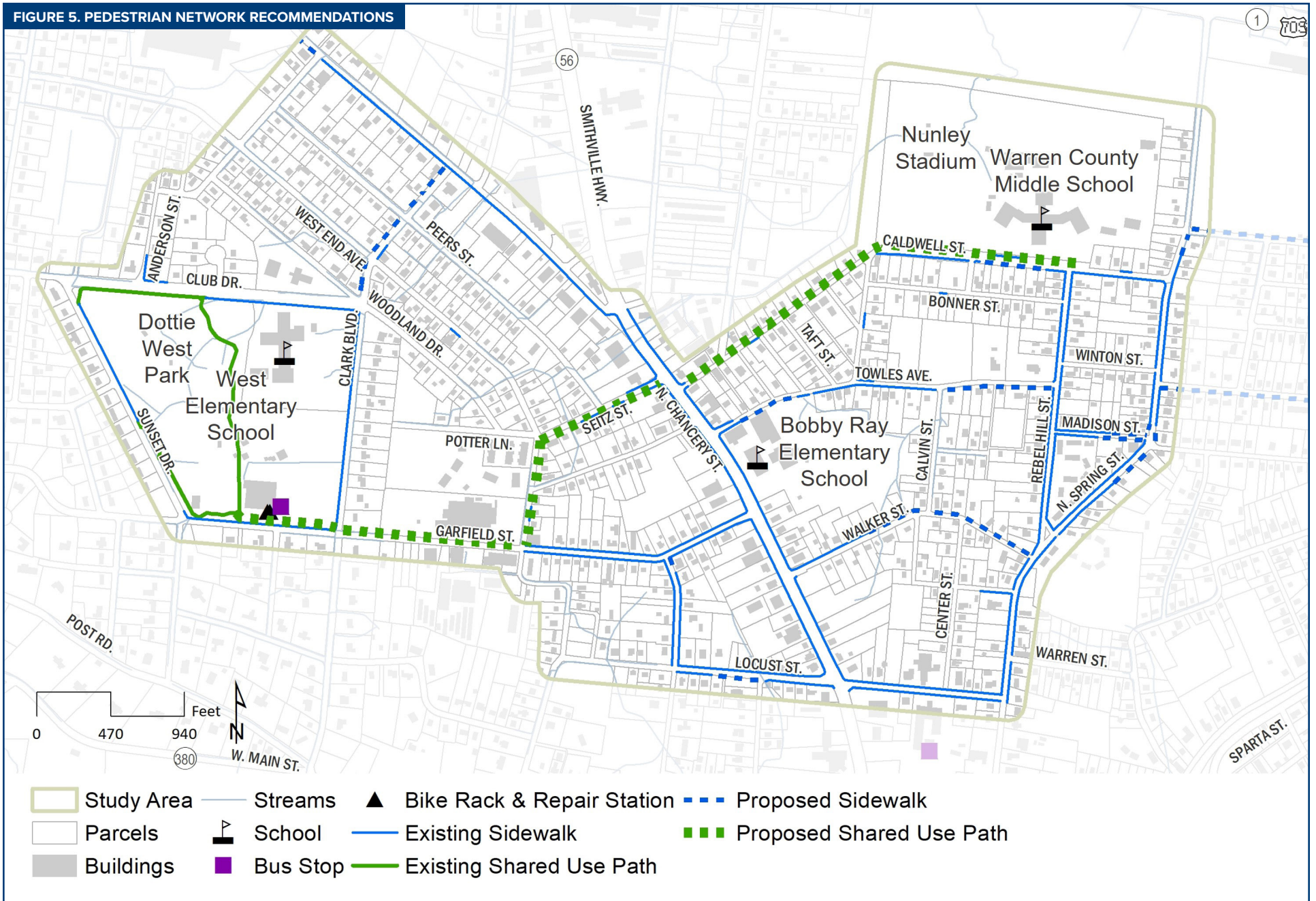
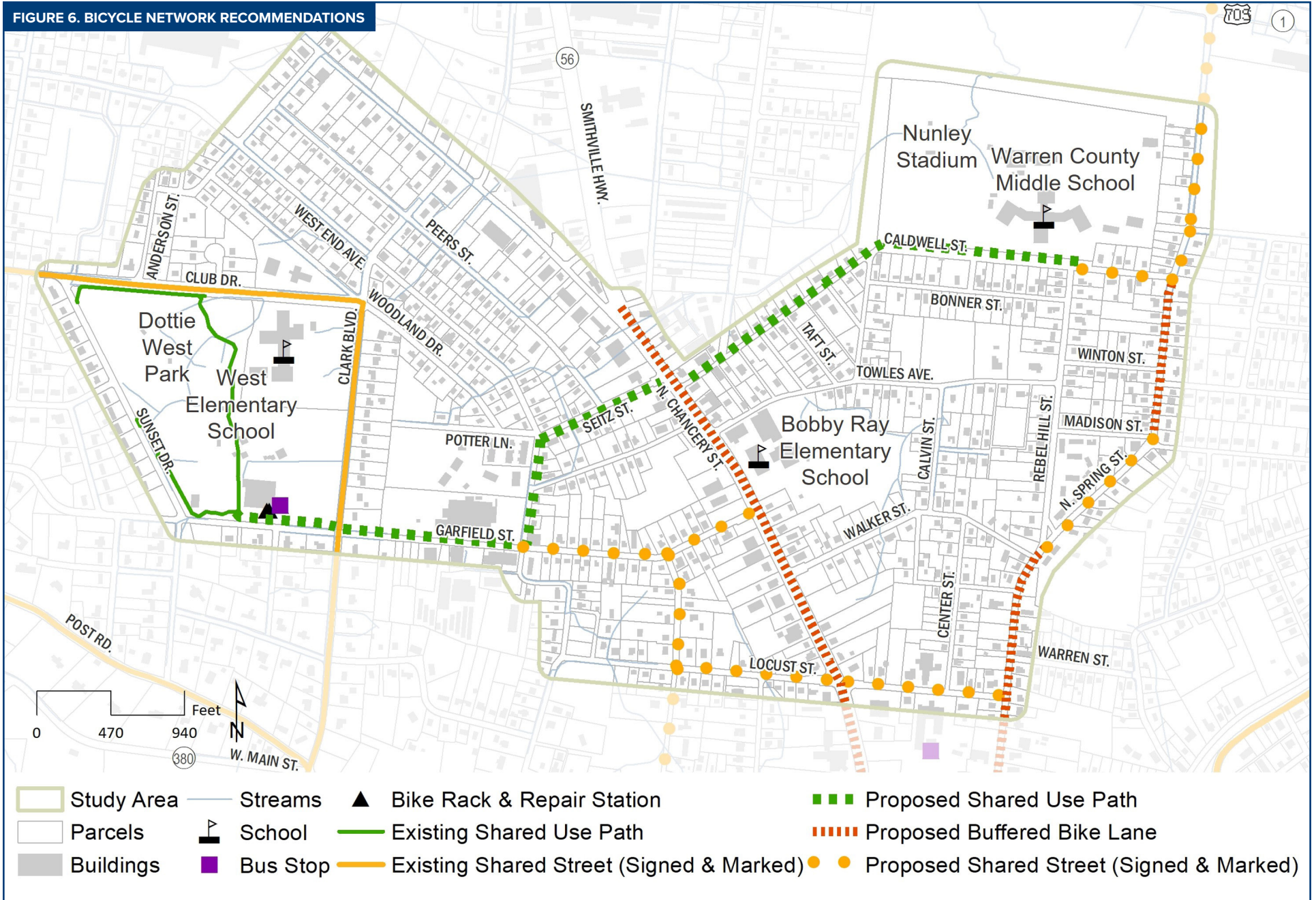
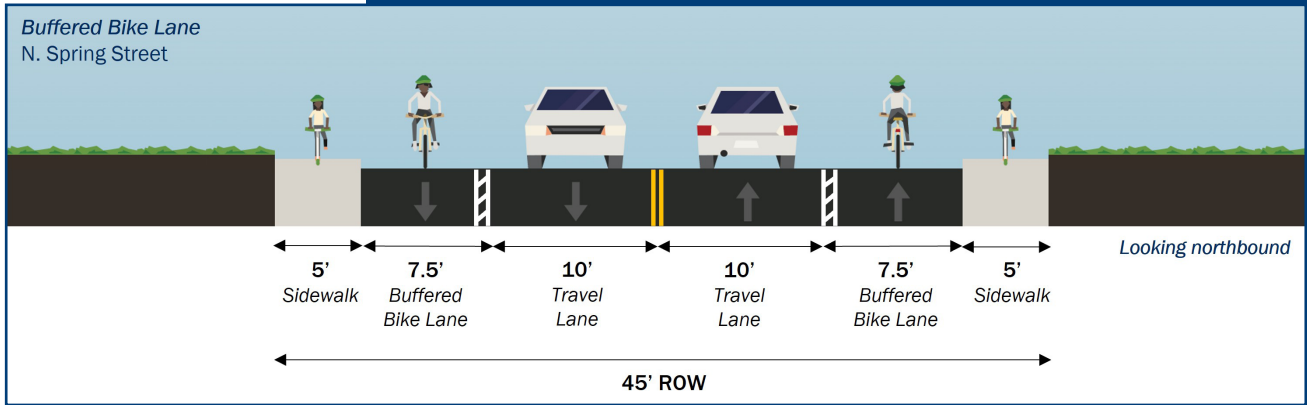


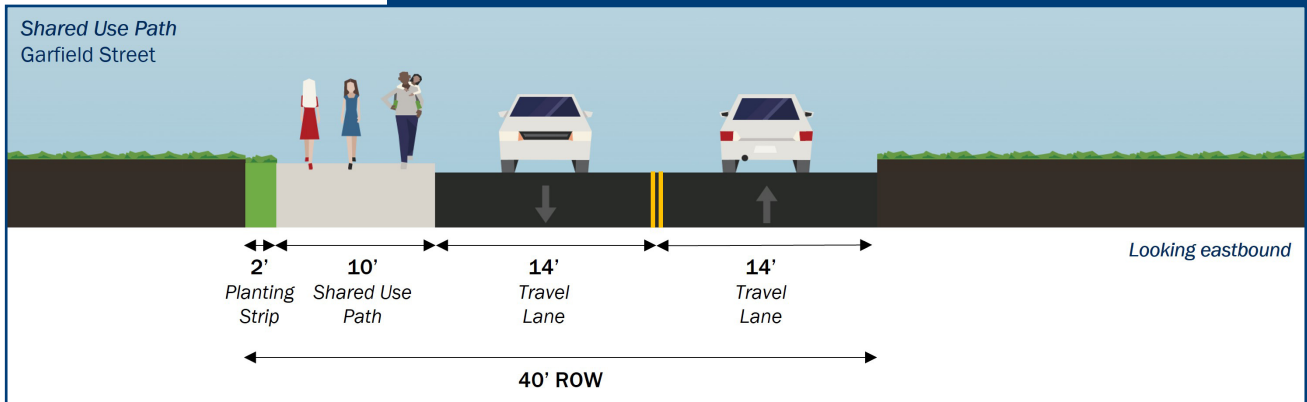
FIGURE 6. BICYCLE NETWORK RECOMMENDATIONS



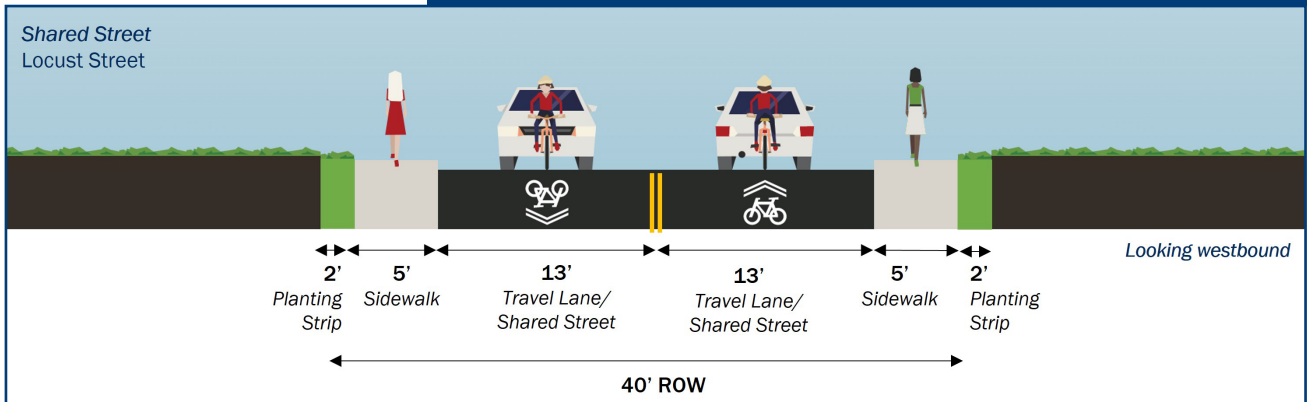
**FIGURE 7. PROPOSED BUFFERED BIKE LANE CROSS-SECTION EXAMPLE: N. SPRING STREET**



