

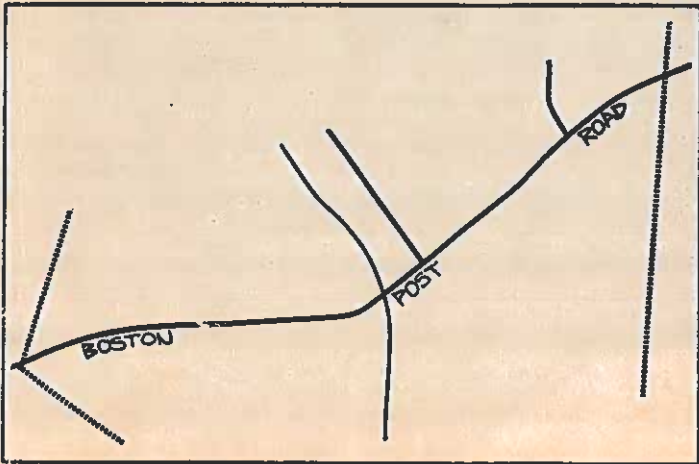
MASTER PLAN UPDATE

VILLAGE
OF
LARCHMONT

TOWN
OF
MAMARONECK



Phase 2



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 Traffic Engineering • Parking Studies

LAND USE PLAN VILLAGE OF LARCHMONT WESTCHESTER COUNTY, NEW YORK

RAYMOND & MAY ASSOCIATES Planning Consultants

CITIZEN'S GUIDE TO THE LARCHMONT MASTER PLAN

Larchmont today is unquestionably one of the most attractive suburban communities on the northern shore of Long Island Sound because of its aesthetic appeal, excellent public services and nearness to New York City.

Because it is almost entirely developed, with a remaining population growth potential of less than twenty percent under its present zoning, Larchmont could retain and even enhance its residential quality once it successfully solves its current problems.

In spite of the general excellence of the environment it offers, Larchmont faces several problems which, if left unattended, may become sources of blight and cause a general decline accompanied by a lowering of property values.

PRIME PROBLEMS TO BE SOLVED

1. Providing adequate parking space.
2. Beautifying and improving its business areas.
3. Improving traffic flow.
4. Enhancing open spaces and recreation areas.
5. Maintaining the quality of older housing.

These problems and proposed solutions thereto are discussed in detail in the report for the Village on the Comprehensive Master Plan which was prepared by Raymond & May Associates, professional planners. The work of the consultants was directed by the Mamaroneck-Larchmont Joint Planning Committee, with the cooperation of various municipal officials of the Village of Larchmont and Town of Mamaroneck, and interested citizens. This detailed report, the result of a two-year study, is available in the Larchmont public library.

THE VILLAGE'S BUSINESS DISTRICTS

The continued vitality of the Village's business districts is essential, not only to provide the services residents need and want, but to prevent blight and deterioration from spreading into the adjoining sound residential areas. Growing competition from new shopping areas outside the Village increase the urgency of the need for remedies.

The Plan recommends these measures:

1. *In the Palmer Avenue District:*
 - (a) Expand Municipal Parking Area #4 across Wendt Avenue to the rear of stores fronting on Chatsworth and Palmer Avenues.
 - (b) Establish a new municipal parking facility in the area now occupied by three private parking areas and two residential buildings to the rear of stores fronting on the north side of Palmer Avenue, between West and East Avenues.
 - (c) Build a new overpass, paralleling the Chatsworth Avenue overpass, from Depot Way West to Myrtle Boulevard. This would include a new bridge over the tracks as far as the Village Parking Plaza over the Thruway, as well as a widening of the existing down-ramp to Myrtle Boulevard.
2. *In the Boston Post Road District:*
 - (a) Expand the Gilder Street Parking Area easterly to the rear of stores fronting on Larchmont Avenue.
 - (b) Add to the Addison Street Municipal Parking Area all adjoining private parking areas, to guarantee their permanent retention in public ownership.
 - (c) Add another vehicular access to the Addison lot and improve pedestrian access.

Once these improvements are realized and their effect evaluated, the Village may be in a position to provide incentives for the gradual private rebuilding of the more obsolete portions of its two business districts.