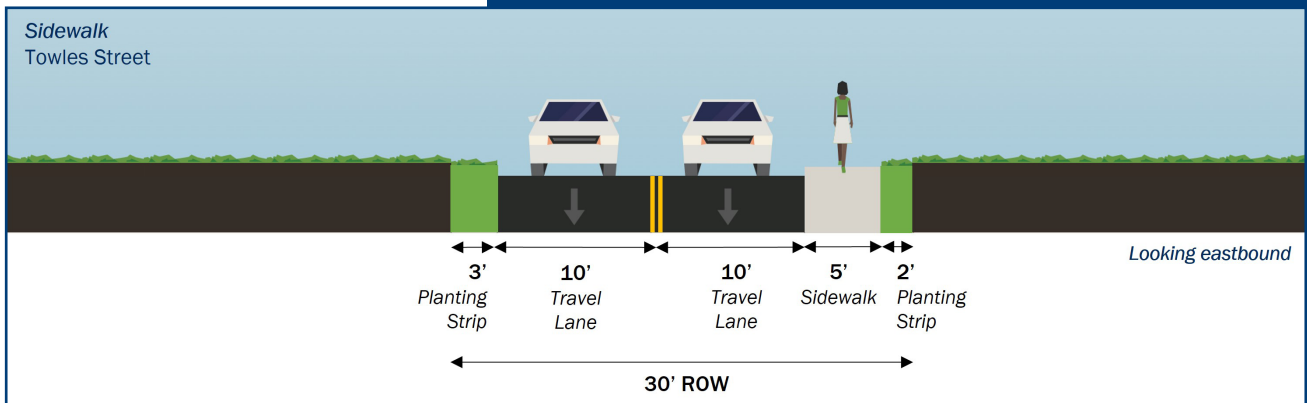
**FIGURE 8. PROPOSED SHARED USE PATH CROSS-SECTION EXAMPLE: GARFIELD STREET**



**FIGURE 9. PROPOSED SHARED STREET CROSS-SECTION EXAMPLE: LOCUST STREET**



**FIGURE 10. PROPOSED SIDEWALK CROSS-SECTION EXAMPLE: TOWLES STREET**



**TABLE 1. RECOMMENDATION COST ESTIMATES**

ID	ROAD	LINEAR FEET (LF)	FACILITY TYPE	UNIT COST (PER LF)	ESTIMATED COST (2019 DOLLARS)
1	<b>GARFIELD ST./WEST END AVE./ SEITZ AVE./CALDWELL AVE.</b>	6,036	Shared Use Path	\$115.00	\$694,101.00
	<b>FROM</b> Existing Shared Use Path <b>TO</b> Rebel Hill St.				
2	<b>N. CHANCERY ST./SR 56 (ROAD DIET)</b>	5,330	Buffered Bike Lane	\$20.00	\$106,600.00
	<b>FROM</b> N. Chancery St. <b>TO</b> Locust St.				
3	<b>N. CHANCERY ST./SR 56 (ROAD DIET ALTERNATIVE)</b>	2,665	Shared Use Path	\$115.00	\$306,475.00
	<b>FROM</b> N. Chancery St. <b>TO</b> Locust St.				
4	<b>CLARK BLVD.</b>	901	Sidewalk	\$65.00	\$58,565.00
	<b>FROM</b> N. Chancery St. <b>TO</b> Woodland Dr./Club Dr.				
5	<b>N. CHANCERY ST.</b>	122	Sidewalk	\$65.00	\$7,930.00
	<b>FROM</b> Existing Sidewalk <b>TO</b> Anderson St.				
6	<b>LIND ST./LOCUST ST.</b>	406	Sidewalk	\$65.00	\$26,390.00
	<b>FROM</b> Existing Sidewalk <b>TO</b> Existing Sidewalk				
7	<b>WALKER ST.</b>	1,095	Sidewalk	\$65.00	\$71,175.00
	<b>FROM</b> Existing Sidewalk <b>TO</b> Calvin St.				
8	<b>TOWLES AVE./FAIR ST.</b>	2,706	Sidewalk	\$65.00	\$175,890.00
	<b>FROM</b> Existing Sidewalk <b>TO</b> Red Rd.				
9	<b>N. SPRING ST./MADISON ST.</b>	1,014	Sidewalk	\$65.00	\$65,910.00
	<b>FROM</b> Existing Sidewalk /N. Spring St. <b>TO</b> N. Spring St./ Existing Sidewalk				
10	<b>CALDWELL ST./FAIR ST./N. SPRING ST.</b>	2,155	Sidewalk	\$65.00	\$140,075.00
	<b>FROM</b> Existing Sidewalk /N. Spring St. <b>TO</b> Existing Sidewalk/Red Rd.				
11	<b>GARFIELD ST./LIND ST./ LOCUST ST.</b>	4,310	Shared Street	\$10.00	\$43,100.00
	<b>FROM</b> West End Ave. <b>TO</b> N. Spring St.				
12	<b>N. SPRING ST./CALDWELL ST.</b>	5,365	Buffered Bike Lane/Shared Street	\$10.00/	\$74,030.00
	<b>FROM</b> Highway 70 <b>TO</b> Locust St.			\$20.00	

\* PLANNING LEVEL COST ESTIMATES ARE BASED UPON TDOT'S 2019 AVERAGE UNIT PRICES. A 40% CONTINGENCY WAS APPLIED FOR ENGINEERING AND CONSTRUCTION PHASES.

## 4.2 IMPLEMENTATION STRATEGIES

The proposed recommendations will improve walking and bicycling between neighborhoods and destinations along the N. Chancery Street/SR 56 corridor. In addition to the major civic destinations — the schools and the Milner Recreation Center — a cluster of retail businesses line N. Chancery Street and N. Chancery Street/SR 56. Improved connections to these commercial areas can boost local businesses and create places where people visit regularly.

## POTENTIAL PRIORITIZATION

A prioritization framework for the long-term recommendations was developed to reflect community needs and priorities. Bicycle and pedestrian recommendations were scored using criteria based on the plan goals, including:

1. Connected and Accessible Walkways and Bikeways (proximity to schools)
2. Safe and Comfortable Streets (existing sidewalks)
3. Active and Healthy Community (proximity to parks)
4. Cost-Effective Implementation (eligibility for TDOT grant funding and critical north-south/east-west shared street connections)
5. Public and Stakeholder Input (proximity to priority destinations)

In addition to strategizing funding availability, the prioritization framework allows for implementation of projects in conjunction with development or redevelopment along the corridor. The prioritization matrix (Appendix B) is intended to provide an idea of which project recommendations best meet the plan’s goals and objectives. Table 2 shows the top projects according to the scoring criteria listed above.

**TABLE 2. POTENTIAL PRIORITIZATION: TOP PROJECTS**

ROADWAY/ FACILITY	GOAL 1: CONNECTED & ACCESSIBLE	GOAL 2: SAFE & COMFORTABLE STREETS	GOAL 3: ACTIVE & HEALTHY COMMUNITY		GOAL 4: COST- EFFECTIVE	GOAL 5: PUBLIC & STAKEHOLDER INPUT
	Proximity to Schools	Proximity to Sidewalks	Proximity to Parks	Grant Eligibility	North-South or East-West Connections	Proximity to Top Destinations Ranked by Public
Shared Use Path along Garfield St./ West End Ave./Seitz Ave./Caldwell Ave.	HIGH	HIGH	HIGH	MEDIUM	LOW	HIGH
Shared Street along Garfield St./Lind St./ Locust St.	MEDIUM	LOW	MEDIUM	MEDIUM	HIGH	HIGH
Buffered Bike Lane/ Shared Street along N. Spring St./ Caldwell St.	MEDIUM	MEDIUM	MEDIUM	LOW	MEDIUM	HIGH
Sidewalk along Clark Blvd.	MEDIUM	LOW	MEDIUM	LOW	LOW	HIGH
Sidewalk along Towles Ave./Fair St.	MEDIUM	LOW	MEDIUM	MEDIUM	LOW	MEDIUM

### 4.3 NON-INFRASTRUCTURE RECOMMENDATIONS AND FUNDING OPTIONS

The recommended capital improvements provide specific solutions that would increase the safety and connectivity for bicyclists and pedestrians in McMinnville. Non-infrastructure initiatives, such as programming and educational materials, provide additional opportunities to encourage walking and bicycling at minimal or no cost to the municipality and can be implemented immediately. Education and awareness initiatives supported through non-infrastructure strategies provide the tools for a community to promote safe streets for all roadway users.

Recommended non-infrastructure strategies are categorized into four groups: education, encouragement, enforcement, and evaluation. Education through schools and other community resources should be targeted towards both active users and automobile drivers. Following educational efforts, encouragement at a municipal level provides the motivation for the community to utilize existing facilities and lets everyone know about the health benefits of walking and bicycling. Enforcement of traffic laws ensures the protection of vulnerable roadway users and reinforces educational measures for drivers. Finally, consistent evaluation ensures an assessment of the quantity and quality of non-infrastructure programming. Specific action steps for each type of non-infrastructure strategy are listed on the following pages.

#### EDUCATION

##### Neighborhood Walking/Bicycling Plans

- The city can work with local partners to create additional walking and bicycling plans that highlight the existing network and local strategies for enhancing bicycle and pedestrian infrastructure beyond the study area. Plans can be broken down by neighborhood to highlight specific infrastructure gaps and needs. Public engagement should be emphasized to ensure all plans represent the needs and priorities of their respective communities.



## Programming

- Programming for school-aged children could be incorporated through partnerships with local schools to promote bicycle and pedestrian safety and healthy lifestyles. Annual events, such as Walk or Roll to School Day, encourage children and their parents to explore transportation options. Schools should provide bicycles for those students who may not have access to a bicycle, promoting equitable participation of students from all backgrounds. Route planning programs, such as Walk Smart, provide students and parents with the location of the safest route to school and should account for intersections, traffic lights, crosswalks, speed limits, roadway classification, and the availability of infrastructure. Additional hands-on education programming can be incorporated into physical education classes at schools. For example, a bike rodeo is an event in which students complete a challenge course on their bicycles, learning how to safely avoid obstacles they may face when bicycling on the street.

## Handout Materials

- Promotional materials, such as yard signs, can be distributed throughout the community to increase the education of residents. “Slow Down” yards signs can be made available for residential areas upon request of homeowners. In areas with limited English-speaking populations, signs should be available in other languages.

## Bicycle and Pedestrian Maps

- The City should continue its partnership with Bike McMinnville, producing and distributing maps created by the organization that highlight destinations in the community. Maps identifying the bicycle and pedestrian networks can be distributed throughout the community in civic buildings – such as schools, the library, and city hall – as well as private businesses.



### Business Educational Materials

- Informational brochures can be distributed to local business owners about providing active transportation infrastructure, such as bike racks. Benefits and general costs for common infrastructure should be included.

### Crossing Guard Training Videos

- Educational training through in-person sessions and videos can be provided for crossing guards near schools. Additional resources, such as an annual stipend, can be provided for non-law enforcement citizens who wish to volunteer to be a crossing guard at high-risk intersections.

## ENCOURAGEMENT

### Provisional Accessories

- The city should consider providing accessories that promote bicycling and walking near schools. Bicycle helmets for children could be provided for those who cannot afford one. Vests and stop paddles could be provided for crossing guards and other enforcement volunteers.

### Tactical Urbanism

- The city could partner with local organizations to spearhead tactical urbanism and/or pilot projects in the community. Annual demonstrations could enable residents and stakeholders to participate in relatively inexpensive temporary transformations with bicycle and pedestrian infrastructure or traffic calming measures, allowing them to experience and observe changes themselves.

### Complete Streets Policy

- The adoption of a Complete Streets Policy would assist in prioritizing safety, comfort, and access to destinations for those walking or bicycling. The policy would require planners and engineers to include considerations for those walking and/or bicycling when designing streets. Policies may include slower vehicle speeds in certain areas or require the provision of sidewalks and other appropriate infrastructure. Considerations for transit could also be incorporated into the policy, where applicable.



*Demonstration Traffic Circle in Nashville, TN*

### Prioritize Bicycle and Pedestrian Projects

- The city should prioritize bicycle and pedestrian projects to expand infrastructure throughout the city, including with development or redevelopment opportunities. Throughout the project scoring process, those with active transportation infrastructure should be given additional points, particularly if the proposed infrastructure will complete an identified gap in the bicycle or pedestrian network. Additional priority should be implemented for projects in areas with traditionally underserved communities.

### Community-Benefit Agreement

- A community-benefit agreement (CBA) would provide developers incentives to incorporate active infrastructure into new developments. Requiring the incorporation of public input through a CBA promotes transparency and allows opportunities for the community to shape aspects of the upcoming development. In return, the support from the community reduces the project risk for private developers. CBAs could also require the developer to improve existing infrastructure for the community, such as making sidewalk repairs or adding a pedestrian signal head to an existing traffic signal.

## **ENFORCEMENT**

### Enforcement Campaign

- The city and local partners can host a publicized enforcement campaign for ordinances related to bicycle and pedestrian safety. Public service announcements can be aired via radio, local newspapers, and social media about local right-of-way laws and ordinances that protect bicyclists and pedestrians. Equitable enforcement can be applied to areas more densely populated, such as Downtown, or in areas where speeding is occurring.

### Safe Driver Pledge

- Campaigns that promote individuals' commitments to safety, such as a safe driver pledge, can be encouraged by the city. Local leaders in the community can promote the social responsibility of individuals.

## **EVALUATION**

### Bicycle/Pedestrian Program Staff

- The creation of a full-time staff position specifically for bicycle and pedestrian implementation would ensure the city is constantly gathering feedback on existing infrastructure, needs, and opportunities. Staff leading an active transportation program could provide knowledge and experience for community organizations and city staff. With a deep knowledge of active transportation infrastructure, the position would be prepared to apply for additional grants and funding opportunities for local infrastructure.

### Data Collection

- Continuous data collection should be encouraged, including pedestrian and bicycle counts, up-to-date information on crashes, and evaluation of speed-related issues on shared streets. Furthermore, completing a detailed inventory of existing sidewalks and their conditions should be

considered. Coordinating with nearby municipalities and/or destinations (such as Rock Island State Park) regarding bicycling or walking connectivity, should be considered to ensure the networks are continuous.

#### Public Outreach

- Feedback on existing infrastructure and the implementation of additional active transportation infrastructure could be achieved through various outreach efforts to the general public. Mail or online surveys allow residents to comment on specific needs. Public input should be gathered regularly to ensure the city is aware of community needs. Additional outreach should be encouraged in areas with traditionally underserved populations.

#### Equitable Funding Distribution

- Funding for active transportation infrastructure should be reviewed annually to ensure equitable distribution among residents. Continual analysis to identify areas with a high population of traditionally underserved communities allows the city to ensure infrastructure is provided to those who may not have access to a vehicle.

#### Integration with Other Plans

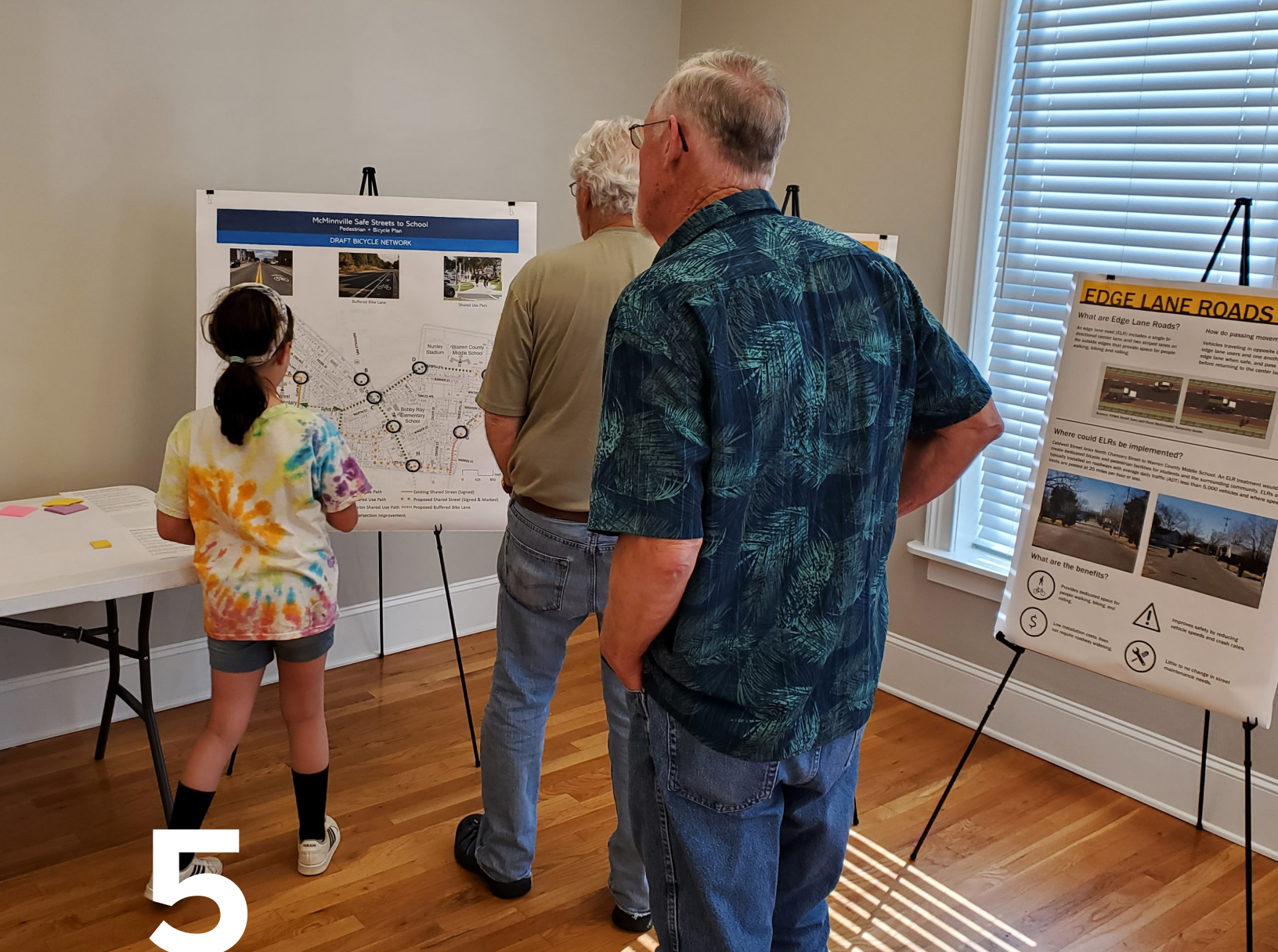
- Strategies for implementing bicycle and pedestrian infrastructure should be incorporated into city and regional plans. Long range transportation plans, transportation improvement programs, and other regional planning documents should identify active transportation infrastructure based on the recommendations of this plan.

Grant programs administered by various state departments are available to counties and municipalities for transportation plan development, bicycle and pedestrian infrastructure design, construction, and maintenance. Table 3 provides an overview of some of the available funding sources for counties and municipalities in Tennessee.



**TABLE 3. FUNDING SOURCES**

<b>GRANT/PROGRAM &amp; ADMINISTERING AGENCY</b>	<b>PROGRAM FOCUS</b>	<b>ELIGIBILITY</b>	<b>FUNDING DETAILS</b>
<b>TRANSPORTATION ALTERNATIVES PROGRAM (TAP)</b> TDOT LOCAL PROGRAMS DEVELOPMENT OFFICE	On- and off-road pedestrian and bicycle facilities, rails to trails, safe routes to school, scenic overlooks, and highway archaeological assessments.	All governmental agencies are eligible.	20% local match required. Local agency is responsible for all preliminary engineering, design, and right-of-way expenses.
<b>MULTIMODAL ACCESS GRANT (MMAG)</b> TDOT MULTIMODAL TRANSPORTATION RESOURCES DIVISION	Pedestrian, bicycle, and transit infrastructure projects along a state route for new construction or rehab of existing facilities.	Priority given for projects along state routes and are located within an at-risk or distressed county.	5% local match required. State match amount will not exceed \$950,000.
<b>SURFACE TRANSPORTATION BLOCK GRANT (STBG) PROGRAM</b> FEDERAL HIGHWAY ADMINISTRATION	Improvements to federal aid eligible roads, Rideshare/ vanpool projects, intelligent transportation systems, incident management activities, and any activity that is also eligible under TAP	Cities with a population of at least 5,000.	20% local match required. Some projects (traffic signalization, pavement markings, traffic circles, guardrail) eligible for 100% federal funding. TDOT may provide the required 20% match upon request.
<b>COMMUNITY TRANSPORTATION PLANNING GRANT (CTPG)</b> TDOT LONG RANGE PLANNING DIVISION – OFFICE OF COMMUNITY TRANSPORTATION	Complete streets plan; corridor study; bicycle/ pedestrian plan; transportation systems management & operations; road diet analysis; or a community mobility plan.	County or municipality outside of an MPO boundary. Plan with focus area that includes a state route.	\$125,000 maximum grant. 10% local match required. Distressed counties qualify for no local match.
<b>LOCAL PARKS AND RECREATION FUND (LPRF)</b> TENNESSEE DEPARTMENT OF ENVIRONMENT AND CONSERVATION	Land acquisition, indoor/ outdoor recreation facilities, and trail development.	City or county governments eligible.	50% local match required.
<b>RECREATIONAL TRAILS PROGRAM (RTP)</b> TENNESSEE DEPARTMENT OF ENVIRONMENT AND CONSERVATION	Land acquisition, trail development, construction, and maintenance, trail restoration, and trailhead support facilities.	Government agencies and eligible non-profits. Must be on publicly owned land.	20% local match required.
<b>ACCESS TO HEALTH THROUGH HEALTHY BUILT ENVIRONMENTS</b> TENNESSEE DEPARTMENT OF HEALTH	Pedestrian and bicycle infrastructure and trailhead design.	All government agencies eligible.	\$85,000 maximum grant.
<b>PROJECT DIABETES</b> TENNESSEE DEPARTMENT OF HEALTH	Pedestrian and bicycle infrastructure and trailhead design.	All government agencies eligible.	\$85,000 maximum grant.



# 5

## CONCLUSION

The development of McMinnville’s Safe Streets to School Bicycle and Pedestrian Plan is an important step in solidifying the community’s vision for a seamlessly connected network of safe and comfortable streets near the centrally located schools. Strategies and recommendations will aid in the implementation and development of bicycle and pedestrian infrastructure between major destinations in the area, including the schools, Milner Recreation Center, and Downtown. Expanding the network of existing infrastructure will benefit the health and safety of the community and provide a greater return on investment with improvements. With the plan, McMinnville will have affordable, healthy transportation and recreation options that benefit the community for years to come.

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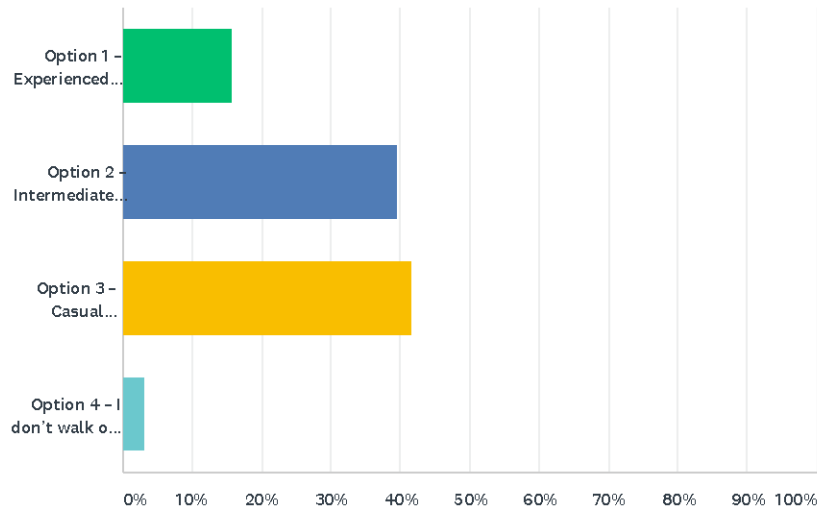


# PUBLIC ENGAGEMENT DOCUMENTATION

PUBLIC WORKSHOP #1:  
**ONLINE SURVEY RESPONSES**

Q1 How would you categorize yourself in terms of your level of confidence in walking or bicycling?

Answered: 255 Skipped: 3

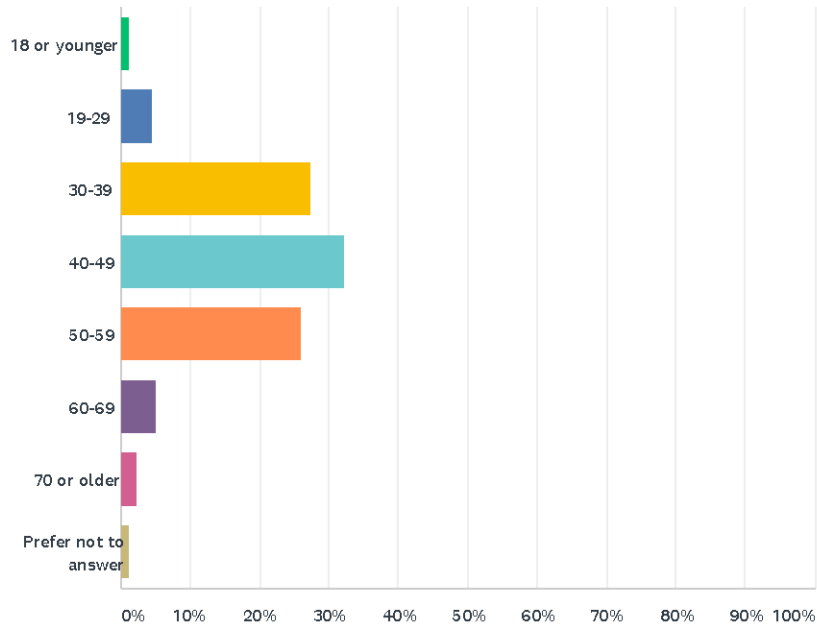


ANSWER CHOICES	RESPONSES
Option 1 – Experienced (strong and fearless) – Confident and comfortable riding with or walking next to traffic (i.e., shared roadways) in most situations	15.69% 40
Option 2 – Intermediate (enthused and confident) – Prefer to walk or bike on separated paths, but will walk or ride on roads in the right situations (i.e., where space is available, such as a bike lane or a paved shoulder, and traffic is manageable)	39.61% 101
Option 3 – Casual (interested but concerned) – Only feel safe on separated pathways (greenways) or sidewalks with few larger roadway crossings	41.57% 106
Option 4 – I don't walk or bike and don't plan on it	3.14% 8
<b>TOTAL</b>	<b>255</b>

McMinnville's Bicycle and Pedestrian Plans

Q2 What age group are you in?

Answered: 255 Skipped: 3

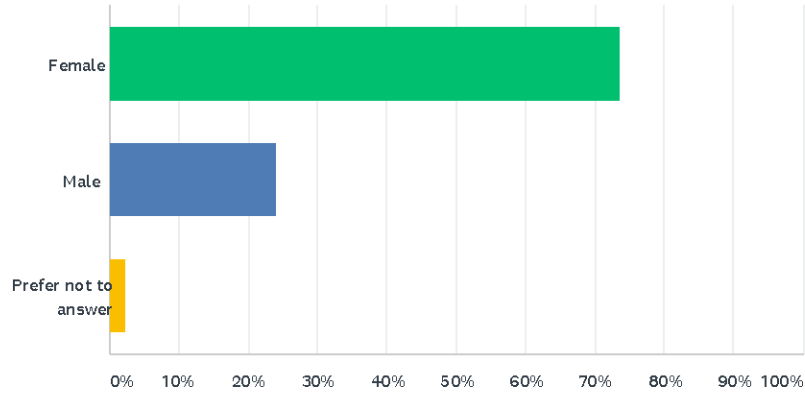


ANSWER CHOICES	RESPONSES	
18 or younger	1.18%	3
19-29	4.71%	12
30-39	27.45%	70
40-49	32.16%	82
50-59	25.88%	66
60-69	5.10%	13
70 or older	2.35%	6
Prefer not to answer	1.18%	3
<b>TOTAL</b>		<b>255</b>

McMinnville's Bicycle and Pedestrian Plans

Q3 What is your gender?