The Plan also suggests a number of improvements designed to enhance the appearance of Larchmont's business districts to a level more in keeping with the community's over-all character.

HOMES

The Plan foresees an ultimate maximum population of between 7,700 and 8,100, as against its reported 1965 population of 6,860. The present one-family residential pattern would remain unchanged. The Plan recommends against any extension of apartment zoning, and suggests that further residential construction in business and industrial districts be prohibited.

COMMUNITY FACILITIES

Open spaces and well-developed recreation areas are priceless community assets. The importance of such facilities to Larchmont residents is shown by the continuing increase in the number of supervised recreation programs.

The Plan proposes:

1. The conservation of 20 to 25 acres of wetlands along the Premium River in the Village of Larchmont and Town of Mamaroneck. A small portion of the area might be developed for active recreation.
2. Expansion of Pine Brook Park.
3. Improvements to all existing Village parks.
4. Coordination of Hommocks area development with that of Flint Park, to avoid duplication of facilities.

The Plan also reports on the Village's schools, library, fire protection, water, sanitary sewers, and other municipal facilities.

IMPLEMENTATION OF PROPOSALS

The Plan discusses the Village's finances and describes in detail ways of implementing its proposals. Many can be executed and paid for by the County and the State. Also available are varied financial aid programs shared by Federal and State governments.

HOW CAN CITIZENS HELP?

1. Read the full and detailed Plan in the Larchmont library.
2. Use the opportunity to discuss, question, and express your thoughts on all aspects of the Plan at public meetings. Planning Commission members will listen to citizen opinions before making recommendations to the Village Board.
3. Express your opinions to the Village officials responsible for implementing any proposal (such as members of the Library or School Boards).

The Planning Commission can make recommendations only. The power to carry them out rests with the Village Board of Trustees.

Whether Larchmont is as attractive tomorrow as it is today is really up to you!

VILLAGE OF LARCHMONT

BOARD OF TRUSTEES

Mayor

Leo Goldsmith, Jr.
Edward F. X. Ryan (term expired 1966)

Trustees

Eugene C. Merkert*
John B. Forrest*
Kenneth H. Wanderer
Edmund S. Purves
Richard J. Scheuer* (term expired 1965)