Answered: 254 Skipped: 4

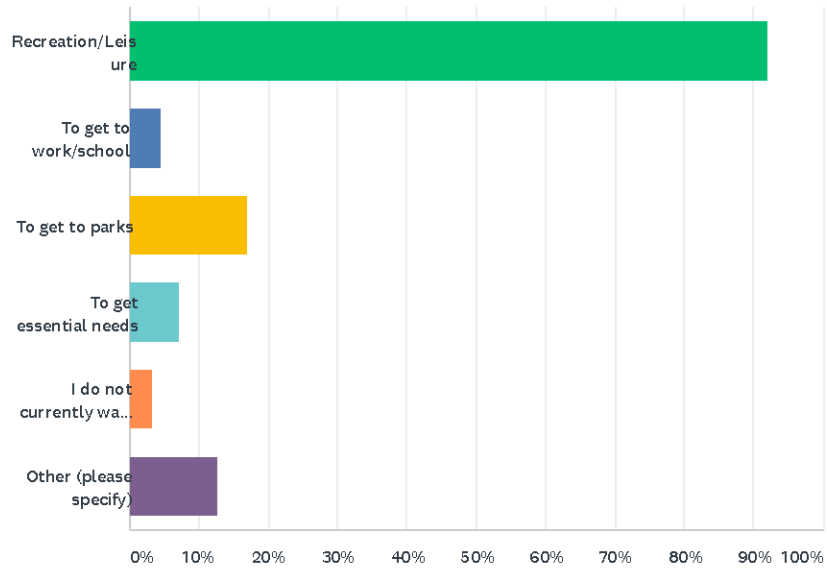


ANSWER CHOICES	RESPONSES	
Female	73.62%	187
Male	24.02%	61
Prefer not to answer	2.36%	6
TOTAL		254

McMinnville's Bicycle and Pedestrian Plans

Q4 What are your reasons for walking and bicycling? (Select all that apply)

Answered: 242 Skipped: 16



ANSWER CHOICES	RESPONSES
Recreation/Leisure	92.15% 223
To get to work/school	4.55% 11
To get to parks	16.94% 41
To get essential needs	7.02% 17
I do not currently walk or bicycle	3.31% 8
Other (please specify)	12.81% 31
Total Respondents: 242	

#	OTHER (PLEASE SPECIFY)	DATE
1	Exercise	3/25/2021 4:18 PM
2	Walk for physical activity and exercise	3/25/2021 10:59 AM
3	Exercise	3/25/2021 12:02 AM
4	Walking the dog	3/24/2021 9:06 AM
5	Exercise and better Health	3/24/2021 7:18 AM
6	Maintaining fitness	3/24/2021 5:49 AM
7	Health reasons	3/23/2021 11:41 AM
8	exercise	3/23/2021 10:24 AM
9	exercise	3/23/2021 7:27 AM
10	Fitness	3/22/2021 8:32 PM

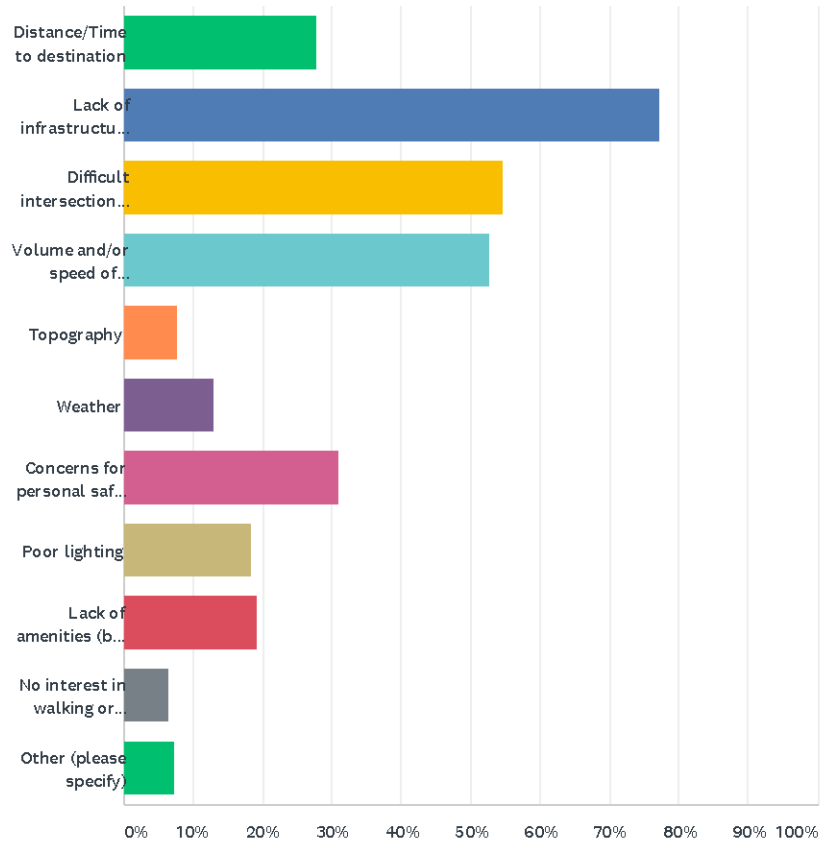
### McMinnville's Bicycle and Pedestrian Plans

11	Health	3/22/2021 5:25 PM
12	Pet exercise	3/22/2021 5:21 PM
13	Exercise	3/22/2021 5:04 PM
14	Exercise	3/22/2021 3:27 PM
15	Have kids that wants to ride a bike	3/22/2021 3:03 PM
16	exercise	3/22/2021 2:37 PM
17	Health	3/22/2021 2:05 PM
18	Walking Dogs	3/22/2021 1:42 PM
19	fitness/exercise	3/22/2021 1:22 PM
20	exercise	3/22/2021 1:19 PM
21	Exercise	3/22/2021 1:17 PM
22	exercise	3/22/2021 12:56 PM
23	workout	3/22/2021 12:44 PM
24	Exercise	3/19/2021 8:51 AM
25	Exercise/doctor's orders	3/18/2021 8:33 PM
26	Exercise	3/17/2021 5:48 PM
27	Tax money better used elsewhere	3/17/2021 7:53 AM
28	Exercise	3/16/2021 12:27 PM
29	Exercise	3/16/2021 11:06 AM
30	Physical Fitness	3/16/2021 11:04 AM
31	Exercise for small kids	3/16/2021 8:14 AM

McMinnville's Bicycle and Pedestrian Plans

Q5 Pick your top 3 reasons for NOT walking or bicycling more in McMinnville? (Select up to 3)

Answered: 245 Skipped: 13



### McMinnville's Bicycle and Pedestrian Plans

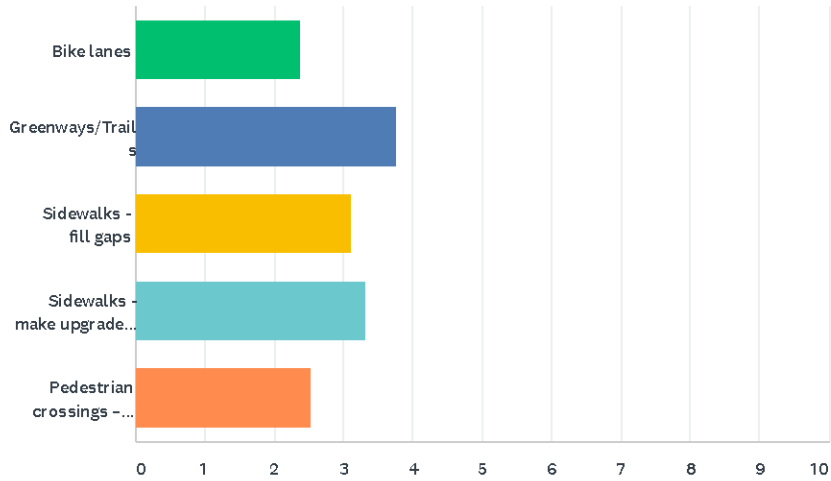
ANSWER CHOICES	RESPONSES	
Distance/Time to destination	27.76%	68
Lack of infrastructure (sidewalks, bike lanes, etc.)	77.14%	189
Difficult intersections/Crossings	54.69%	134
Volume and/or speed of traffic	52.65%	129
Topography	7.76%	19
Weather	13.06%	32
Concerns for personal safety (beyond traffic)	31.02%	76
Poor lighting	18.37%	45
Lack of amenities (bike racks, benches, water fountains, etc.)	19.18%	47
No interest in walking or bicycling	6.53%	16
Other (please specify)	7.35%	18
Total Respondents: 245		

#	OTHER (PLEASE SPECIFY)	DATE
1	Trash	4/7/2021 2:34 PM
2	I walk and bike quite a bit, and I have been very frustrated by the number of loose, aggressive dogs, both on city streets and more rural roads.	3/27/2021 6:01 PM
3	Need trails away from traffic	3/25/2021 4:18 PM
4	Lack of infrastructure in my area.	3/24/2021 8:10 AM
5	The downtown area wants riders on the road and not on the sidewalk. This is dangerous for adults and children.	3/24/2021 7:43 AM
6	Speeders and people who disregard traffic laws/ unsafe people moving to McMinnville	3/23/2021 6:34 PM
7	I think our money should be spent in better areas	3/22/2021 9:05 PM
8	There plenty of places to walk or ride a bicycle in McMinnville. How bout we get out of debt be4 we get into more w bicycle lanes that I very rarely see used . Waiste of tac payer money	3/22/2021 5:25 PM
9	No interest in McMinnville	3/22/2021 1:29 PM
10	Dogs off leash	3/22/2021 1:08 PM
11	I love in the Irving college community	3/22/2021 12:57 PM
12	Drivers	3/22/2021 12:50 PM
13	this is a distraction to me as a driver	3/22/2021 12:46 PM
14	Concerns for personal safety - dogs	3/22/2021 12:42 PM
15	Traffic lights are not tripped by bicycle. Makes it difficult to cross major intersections such as hwy 70	3/17/2021 9:09 PM
16	Taxes will go up	3/17/2021 7:53 AM
17	Lack of sidewalks	3/16/2021 7:56 PM
18	Lack of locations	3/16/2021 3:59 PM

McMinnville's Bicycle and Pedestrian Plans

Q6 What types of walking and bicycling infrastructure would you like to see more of? (Rank in order of priority)

Answered: 236 Skipped: 22

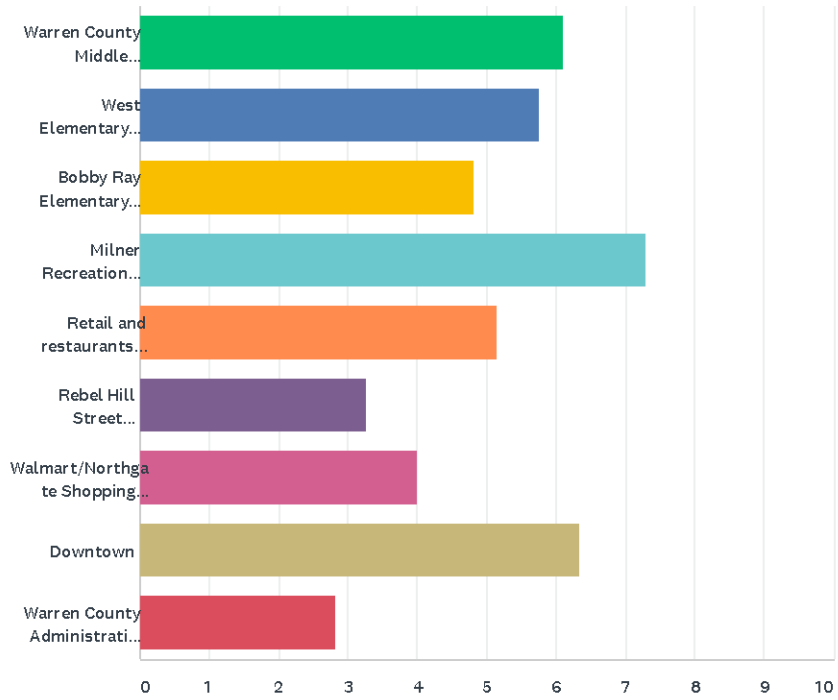


	1	2	3	4	5	TOTAL	SCORE
Bike lanes	14.61% 32	10.96% 24	14.61% 32	18.72% 41	41.10% 90	219	2.39
Greenways/Trails	46.46% 105	16.37% 37	14.16% 32	14.16% 32	8.85% 20	226	3.77
Sidewalks - fill gaps	17.33% 39	24.89% 56	24.89% 56	17.33% 39	15.56% 35	225	3.11
Sidewalks - make upgrades (widen, remove obstacles, etc.)	15.22% 35	34.78% 80	26.09% 60	15.22% 35	8.70% 20	230	3.33
Pedestrian crossings – signals and crosswalks	8.41% 19	14.60% 33	22.12% 50	31.42% 71	23.45% 53	226	2.53

McMinnville's Bicycle and Pedestrian Plans

Q7 Which of the following are the most important walking and bicycling destinations in and around the study area? (Please rank)

Answered: 190 Skipped: 68



McMinnville's Bicycle and Pedestrian Plans

	1	2	3	4	5	6	7	8	9	TOTAL	SCORE
Warren County Middle School/Nunley Stadium	15.82% 28	14.69% 26	17.51% 31	16.95% 30	11.86% 21	9.04% 16	5.08% 9	5.65% 10	3.39% 6	177	6.10
West Elementary School	7.78% 14	20.56% 37	17.22% 31	16.11% 29	8.89% 16	9.44% 17	7.78% 14	6.11% 11	6.11% 11	180	5.76
Bobby Ray Elementary School	1.71% 3	5.14% 9	12.57% 22	17.71% 31	20.57% 36	14.86% 26	15.43% 27	9.71% 17	2.29% 4	175	4.81
Milner Recreation Center/Dottie West Park (pool, ballfields, skatepark, walking trail, etc.)	39.67% 73	22.28% 41	11.96% 22	9.24% 17	4.89% 9	4.35% 8	2.17% 4	1.63% 3	3.80% 7	184	7.30
Retail and restaurants along N. Chancery Street	6.21% 11	10.73% 19	12.99% 23	12.43% 22	16.38% 29	19.21% 34	8.47% 15	9.04% 16	4.52% 8	177	5.14
Rebel Hill Street Community Center (McMinnville Housing Authority)	0.57% 1	1.71% 3	4.57% 8	6.29% 11	9.71% 17	18.86% 33	18.86% 33	17.14% 30	22.29% 39	175	3.26
Walmart/Northgate Shopping Center	6.82% 12	6.25% 11	5.68% 10	6.82% 12	10.80% 19	7.39% 13	22.16% 39	22.73% 40	11.36% 20	176	3.96
Downtown	24.59% 45	18.03% 33	15.85% 29	8.74% 16	8.74% 16	4.37% 8	6.01% 11	12.57% 23	1.09% 2	183	6.34
Warren County Administration Building	0.57% 1	3.98% 7	3.98% 7	5.68% 10	6.82% 12	10.80% 19	11.93% 21	13.64% 24	42.61% 75	176	2.82

McMinnville's Bicycle and Pedestrian Plans

Q8 Name the top 3 streets in the study area that you think are most in need of improvements (i.e., need sidewalks, maintenance of existing sidewalks, bike lanes, etc.).

Answered: 105 Skipped: 153

ANSWER CHOICES	RESPONSES	
1.	100.00%	105
2.	84.76%	89
3.	78.10%	82

#	1.	DATE
1	Caldwell Street	4/11/2021 10:21 AM
2	Chancery St.	4/9/2021 11:33 AM
3	Durham Street	4/7/2021 2:36 PM
4	Rebel hill	4/6/2021 5:06 PM
5	Walmart	3/31/2021 12:45 PM
6	Morford Street	3/29/2021 11:14 AM
7	Chancery	3/29/2021 11:13 AM
8	West Main St	3/29/2021 10:00 AM
9	Clark Blvd	3/28/2021 12:29 PM
10	Yager	3/27/2021 6:56 PM
11	Garfield	3/27/2021 6:30 PM
12	North chancery	3/26/2021 7:09 PM
13	Caldwell	3/26/2021 2:55 PM
14	near parks	3/25/2021 11:21 AM
15	N. Chancery Street	3/25/2021 11:06 AM
16	N Chancery	3/25/2021 8:19 AM
17	I'm not	3/25/2021 12:04 AM
18	All around Milner Center	3/24/2021 2:01 PM
19	Morford st.	3/24/2021 9:45 AM
20	Caldwell St	3/24/2021 9:14 AM
21	Caldwell Street	3/24/2021 8:57 AM
22	Garfield from the Cleaners to Civic Center	3/24/2021 8:16 AM
23	Walmart/Northgate shopping Center	3/24/2021 7:23 AM
24	Morrison St	3/24/2021 5:52 AM
25	Highway 70 from Walmart and strip.	3/24/2021 5:03 AM
26	Rebel Hill	3/24/2021 12:30 AM
27	West End	3/23/2021 3:13 PM

### McMinnville's Bicycle and Pedestrian Plans

28	North Chancery	3/23/2021 12:08 PM
29	West Main	3/23/2021 10:27 AM
30	Milner rec.	3/23/2021 10:14 AM
31	Garfield	3/23/2021 8:49 AM
32	Walmart	3/23/2021 8:10 AM
33	Vervilla Road	3/23/2021 8:05 AM
34	Smithville hwy n chancery	3/23/2021 8:04 AM
35	Don't know.	3/23/2021 7:40 AM
36	Caldwell St	3/23/2021 7:38 AM
37	N. Chancery St	3/23/2021 7:34 AM
38	North Chancery St @ Smithville Hwy	3/23/2021 12:02 AM
39	Spring St.	3/22/2021 9:39 PM
40	Caldwell St	3/22/2021 9:26 PM
41	West main	3/22/2021 8:36 PM
42	Main St	3/22/2021 8:22 PM
43	Chancery	3/22/2021 7:55 PM
44	Main Street	3/22/2021 7:30 PM
45	North Chancery	3/22/2021 6:42 PM
46	Clark blvd	3/22/2021 6:23 PM
47	None	3/22/2021 5:26 PM
48	WCMS	3/22/2021 5:25 PM
49	West Main Street	3/22/2021 4:26 PM
50	Unknown	3/22/2021 3:30 PM
51	Centertown community center	3/22/2021 3:05 PM
52	N Chancery (between National Carwash and Walmart)	3/22/2021 2:12 PM
53	N. Chancery St.	3/22/2021 2:11 PM
54	Chancery	3/22/2021 2:09 PM
55	Main Street	3/22/2021 2:01 PM
56	Walmart/ Northgate	3/22/2021 2:01 PM
57	N. Chancery St.	3/22/2021 2:01 PM
58	Caldwell St	3/22/2021 1:58 PM
59	Caldwell	3/22/2021 1:57 PM
60	Colville St	3/22/2021 1:56 PM
61	North chancery	3/22/2021 1:25 PM
62	WCMS	3/22/2021 1:25 PM
63	Vervilla Road	3/22/2021 1:25 PM
64	Chancery	3/22/2021 1:24 PM
65	caldwell st	3/22/2021 1:23 PM
66	N. Chancery St	3/22/2021 1:23 PM
67	Caldwell St.	3/22/2021 1:20 PM

### McMinnville's Bicycle and Pedestrian Plans

68	Sidewalk	3/22/2021 1:14 PM
69	caldwell st	3/22/2021 1:14 PM
70	North Spring Street	3/22/2021 1:10 PM
71	Chancery	3/22/2021 1:09 PM
72	Durham Street	3/22/2021 1:07 PM
73	North Spring St.	3/22/2021 1:06 PM
74	Garfield	3/22/2021 1:02 PM
75	Caldwell street	3/22/2021 1:01 PM
76	Caldwell Street	3/22/2021 1:00 PM
77	North Chancery Street	3/22/2021 12:59 PM
78	North Chancery Street	3/22/2021 12:57 PM
79	Walmart	3/22/2021 12:55 PM
80	North Spring Street	3/22/2021 12:54 PM
81	Walmart/Northgate Shopping Center	3/22/2021 12:52 PM
82	Spring St	3/22/2021 12:51 PM
83	Locust ST.	3/22/2021 12:49 PM
84	Chancery	3/22/2021 12:48 PM
85	N. Chancery Street	3/22/2021 12:47 PM
86	West Main Street	3/22/2021 12:46 PM
87	Sparta Street	3/22/2021 12:44 PM
88	Mullican St	3/22/2021 12:43 PM
89	All of the commercial areas should be accessible by walking or bicycle.	3/20/2021 7:58 AM
90	Main St	3/19/2021 3:07 PM
91	West Main, sidewalk repair, obstacles, bike lane	3/19/2021 12:29 PM
92	Main Street	3/19/2021 10:35 AM
93	N. Chancery & Clark Blvd needs traffic light or Turn lane	3/18/2021 8:39 PM
94	Caldwell	3/17/2021 5:57 PM
95	Existing sidewalks need repair	3/17/2021 7:55 AM
96	N chancery	3/17/2021 6:22 AM
97	Downtown	3/16/2021 8:08 PM
98	Crossing N chancery	3/16/2021 7:59 PM
99	Garfield Street	3/16/2021 4:03 PM
100	Chancery	3/16/2021 12:29 PM
101	Chancery	3/16/2021 10:39 AM
102	Walmart	3/16/2021 9:52 AM
103	Sunset drive	3/16/2021 8:41 AM
104	N. Chancery	3/16/2021 8:27 AM
105	W Main Street	3/16/2021 8:17 AM
#	2.	DATE

### McMinnville's Bicycle and Pedestrian Plans

1	Rebel Hill Street	4/11/2021 10:21 AM
2	The Bypass	4/9/2021 11:33 AM
3	Durham Street	4/7/2021 2:36 PM
4	N.Chancery	4/6/2021 5:06 PM
5	Rebel hill	3/31/2021 12:45 PM
6	Rebel Hill	3/29/2021 11:14 AM
7	Main St	3/29/2021 11:13 AM
8	High St	3/29/2021 10:00 AM
9	Chancery	3/28/2021 12:29 PM
10	Old smithville rd	3/27/2021 6:56 PM
11	Locust	3/27/2021 6:30 PM
12	Spring street	3/26/2021 7:09 PM
13	Locust	3/26/2021 2:55 PM
14	Near City	3/25/2021 11:21 AM
15	Rebel Hill	3/25/2021 8:19 AM
16	Good with	3/25/2021 12:04 AM
17	North Chancery	3/24/2021 2:01 PM
18	Sparta st.	3/24/2021 9:45 AM
19	Sparta St	3/24/2021 9:14 AM
20	Garfield Street	3/24/2021 8:57 AM
21	Street to the left of Bobby Ray	3/24/2021 8:16 AM
22	Rebel Hill Street Community Center	3/24/2021 7:23 AM
23	Morford	3/24/2021 5:52 AM
24	Sparta street kroger area	3/24/2021 5:03 AM
25	Milner Recreation	3/24/2021 12:30 AM
26	Locust	3/23/2021 3:13 PM
27	Caldwell St	3/23/2021 12:08 PM
28	N Chancery	3/23/2021 10:14 AM
29	N. Chancery	3/23/2021 8:49 AM
30	Rebel Hill	3/23/2021 8:10 AM
31	Downtown morford	3/23/2021 8:04 AM
32	Don't know.	3/23/2021 7:40 AM
33	North Chancery St	3/23/2021 7:38 AM
34	Smithville Highway	3/23/2021 7:34 AM
35	North Chancery St @ Caldwell	3/23/2021 12:02 AM
36	Red Rd.	3/22/2021 9:39 PM
37	Towels Ave	3/22/2021 9:26 PM
38	N. Chancery	3/22/2021 7:30 PM
39	Garfield	3/22/2021 6:42 PM
40	West Main	3/22/2021 6:23 PM

### McMinnville's Bicycle and Pedestrian Plans

41	None	3/22/2021 5:26 PM
42	Walmart	3/22/2021 5:25 PM
43	N. Chancery Street	3/22/2021 4:26 PM
44	Unknown	3/22/2021 3:30 PM
45	Post Rd	3/22/2021 2:12 PM
46	Towels Ave.	3/22/2021 2:11 PM
47	Calwell	3/22/2021 2:09 PM
48	Rebel Hill	3/22/2021 2:01 PM
49	Retail along N. Chancery	3/22/2021 2:01 PM
50	Smithville Hwy	3/22/2021 2:01 PM
51	Garfield St	3/22/2021 1:58 PM
52	Garfield	3/22/2021 1:57 PM
53	South High St	3/22/2021 1:56 PM
54	Smithville highway	3/22/2021 1:25 PM
55	Walmart area	3/22/2021 1:25 PM
56	Sunset Drive	3/22/2021 1:25 PM
57	Clark	3/22/2021 1:24 PM
58	rebel hill	3/22/2021 1:23 PM
59	Caldwell Street	3/22/2021 1:23 PM
60	Towles Ave.	3/22/2021 1:20 PM
61	Rebel Hill	3/22/2021 1:10 PM
62	Caldwell St.	3/22/2021 1:06 PM
63	South High	3/22/2021 1:02 PM
64	Morford street	3/22/2021 1:01 PM
65	North chancery Street	3/22/2021 1:00 PM
66	Walmart/Northgate Shopping Center	3/22/2021 12:59 PM
67	Rebel Hill Street	3/22/2021 12:57 PM
68	Retail and restaurants	3/22/2021 12:55 PM
69	North Chancery Street	3/22/2021 12:54 PM
70	Warren County Middle School/Nunley Stadium	3/22/2021 12:52 PM
71	Rd next to WalMart, Maybe N. Chancery?	3/22/2021 12:51 PM
72	N Spring St.	3/22/2021 12:49 PM
73	Caldwell St.	3/22/2021 12:47 PM
74	North Chancery	3/22/2021 12:46 PM
75	intersection New Smithville Hwy	3/22/2021 12:44 PM
76	Spring St	3/22/2021 12:43 PM
77	Post Rd	3/19/2021 3:07 PM
78	Morford St. , sidewalks, bike lane	3/19/2021 12:29 PM
79	Post Road	3/19/2021 10:35 AM

McMinnville's Bicycle and Pedestrian Plans

80	Caldwell	3/18/2021 8:39 PM
81	N. Spring St	3/17/2021 5:57 PM
82	Existing roads need repair	3/17/2021 7:55 AM
83	Sunset drive	3/17/2021 6:22 AM
84	Milner Rec Center	3/16/2021 8:08 PM
85	North Chancery Street	3/16/2021 4:03 PM
86	Milner	3/16/2021 9:52 AM
87	Club drive	3/16/2021 8:41 AM
88	Caldwell	3/16/2021 8:27 AM
89	Clark Blvd/West Elementary	3/16/2021 8:17 AM
#	<b>3.</b>	<b>DATE</b>
1	N. Chancery Street	4/11/2021 10:21 AM
2	Morford St.	4/9/2021 11:33 AM
3	Sparta Street	4/7/2021 2:36 PM
4	Sparta St.	4/6/2021 5:06 PM
5	Retail N chancery St	3/31/2021 12:45 PM
6	Colville	3/29/2021 11:13 AM
7	Depot St	3/29/2021 10:00 AM
8	Caldwell	3/28/2021 12:29 PM
9	Bybee branch rd	3/27/2021 6:56 PM
10	N Chancery St.	3/27/2021 6:30 PM
11	West Main Street	3/26/2021 7:09 PM
12	Chancery	3/26/2021 2:55 PM
13	jjkjddj	3/25/2021 8:19 AM
14	Street names	3/25/2021 12:04 AM
15	Rebel Hill	3/24/2021 2:01 PM
16	North chancery street	3/24/2021 9:45 AM
17	Garfield St	3/24/2021 9:14 AM
18	Rebel Hill Street	3/24/2021 8:57 AM
19	Rebel Hill around middle school	3/24/2021 8:16 AM
20	Warren County Administration Buidling	3/24/2021 7:23 AM
21	S chancery	3/24/2021 5:52 AM
22	Projects to downtown	3/24/2021 5:03 AM
23	Retail n Resturrants on N.Chancery St.	3/24/2021 12:30 AM
24	Caldwell	3/23/2021 3:13 PM
25	Garfield St	3/23/2021 12:08 PM
26	Northgate	3/23/2021 10:14 AM
27	Caldwell	3/23/2021 8:49 AM
28	Bobby Ray	3/23/2021 8:10 AM
29	Don't know	3/23/2021 7:40 AM