PLANNING COMMISSION

Chairman

James F. Reville*

Vice-Chairman

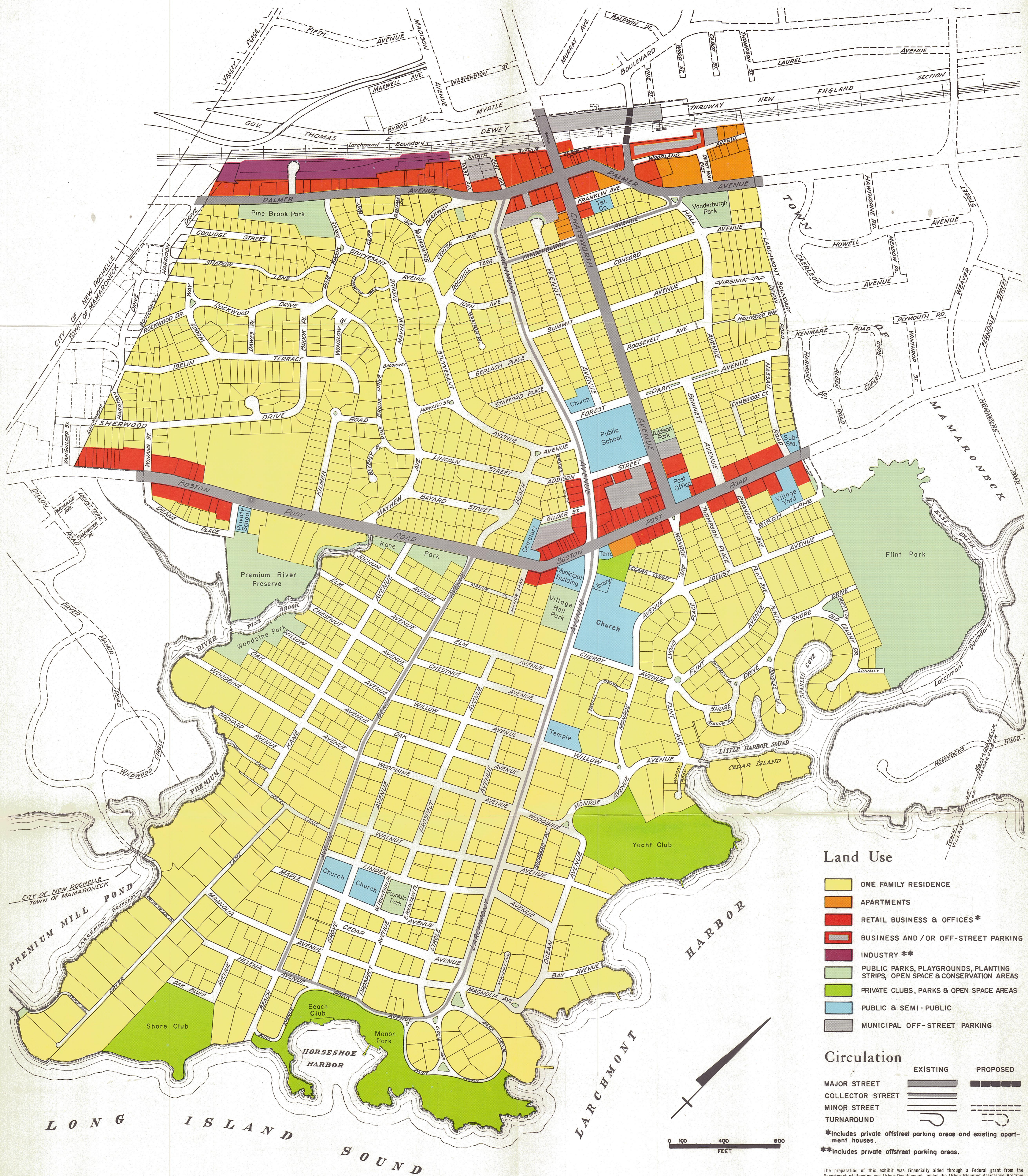
Paul Smith, Jr.
S. H. Ensinger
Harry L. Hampton*
Mrs. Frank A. Homer
Dudley B. Smith
Edward E. White

*Member, Mamaroneck-Larchmont Joint Planning Committee
Raymond & May Associates, Planning Consultants.

LAND USE PLAN

VILLAGE OF LARCHMONT

WESTCHESTER COUNTY, NEW YORK
 RAYMOND & MAY ASSOCIATES Planning Consultants



- ### Land Use
- ONE FAMILY RESIDENCE
 - APARTMENTS
 - RETAIL BUSINESS & OFFICES*
 - BUSINESS AND /OR OFF-STREET PARKING
 - INDUSTRY**
 - PUBLIC PARKS, PLAYGROUNDS, PLANTING STRIPS, OPEN SPACE & CONSERVATION AREAS
 - PRIVATE CLUBS, PARKS & OPEN SPACE AREAS
 - PUBLIC & SEMI-PUBLIC
 - MUNICIPAL OFF-STREET PARKING

- ### Circulation
- | | EXISTING | PROPOSED |
|------------------|----------|----------|
| MAJOR STREET | | |
| COLLECTOR STREET | | |
| MINOR STREET | | |
| TURNAROUND | | |
- *Includes private offstreet parking areas and existing apartment houses.
 **Includes private offstreet parking areas.

The preparation of this exhibit was financially aided through a Federal grant from the Department of Housing and Urban Development, under the Urban Planning Assistance Program for the New York Department of Commerce. It was financed in part by the State of New York.
 1966