### McMinnville's Bicycle and Pedestrian Plans

30	Locust St	3/23/2021 7:38 AM
31	Locust Street	3/23/2021 7:34 AM
32	North Chancery St @ Locust	3/23/2021 12:02 AM
33	N. Chancery St.	3/22/2021 9:39 PM
34	Rebel Hill	3/22/2021 7:30 PM
35	Caldwell	3/22/2021 6:42 PM
36	Club drive	3/22/2021 6:23 PM
37	None	3/22/2021 5:26 PM
38	Milner Rec Center	3/22/2021 5:25 PM
39	Sunset Drive	3/22/2021 4:26 PM
40	Unknown	3/22/2021 3:30 PM
41	W Main	3/22/2021 2:12 PM
42	Caldwell St.	3/22/2021 2:11 PM
43	Main	3/22/2021 2:09 PM
44	Garfield	3/22/2021 2:01 PM
45	WCMS	3/22/2021 2:01 PM
46	Locust St	3/22/2021 2:01 PM
47	Locust St	3/22/2021 1:58 PM
48	North Spring	3/22/2021 1:57 PM
49	Locust street	3/22/2021 1:25 PM
50	N Chancery	3/22/2021 1:25 PM
51	North Chancery	3/22/2021 1:25 PM
52	Club	3/22/2021 1:24 PM
53	Spring Street	3/22/2021 1:23 PM
54	Garfield St.	3/22/2021 1:20 PM
55	Bobby Ray School	3/22/2021 1:10 PM
56	Garfield St.	3/22/2021 1:06 PM
57	East Colville	3/22/2021 1:02 PM
58	Sparta street	3/22/2021 1:01 PM
59	Locust	3/22/2021 1:00 PM
60	Warren County Middle School	3/22/2021 12:59 PM
61	Walmart/Northgate Shopping Center area	3/22/2021 12:57 PM
62	Warren co administration building	3/22/2021 12:55 PM
63	Locust street	3/22/2021 12:54 PM
64	Retail and restaurants along N. Chancery Street	3/22/2021 12:52 PM
65	Area where residents of Arms have to cross the road to walk to Walmart	3/22/2021 12:51 PM
66	Garfield St.	3/22/2021 12:47 PM
67	Garfield Street	3/22/2021 12:46 PM
68	intersection @ Walmart	3/22/2021 12:44 PM

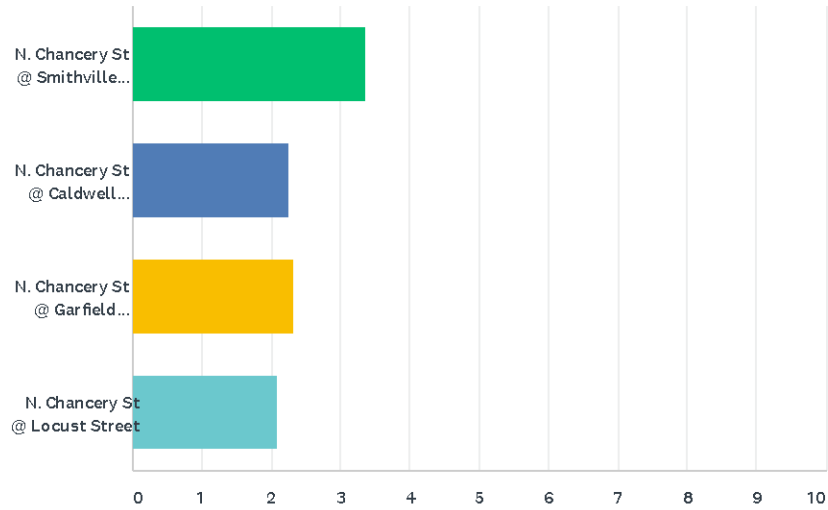
### McMinnville's Bicycle and Pedestrian Plans

69	Caldwell St	3/22/2021 12:43 PM
70	W Lawn St.	3/19/2021 3:07 PM
71	N. Chancery , bike lanes	3/19/2021 12:29 PM
72	South High Street	3/19/2021 10:35 AM
73	N. spring St	3/18/2021 8:39 PM
74	N Chancery	3/17/2021 5:57 PM
75	Police presence to reduce speeders	3/17/2021 7:55 AM
76	N spring street	3/17/2021 6:22 AM
77	Chancery St	3/16/2021 8:08 PM
78	Locust Street	3/16/2021 4:03 PM
79	Downton	3/16/2021 9:52 AM
80	West end	3/16/2021 8:41 AM
81	Garfield	3/16/2021 8:27 AM
82	N Chancery	3/16/2021 8:17 AM

McMinnville's Bicycle and Pedestrian Plans

Q9 Rank the following intersections in terms of their need for a safer crossing across N. Chancery Street.

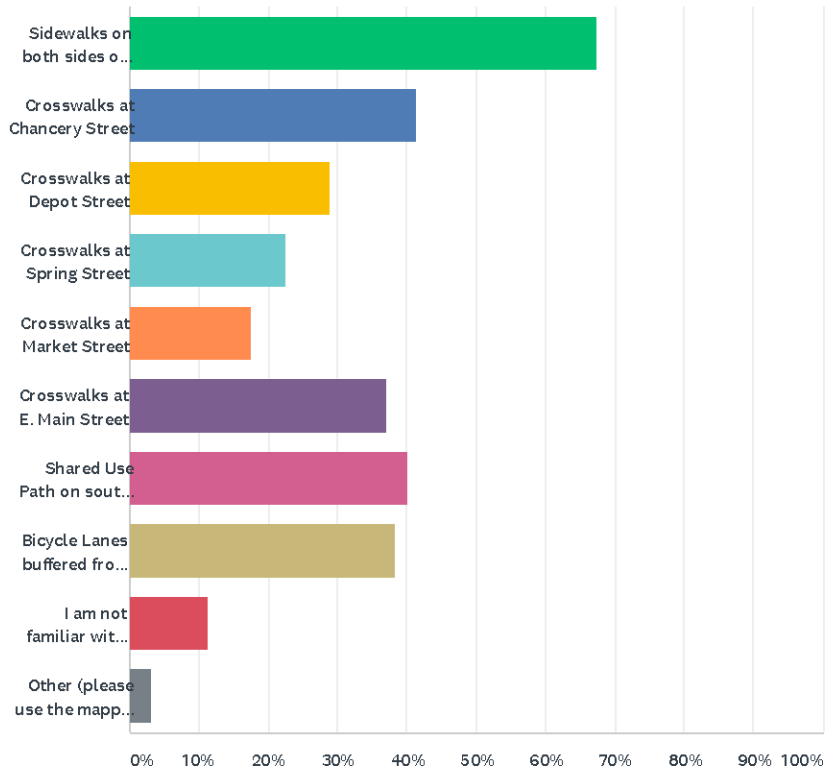
Answered: 163 Skipped: 95



	1	2	3	4	TOTAL	SCORE
N. Chancery St @ Smithville Highway/N. Chancery Street	62.35% 101	19.75% 32	9.26% 15	8.64% 14	162	3.36
N. Chancery St @ Caldwell St/Seitz Street	13.29% 21	28.48% 45	27.85% 44	30.38% 48	158	2.25
N. Chancery St @ Garfield Street	11.54% 18	28.21% 44	42.31% 66	17.95% 28	156	2.33
N. Chancery St @ Locust Street	14.10% 22	23.72% 37	19.87% 31	42.31% 66	156	2.10

Q10 If you could add pedestrian- and bicycle-friendly elements to Colville Street (between S. High Street and E. Main Street), what would they be? (Select all that apply)

Answered: 159 Skipped: 99



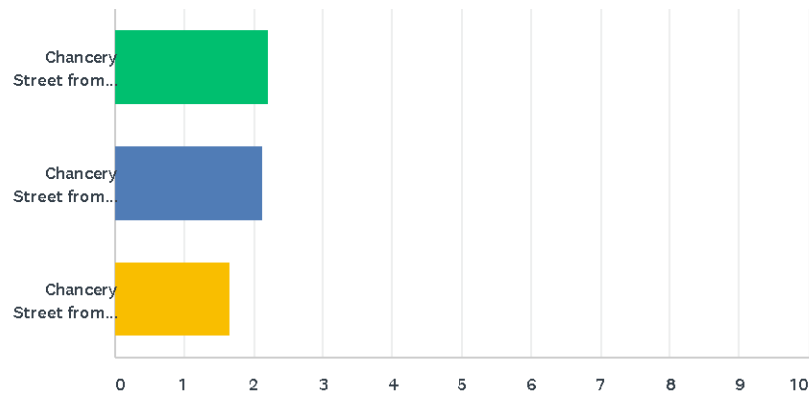
### McMinnville's Bicycle and Pedestrian Plans

ANSWER CHOICES	RESPONSES	
Sidewalks on both sides of the road	67.30%	107
Crosswalks at Chancery Street	41.51%	66
Crosswalks at Depot Street	28.93%	46
Crosswalks at Spring Street	22.64%	36
Crosswalks at Market Street	17.61%	28
Crosswalks at E. Main Street	37.11%	59
Shared Use Path on south side of Colville Street where there are currently no sidewalks	40.25%	64
Bicycle Lanes buffered from the travel lane	38.36%	61
I am not familiar with this location	11.32%	18
Other (please use the mapping survey to identify)	3.14%	5
Total Respondents: 159		

McMinnville's Bicycle and Pedestrian Plans

Q11 Rank which facility you would prefer to use to get from Pepperbranch Park to downtown, assuming they all had bicycle and pedestrian facilities separated from traffic (differentiators may be volume/speed of traffic, distance, topography, etc.). Please skip this question if you are not familiar with this location.

Answered: 131 Skipped: 127

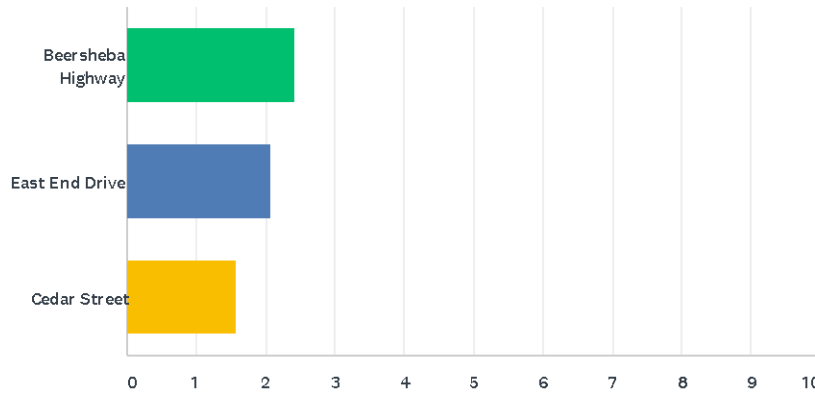


	1	2	3	TOTAL	SCORE
Chancery Street from Pepperbranch Park to Main Street	46.46% 59	29.13% 37	24.41% 31	127	2.22
Chancery Street from Pepperbranch Park to S High Street to Main Street	33.59% 43	46.88% 60	19.53% 25	128	2.14
Chancery Street from Pepperbranch Park to Bridge Street to Depot Street to Main Street	21.09% 27	22.66% 29	56.25% 72	128	1.65

McMinnville's Bicycle and Pedestrian Plans

Q12 Rank which facility you would prefer to use to get from Rocket Park to Colville Street, assuming they all had bicycle and pedestrian facilities separated from traffic (differentiators may be volume/speed of traffic, distance, topography, etc.). Please skip this question if you are not familiar with this location.