SUMMARY OF RECOMMENDATIONS

Master Plan Update - Phase 1 and 2
 Village of Larchmont/Town of Mamaroneck

| | Phase 1 | Phase 2 | | Phase 1 | Phase 2 |
|---|---------|---------|--|---------|---------|
| A. ZONING | | | C. PARKING | | |
| 1. <u>Town of Mamaroneck</u> | | | 1. <u>Town of Mamaroneck</u> | | |
| a. Establish and map Service Business (SB) District. | * | * | a. Adjust regulations to accommodate employee parking. | * | |
| b. Allow Mixed-Use Development in B District with special permit. | * | * | 2. <u>Village of Larchmont</u> | | |
| c. Amend parking requirement in B District. | * | * | a. Provide long-term parking on certain streets for employees. | * | |
| d. Amend B District to limit automotive uses and require minimum width. | | * | b. Modify regulations in Lots 4, 7, 9 and 10 to expand shopper parking on Saturdays. | * | |
| e. Revise standards for home occupations. | | * | c. Eliminate all non Village/Town resident commuter permits. | * | |
| 2. <u>Village of Larchmont</u> | | | d. Strictly enforce all parking regulations. | * | |
| a. Establish and map RC District. | * | * | e. Add long-term parking at St. Augustine lot. | | * |
| b. Reduce area required in R-7.5 District for cluster development in certain areas. | | * | f. Construct new spaces (Gilder Street, Addison Street, Village Hall Park). | | * |
| c. Revise standards for home occupations | | * | D. DESIGN | | |
| B. TRAFFIC | | | 1. <u>Town of Mamaroneck/Village of Larchmont</u> | | |
| 1. <u>Village of Larchmont</u> | | | a. Establish design vocabulary and review criteria. | * | * |
| a. Standardize pedestrian controls. | * | | b. Institute awards program. | * | * |
| b. Establish loading zones. | * | | c. Revise sign regulations. | * | * |
| c. Modify Post Road intersection with Larchmont and Chatsworth Avenues. | | * | d. Implement Four Year Capital Improvement Program. | | * |
| d. Coordinate traffic signalization. | | * | | | |
| e. Revise flow at Grand Union. | | * | | | |

Joint Planning Group

VILLAGE
OF
LARCHMONT

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Syrette Dym Grant
Bernadette Kingham
James Levi
Lawrence Lowy
Alan Parter
Doris Patt Smith
Lisa Rey
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OF
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Town Board Liaison

October 1987

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EXECUTIVE SUMMARY

This report is the second phase of the Master Plan Update initiated in 1985. The first phase dealt with the Larchmont railroad station business district and adjacent areas in both the Town and Village. This phase deals with the entire Boston Post Road--from the Village of Mamaroneck to the City of New Rochelle.

The purpose of the plan is to establish policies and guidelines for land use, traffic control, parking and design of public and private properties in the study area. Specific actions--physical, legislative or administrative--are proposed to implement the proposed policies.

A. LAND USE

1. Findings

The Post Road reflects the evolution of development patterns from the pre-World War II high density core area around the Village Hall to the auto oriented shopping centers at the east end of the Town. For purposes of analysis, the study area was divided into five "planning units" which have common characteristics (see Map No. 6).

The following specific issues of concern were identified:

- a. The impact of additional development on parcels with remaining capacity under zoning.
- b. The appropriate location of automotive uses and means to avoid adverse impacts from existing uses.
- c. The proper balance, in the Village Hall area, between possible improvements to property through expansion and the adverse impact due to increased parking demand created by such expansion.
- d. The appropriate long term use of two non-conforming nurseries in the residential area of the Village.

2. Policies and Implementation

Planning Unit #1: (Village of Mamaroneck to East Creek)

Land Use Policy: This area should be maintained as the location of large, self contained retail or office complexes. Further subdivision of parcels should be prevented and creation of additional curb cuts discouraged. Mixed use development on large parcels is also appropriate and consistent with this policy.

Implementation:

- a. Amend the zoning regulations to establish a minimum frontage for business use and to refine the list of permitted uses to exclude automotive uses.
- b. Encourage mixed (residential and commercial) use, subject to a special use permit, on large sites as an alternative to further strip commercial development by modifying the zoning amendment proposed in the Phase I report to reduce the minimum site area required to 60,000 square feet.

Planning Unit #2: (East Creek to Thompson Place)

Land Use Policy: The area within which automotive uses are permitted should be kept to a minimum since most sites are inadequate by today's standards. All regulations dealing with use, access and design must be strictly enforced so as to protect adjacent residential areas and minimize traffic conflicts and visual blight from automotive uses.