Answered: 110 Skipped: 148



	1	2	3	TOTAL	SCORE
Beersheba Highway	62.04% 67	17.59% 19	20.37% 22	108	2.42
East End Drive	27.18% 28	52.43% 54	20.39% 21	103	2.07
Cedar Street	13.73% 14	28.43% 29	57.84% 59	102	1.56

## WIKIMAP RESPONSES

DATE	CATEGORY	INITIAL COMMENT	COMMENT	LIKE	DISLIKE
3/22/21	Dangerous Intersection	No traffic lights or cross walk signals - but there is a cross walk on the pavement.	I Agree	3	1
3/22/21	Dangerous Intersection	No traffic lights or cross walk signals - but there is a cross walk on the pavement.	I Disagree, there is ample sidewalks and walkways that many other parts of the planned areas are denied.	3	1
3/22/21	Dangerous Intersection	No traffic lights or cross walk signals - but there is a cross walk on the pavement.	I Agree	3	1
3/22/21	Dangerous Intersection	No cross walk signal	I Agree	1	0
3/24/21	Dangerous Intersection	There is no crosswalk or signal to help pedestrian.	--	--	--
3/27/21	Dangerous Intersection	This can be a difficult place to cross on foot or bike since there are so many lanes of traffic. A crosswalk would be great!	--	1	0
3/27/21	Dangerous Intersection	There is no crosswalk at the corner of Morford and Sparta. If crossing, you have to look ahead and behind to make sure there are no cars coming, since there is no stopping for either direction of traffic.	--	1	0
3/30/21	Dangerous Intersection	Opportunity for crossing rather than redirecting down to Locust/Chancery	--	--	--
3/30/21	Dangerous Intersection	Seitz to Caldwell- immediate left required for drivers	--	--	--
3/30/21	Dangerous Intersection	Lane widens; no left turn	--	--	--
3/30/21	Dangerous Intersection	Residents crossing to Community Center; people speeding down Spring St and Rebel Hill; frequent congestion during school times	--	--	--
4/12/21	Dangerous Intersection	People trying to cross busy, huge highway intersection with no pedestrian infrastructure.	--	--	--
4/12/21	Dangerous Intersection	Big, busy intersection with no pedestrian or bike infrastructure but people still try to cross	--	--	--
4/12/21	Dangerous Intersection	People on bikes and walking try to cross here but it is an awkward intersection (one way traffic yielding to oncoming one way traffic) and there is no ped/bike infrastructure	--	--	--
4/12/21	Dangerous Intersection	Warren County Sanitation trucks have to back onto N. Chancery Street but there is a bit of a blind hill on N. Chancery Street and it is a busy street. This can cause dangerous conditions. (from Director of Warren County Sanitation)	--	--	--

DATE	CATEGORY	INITIAL COMMENT	COMMENT	LIKE	DISLIKE
4/12/21	Dangerous Intersection	This curve is hard during school start and release. Kids walking to school, lots of cars on the road and the buses also trying to maneuver.	--	--	--
4/12/21	Dangerous Intersection	This intersection has a bit of a blind along N. Spring St and it is after games with people walking and cars trying to turn.	--	--	--
4/12/21	Dangerous Intersection	Very busy intersection with lots of big truck traffic	--	--	--
4/12/21	Dangerous Intersection	This intersection has utility poles which can block the view of pedestrians and riders. Also no signalization.	--	--	--
4/12/21	Dangerous Intersection	W Main and W Morford - people trying to cross to Hardees but no crosswalk/ signal and confusing intersection for cars	--	--	--
4/12/21	Dangerous Intersection	People and bicycles trying to cross here but it is a large, busy intersection and there is not crosswalk	--	--	--
4/12/21	Dangerous Intersection	This is a busy, large intersection and people try to cross on bikes and walking	--	--	--
4/12/21	Dangerous Intersection	Bicycles try to cross here and have to get into the turning lane because so busy	--	--	--
4/12/21	Dangerous Intersection	Widen street and add sidewalk down caldwell to n. chancery st	--	--	--
4/12/21	Dangerous Intersection	There is a bus stop on E Main and folks cross here into the neighborhood. Also individuals from the two housing towers cross here to get on beersheba to access the convenience stores.	--	--	--
3/22/21	Gap in Sidewalk	No sidewalk all down Durham Street except for in front of Smooth Rapids which they had to pay for, no sidewalk to get to waterway, campgrounds, or rocket park through there.	--	--	--
3/22/21	Maintenance Needed	This road is terrible even though the owners have invested alot of their own money to maintain clean up and maintenance to the best of their ability. The road has pot holes, unmaintained clearance on sides of road around curve and no sidewalks for people to walk along to get to new constructions or businesses on that road.	--	--	--
4/12/21	Maintenance Needed	Maintenance needed on road and sidewalks; also blind hill and nothing to help pedestrians cross	--	--	--

DATE	CATEGORY	INITIAL COMMENT	COMMENT	LIKE	DISLIKE
3/22/21	No Wheelchair/ Stroller Access	No sidewalk, sides of road not defined, nor cleaned up properly. Three property owners including smooth rapids are the only persons/owners who help maintain this roadway and protect our waterway with no help from the city.	--	--	--
3/22/21	Other (Please Describe)	Trash is terrible along this waterway which is an eyesore and the city is not making the factory/owner clean it up. Also, the city owns a lot across the street that has not been maintained nor cleaned up, there is branches that fall from the overhanging trees that the surrounding land owners have to clean up themselves in order to keep roadway clear. There is trash coming from that lot across the street into water way on a daily basis. If a sign could be put up to deter littering at a higher fine rate, a sign to slow down speeding on that roadway, and sidewalks placed along this road, it would be a wonderful benefit to the city and this growing area.	--	--	--
3/23/21	Other (Please Describe)	It would be nice for a safe greenway to connect Rocket Park to Durham St.	--	--	--
3/24/21	Other (Please Describe)	Bybee Branch Rd area has some of the highest density population in the city. There are no sidewalks except on Old Smithville Hwy. This road is very dangerous for pedestrians and should be highly considered for a sidewalk plan.	--	--	--
3/27/21	Other (Please Describe)	The raised planting areas along Main Street take up a large portion of the sidewalk area and make it very difficult to walk in groups, or even as two people across. This makes it cumbersome to stroll Main Street with visitors.	--	1	0
3/27/21	Other (Please Describe)	The sidewalk on the bridge connecting Riverfront and Pepper Branch Parks is very narrow and has no real divider between pedestrians and street traffic. Both the road and sidewalk are often quite busy, which makes for a stressful crossing on an otherwise wonderful trail.	--	1	0
3/29/21	Other (Please Describe)	If you are crossing over North Chancery from Cemetery Street it is difficult to cross over to get down to Pepper Branch because of traffic and no way to stop traffic flow.	--	1	0
3/30/21	Other (Please Describe)	Frequent weekend backups from Little Caesars	--	--	--

DATE	CATEGORY	INITIAL COMMENT	COMMENT	LIKE	DISLIKE
3/30/21	Other (Please Describe)	Opportunity for access management; backup for car wash	--	--	--
3/30/21	Other (Please Describe)	Often speeding along Rebel Hill	--	--	--
3/30/21	Other (Please Describe)	Wide road/shoulder. Opportunity for facilities	--	--	--
3/30/21	Other (Please Describe)	Upcoming renovation; potential opportunities for facilities	--	--	--
3/30/21	Other (Please Describe)	Not occupied; currently industrial and future rezoning could change use of lot	--	--	--
3/30/21	Other (Please Describe)	Old storage building will be converted to three offices; potential increase in traffic near intersection	--	--	--
3/30/21	Other (Please Describe)	Upcoming Housing Authority development	--	--	--
4/5/21	Other (Please Describe)	Steep drop off here with no walls or fence to prevent from a small child running and falling down the hillside	--	--	--
4/5/21	Other (Please Describe)	Loose dogs hazard to walkers. Animal control not available during evenings and weekends when people trying to walk.	--	--	--
4/5/21	Other (Please Describe)	Strip street for nose end parking on Caldwell St. by Stadium, would allow more cars to safely park	--	--	--
4/12/21	Other (Please Describe)	Busy intersection for pedestrians walking students/families across to go to West Elementary and the civic center complex. Three roads meet at one spot so complicated	--	--	--
4/12/21	Other (Please Describe)	Sidewalks do not run the full length of the street - only in front of some houses	--	--	--
3/23/21	Priority Bicycling Connection	A bike lane connecting the two areas would fantastic. N. Spring St. seems the most ideal choice, as I would assume N Chancery is too high traffic. Perhaps West End Ave as well?	--	--	--
3/27/21	Priority Bicycling Connection	It would be great to create a safe, route for biking and walking between three parks. And to connect people from east to west in town. This could be accomplished mainly by upgraded sidewalks, bike sharing arrows on the smaller streets, and separated bike lanes on Colville.	--	1	0
3/27/21	Priority Bicycling Connection	I would be nice to have bike lanes to get to the Civic Center from the east side of town.	--	--	--
3/30/21	Priority Bicycling Connection	Biking/Walking Connection from east side to Civic Center; includes steep hills	--	--	--

DATE	CATEGORY	INITIAL COMMENT	COMMENT	LIKE	DISLIKE
4/12/21	Priority Bicycling Connection	No bicycle infrastructure but a good connector to Old Smithville Hwy and Walmart	--	--	--
4/12/21	Priority Bicycling Connection		--	--	--
4/12/21	Priority Bicycling Connection	Connector bike riders	--	--	--
3/22/21	Priority Walking Connection	Inadequate sidewalks	--	--	--
3/22/21	Priority Walking Connection	Sidewalk broken and holes in places.	--	--	--
3/24/21	Priority Walking Connection	Safer sidewalks and bike lanes to make this walking connection better.	--	--	--
3/24/21	Priority Walking Connection	This area needs existing sidewalks repaired. They are in bad shape.	--	--	--
3/27/21	Priority Walking Connection	It would be great to connect the greenway that runs from Riverfront Park to Pepper Branch Park to the Bigby Trail. Expanding the greenway would create more safe, off-road paths for people to enjoy. Both locals and tourists could take advantage of this.	--	--	--
3/30/21	Priority Walking Connection	Future opportunity for connection to park	--	--	--
3/30/21	Priority Walking Connection	Potentially future residential development	--	--	--
3/30/21	Priority Walking Connection	previously residential area is now commercial; more commuter pedestrians that is non-recreational	--	--	--
4/12/21	Priority Walking Connection	Lots of people walking along this street and people also walk in the cemetery	--	--	--
4/12/21	Priority Walking Connection	No sidewalks on this street but people walk down it alot	--	--	--
4/12/21	Priority Walking Connection	No sidewalk but people walk down this road a lot	--	--	--
4/12/21	Priority Walking Connection	Route parents use to walk kids to West Elementary School	--	--	--
4/12/21	Priority Walking Connection	People frequently walk this route to access the convenience stores	--	--	--
3/22/21	Traffic Too Fast/ Too Heavy	People fly down this hill and its very dangerous for persons walking from parking lot to restaurant. No sidewalk, no walkway or bikeway.	--	--	--

# PUBLIC WORKSHOP #2

## PRESENTATION

Safe Streets to School  
Pedestrian + Bicycle Plan



Public Workshop #2  
May 24<sup>th</sup>, 2021




### Agenda

- Presentation
  - Plan Framework
  - Public Engagement Round I
  - Draft Recommendations
  - Next Steps
- Interactive Stations
  - Vision Networks
  - Recommendations for Further Study

## Plan Framework

McMinnville's  
Safe Streets to  
School  
Pedestrian +  
Bicycle Plan



### 20-Year Vision

- Blueprint for Desired Network
- Implementation Over Time  
→ *Opportunity Driven*
- "Living" Document

### VISION:



McMinnville's Inner City Public Schools and Neighborhoods are Seamlessly Connected Through a Network of Safe and Comfortable Streets, Walkways, and Bikeways That Provide Affordable, Healthy Transportation and Recreation Options.

**GOALS:**

Connected + Accessible	Safe + Comfortable	Active + Healthy	Cost Effective
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## Public Engagement Round I

### Round I

258 Survey Responses

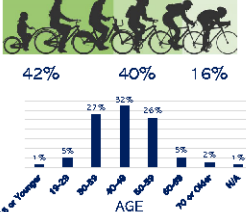


35 Wikimap Responses  
(In Study Area)



### Round I

**CONFIDENCE OR COMFORTABILITY**



Age Group	Percentage
18 or Younger	1%
19-29	5%
30-39	27%
40-49	40%
50-59	26%
60-69	5%
70 or Older	2%
NA	1%

**TOP 3 DESTINATIONS RANKED #1 (MOST IMPORTANT)**

- 1) Milner Rec Center/Dottie West Park
- 2) Downtown
- 3) Warren County Middle/Nunley Stadium

**TOP 3 BARRIERS**

- 1) Lack of Infrastructure
- 2) Difficult Intersections/Crossings
- 3) Volume and/or Speed of Traffic

**TOP 3 STREETS**

- 1) North Chanery Street
- 2) Caldwell Street
- 3) Garfield Street



Shared Street



Shared Street



Shared Street



Neighborhood Greenway



Mid-Block Crossing



Mid-Block Crossing



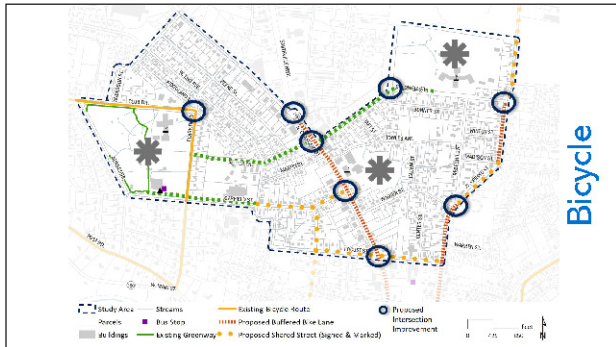
Intersections



### Shared Street (Signed & Marked)



### Buffered Bike Lanes



### Streetscape with Shared Use Path



### Road Diet



Before

After

### Road Diet



Next Steps

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### Next Steps



- Refine Recommendations Based on Input – May
- Draft Document – June
- Final Document and Project End – July

# Stations

## We Want to Hear from You!

**Station 1: Draft Vision Networks**

- Interactive Exercise – Connection Priorities

**Station 2: Recommendations for Additional Study**

- Interactive Exercise – North Chancery Street Vision

## PRESENTATION BOARDS

# TRAFFIC CALMING

**What is traffic calming?**

Traffic calming is a roadway design approach used to improve safety for all roadway users, primarily by reducing vehicle speeds.

There are dozens of traffic calming approaches to choose from, depending on the context and issue to be addressed.

**Traffic calming measures include:**

- Speed cushions
- Bulb-outs
- Raised crosswalks
- Traffic circles
- On-street parking
- Medians
- Chicanes, and more

**Examples:**

**Speed Cushions** reduce vehicle speeds.  
Source: NACTO

**Raised Crosswalks** reduce speeds and increase pedestrian visibility.  
Source: FHWA

**Bulb Outs** reduce crossing distances and increase pedestrian visibility.  
Source: spcr.org

**Traffic Circles** reduce speeds at uncontrolled intersections.  
Source: Seattle DOT

**Where could it be implemented?**

Traffic calming measures can be applied to a variety of streets to reduce vehicle speeds and increase safety. Walker Ave., Mountain St., Fair St., and Rebel Hill St. (pictured below), could all be considered for traffic calming installations.

**What are the benefits?**

Improve safety by reducing vehicle speeds and increasing roadway user visibility.

Improve mobility for people walking and biking.

Reduce conflicts between vehicles and pedestrians.

**Next Steps**

1. Evaluate conditions → Document roadway deficiencies and analyze improvement options
2. Develop concepts → Identify preferred traffic calming locations
3. Document recommend improvements

# STREETSCAPE WITH SIDEPATH

**What are streetscapes?**

The term streetscape refers to the design elements that affect how a roadway appears and operates. Streetscape design can influence a community's identity and aesthetic quality.

**What types of streetscape elements exist?**

Changes to a road's streetscape can include:

- installing medians to improve access management
- creating landscaped buffers to create shade and separate sidewalks from the roadway
- creating sidepaths (located within the street's right-of-way) to provide a dedicated, low-stress space for people walking, biking, and rolling

**Examples:**

Source: City of Shoreline, WA

Source: Town of Chapel Hill, TN

Source: City of Tullahoma, TN

Source: City of Lexington, KY

**Where could it be implemented?**

North Chancery Street has a five-lane cross-section between East Morford Street and Fairgrounds Road, including a turn lane and two travel lanes in each direction. Streetscaping changes along this corridor could include a shared use path or sidepath, the installation of medians, and landscaping.

**What are the benefits?**

Improve sense of place and the user experience

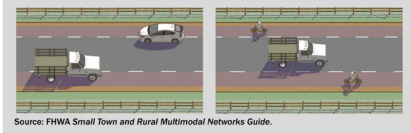
Improve mobility by providing dedicated space for people walking, biking, and rolling.

Reduce conflicts between vehicles and pedestrians; improves access management.

# EDGE LANE ROADS

## What are Edge Lane Roads?

An edge lane road (ELR) includes a single bi-directional center lane and two striped lanes on the outside edges that provide space for people walking, biking and rolling.



Source: FHWA Small Town and Rural Multimodal Networks Guide.

## How do passing movements work?

Vehicles traveling in opposite directions yield to edge lane users and one another, merge into the edge lane when safe, and pass one another before returning to the center lane.

## Where could ELRs be implemented?

Caldwell Street links North Chancery Street to Warren County Middle School. An ELR treatment would create dedicated bicycle and pedestrian facilities for students and the surrounding community. ELRs are typically installed on roadways with average daily traffic (ADT) less than 5,000 vehicles and where speed limits are posted at 25 miles per hour or less.



## What are the benefits?



Provides dedicated space for people walking, biking, and rolling.



Improves safety by reducing vehicle speeds and crash rates.



Low installation costs; does not require roadway widening.



Little to no change in street maintenance needs.

# ROAD DIETS

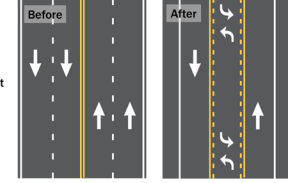
## What are Road Diets?

Road diets reduce the number of travel lanes on a roadway to:

- Improve safety
- Reduce congestion
- Provide space for other travel modes, like transit or active transportation

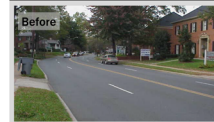
They typically reduce four-lane cross sections to three lanes, creating space for things like parking, sidewalks, bicycle lanes, and streetscaping.

## Typical Road Diet Design:



Source: FHWA Road Diet Informational Guide

## Spotlight: East Boulevard, Charlotte, NC



Source: rethinkingstreets.com

East Boulevard was converted from a four-lane cross section to two travel lanes, a center turn lane, and two bike lanes. Vehicle speeds decreased without negatively affecting travel times.

## Where could it be implemented?

North Chancery Street has a five-lane cross-section between East Morford Street and Fairgrounds Road, including a turn lane and two travel lanes in each direction. One through lane could be removed in each direction, making room for buffered bike lanes, streetscaping, or other improvements.



## What are the benefits?



Improve safety by reducing vehicle speeds.



Reduce crossing distances and improve mobility for people walking and biking.



Increase property values by making the corridor a place to "drive to" instead of "drive through".

## Next Steps



Develop concept → Identify preferred Road Diet elements



Submit request → Submit Corridor Transportation Planning Grant Application to TDOT for analysis



Begin feasibility analysis → Follow TDOT's updated Road Diet Policy Flow Chart

# McMinnville Safe Streets to School Pedestrian + Bicycle Plan

## DRAFT PEDESTRIAN NETWORK



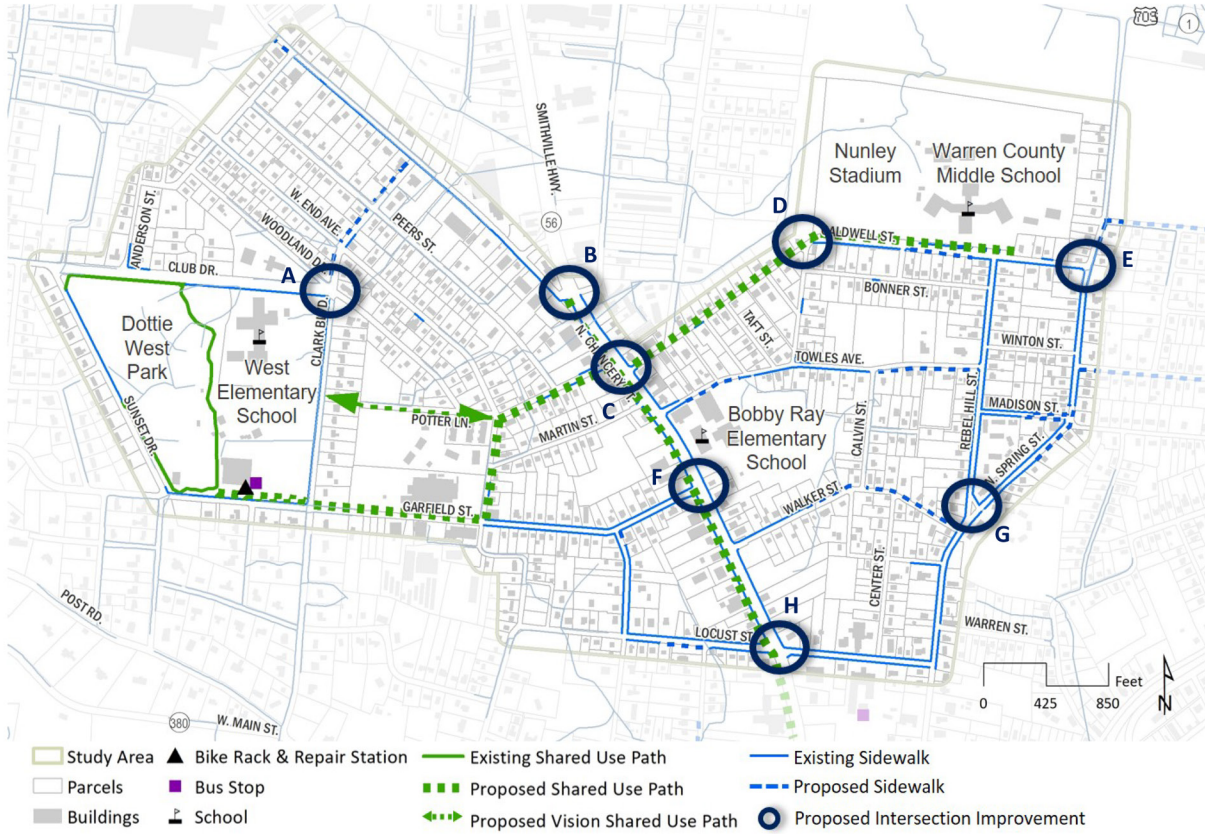
Sidewalk



Shared Use Path



Mid-Block Crossing with Median Refuge Island & Pedestrian-Activated Signal



# McMinnville Safe Streets to School Pedestrian + Bicycle Plan

## DRAFT BICYCLE NETWORK



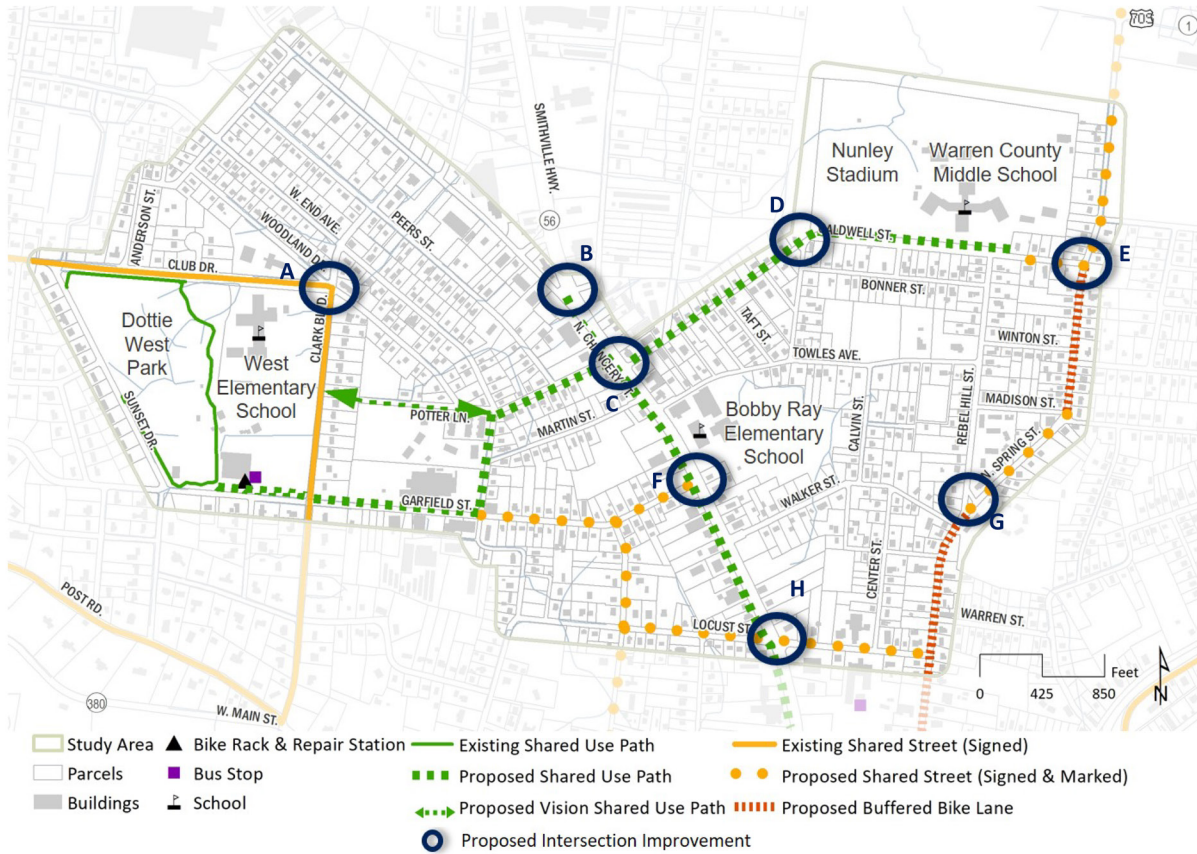
Shared Street



Buffered Bike Lane



Shared Use Path



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# B

## POTENTIAL PRIORITIZATION CRITERIA – FULL PROJECT MATRIX

ROADWAY/FACILITY	FACILITY TYPE	GOAL 1 - SCHOOLS	GOAL 2 - SIDEWALKS	GOAL 3 - PARKS	GOAL 4 - GRANTS	GOAL 4 - NS/EW	GOAL 5 - INPUT	TOTAL SCORE
<b>GARFIELD ST./WEST END AVE./ SEITZ AVE./CALDWELL AVE.</b>	Shared Use Path	HIGH	HIGH	HIGH	MED	LOW	HIGH	21
FROM Existing Shared Use Path TO Rebel Hill St.								
<b>GARFIELD ST./LIND ST./ LOCUST ST.</b>	Shared Street	MED	LOW	MED	MED	HIGH	HIGH	17
FROM West End Ave. TO N. Spring St.								
<b>N. SPRING ST./CALDWELL ST.</b>	Buffered Bike Lane/Shared Street	MED	MED	MED	LOW	MED	HIGH	15
FROM Highway 70 TO Locust St.								
<b>CLARK BLVD.</b>	Sidewalk	MED	HIGH	MED	LOW	LOW	HIGH	13
FROM N. Chancery St. TO Woodland Dr./Club Dr.								
<b>TOWLES AVE./FAIR ST.</b>	Sidewalk	MED	HIGH	MED	MED	LOW	MED	13
FROM Existing Sidewalk TO Red Rd.								
<b>N. SPRING ST./MADISON ST.</b>	Sidewalk	MED	HIGH	MED	LOW	LOW	MED	10
FROM Existing Sidewalk /N. Spring St. TO N. Spring St./ Existing Sidewalk								
<b>N. CHANCERY ST./SR 56 (ROAD DIET ALTERNATIVE)</b>	Shared Use Path	MED	LOW	LOW	HIGH	LOW	MED	8
FROM N. Chancery St. TO Locust St.								
<b>N. CHANCERY ST./SR 56 (ROAD DIET)</b>	Buffered Bike Lane	HIGH	LOW	LOW	HIGH	LOW	LOW	8
FROM N. Chancery St. TO Locust St.								
<b>WALKER ST.</b>	Sidewalk	MED	MED	MED	MED	LOW	LOW	8
FROM Existing Sidewalk TO Calvin St.								
<b>CALDWELL ST./FAIR ST./N. SPRING ST.</b>	Sidewalk	MED	HIGH	LOW	LOW	LOW	MED	8
FROM Existing Sidewalk /N. Spring St. TO Existing Sidewalk/Red Rd.								
<b>LIND ST./LOCUST ST.</b>	Sidewalk	MED	MED	LOW	MED	LOW	LOW	6
FROM Existing Sidewalk TO Existing Sidewalk								
<b>N. CHANCERY ST.</b>	Sidewalk	LOW	HIGH	LOW	LOW	LOW	LOW	4
FROM Existing Sidewalk TO Anderson St.								