Implementation:

- a. Include auto sales as a special permit use, rather than a permitted use, in the RB District and redelineate the district to confine it to the area between Bronson Avenue and East Creek.
- b. Require site plan review as part of any external change.

Planning Unit #3: (Thompson Place to Manor Lane)

Land Use Policy: The Village Hall area should remain primarily a retail/personal service center which encourages internal pedestrian circulation. The present intensity of development should be maintained in terms of height and floor area. A major portion of the responsibility for providing necessary off-street parking must be assumed by the Village to preserve the character of the business area and to provide parking in the most efficient manner. However, any waiver of on-site parking requirements should be tied to the provision of additional public parking spaces or should only apply to very minor additions of floor space.

Implementation: This entire area should be rezoned RC under the provisions recommended in Phase I. This designation precludes automotive uses and imposes design guidelines on development to maintain the continuity and character of existing business structures. No waiver of the on-site parking requirements will be allowed.

Planning Unit #4: (Manor Lane to Lorenzen Street)

Land Use Policy: This low density residential/park section of the Post Road is a sound area which provides important visual and functional relief from the otherwise continuous business development.

No new non-residential uses should be permitted. Clustered-residential development is appropriate on the vacant parcels, including the nursery parcels, subject to careful review of environmental impacts. Such clustering on the nursery parcels, as permitted in the R-7.5 District, will allow the more sensitive portions of the site, adjacent to the Pine Brook and Premium River, to be preserved and restored as part of the natural habitat area while concentrating townhouses near the Post Road.

Implementation:

- a. The cluster provisions of Local Law No. 6 of 1977 should be made applicable along the Post Road by reducing the minimum site area to two acres.
- b. The Zoning Law should be amended to more clearly spell out the conditions under which home occupation offices are permitted to prevent the gradual conversion of existing structures to non-residential use.

Planning Unit #5: (Lorenzen Street to New Rochelle)

Land Use Policy: Development of business uses on small parcels unsuited for such use should be discouraged and opportunities for multi-family residential use, as an alternative, should be provided instead. Incentives should be developed to encourage assemblage of small lots for appropriate business or intensive residential use.

Implementation: Redesignate all or part of this area as the SB Service Business District suggested in Phase 1, and permit multi-family residential development subject to a special permit.

B. TRAFFIC

1. Findings

Analysis of signalized intersections along the Post Road indicates that most have adequate capacity to efficiently accommodate the volumes of traffic which traverse them. The exceptions are during certain peak hours at the intersections of Larchmont and Chatsworth Avenues. At these intersections, delay is not primarily due to the volume of traffic but, instead, to a variety of other factors which unduly delay traffic--exclusive pedestrian phases, long crosswalks, large intersection width and misalignment of lanes--all of which result in a reduced level of service.

2. Recommendations

- a. Widen sidewalks at each intersection to reduce crosswalk length and intersection width and realign lanes.

- b. Adjust timing of existing signals to reflect new alignment and to optimize traffic flow, as a first step.
- c. Install new equipment to provide fully traffic actuated signals and interconnection between signals to achieve maximum improvement.

C. PARKING

1. Findings

Whereas parking along most of the Post Road is strictly a responsibility of each property owner, in the concentrated business area around the Village Hall it is, of necessity, a shared obligation of the Village and property owners. In the heart of this area, surveys revealed that existing parking spaces were overutilized by nearly 20% during peak hours--a deficiency of some 80 spaces. Very few opportunities exist to expand the existing parking supply without considerable cost and/or disruption/

2. Recommended Options

Many proposals for expansion of municipal parking were considered. Most were dismissed as infeasible. The options considered most worthwhile to pursue are as follows:

- a. Expansion of the Gilder Municipal Lot to the east through acquisition of the parcel of land between the existing lot and Post Alley to add 39 parking spaces at an estimated cost of \$550,000, including land acquisition.
- b. Designation of a portion of the spaces in the St. Augustine lot for long-term parking for residents and employees.
- c. Construction of a parking area in front of Village Hall Park by relocating the sidewalk into the park to allow perpendicular parking on Larchmont Avenue to add about 17 spaces at an estimated cost of \$77,000.
- d. The fourth option considered could provide the most spaces at one of the most convenient locations. This would involve construction of a parking facility under the school yard on Addison Street to provide either 66 or 128 spaces at a construction cost of \$528,000 or \$1,056,000, exclusive of any land cost, and restoration of the playground above the parking garage. To implement this proposal, particular care would have to be taken to minimize disruption of the school property and the length of time the playground is unavailable.

D. DESIGN

1. Findings

The Post Road exhibits a diverse visual appearance, partly due to the different time periods during which development has taken place and partly due to inconsistent (or non-existent) attention to architectural and landscape design by both public agencies and private property owners. In general, the older areas in the Village--the Village Hall business area and the residential area to the west--have the most consistent quality, scale and level of design treatment. The other areas reflect the results of haphazard design treatment and "lot-by-lot" development. Because of the diversity of style, scale and function throughout the length of the Post Road, it would be inappropriate to suggest a single theme or character of design. However, it is important to develop design guidelines to address the various conditions which require attention and to establish appropriate procedures to ensure that the appearance of both public and private property is given serious consideration during the design and review process.

Those elements which can and should be influenced by public design policy include signs (business and public), street furniture and paving, parking areas and walkways, entrances to the community, and landscaping and screening.

2. Recommendations

- a. Design guidelines are established which should be agreed upon by all bodies involved in design review and approval.
- b. A design concept plan for the entire Post Road has been prepared illustrating the location and application of design techniques including: street tree planting, parking lot screening and planting, sidewalk design, crosswalk design, bus stop improvements, lighting, signage and special landscaping treatment. The total cost of those elements which are a public responsibility is estimated at over \$400,000.
- c. A four year program to accomplish almost all of the public design improvements (except for long range, high cost items in the Village Hall area) is proposed which would require an expenditure of \$20-25,000 per year by both the Town and Village. This program would result in the planting of trees along the entire Post Road as well as a variety of other improvements.
- d. A number of other actions are proposed to foster improved design including: holding design workshops for involved boards, instituting an awards program to recognize design efforts by private property owners, development of an accepted "design vocabulary" of elements to be used in both public and private projects, and revision of the sign regulations of both Town and Village.

PART I

SCOPE AND PURPOSE

I. Scope and Purpose

The Master Plans for the Village of Larchmont and Town of Mamaroneck were first prepared in 1966. The Town undertook an update in 1976. Some 20 years after preparation of the original plans, both communities determined that a comprehensive update was necessary to provide guidance for future planning and zoning decisions. A Joint Planning Group, which includes members of various boards and commissions involved in planning, zoning, parking and other related municipal functions, was appointed to oversee the Master Plan Update. The planning consulting firm of Shuster Associates, supported by traffic and parking consultants Allan Davis Associates, was retained to assist the Joint Planning Group in its task.

This report is the second phase of the Master Plan Update initiated in 1985. The first phase dealt with the Larchmont railroad station business district and adjacent areas in both the Town and Village. This phase deals with the entire Boston Post Road--from the Village of Mamaroneck to the City of New Rochelle. See Map No. 1

The purpose of the plan is to establish policies and guidelines for land use, traffic control, parking and design of public and private properties in the study area, based on evaluation of existing conditions, projected trends, and community objectives. Implementation of the policies is proposed in the form of specific actions--physical, legislative or administrative.

A. Inventory and Analysis

Field surveys were undertaken and data gathered from November 1986 to January 1987 in sufficient detail to provide a basis for detailed planning. Such data included:

1. Analysis of land use distributions and relationships and detailed inventory of land use by structure.
2. Analysis of existing zoning regulations in relation to existing land use and potential development capacity.
3. Photographs and analysis of selected design attributes and problem areas.
4. Surveys of parking spaces by type and usage.
5. Compilation and analysis of existing parking policies and regulations.
6. Traffic signal features and design and analysis of intersection capacity.
7. Public transit schedules, routes and stopping locations.
8. Identification of land use conflicts, traffic problems and parking deficiencies.

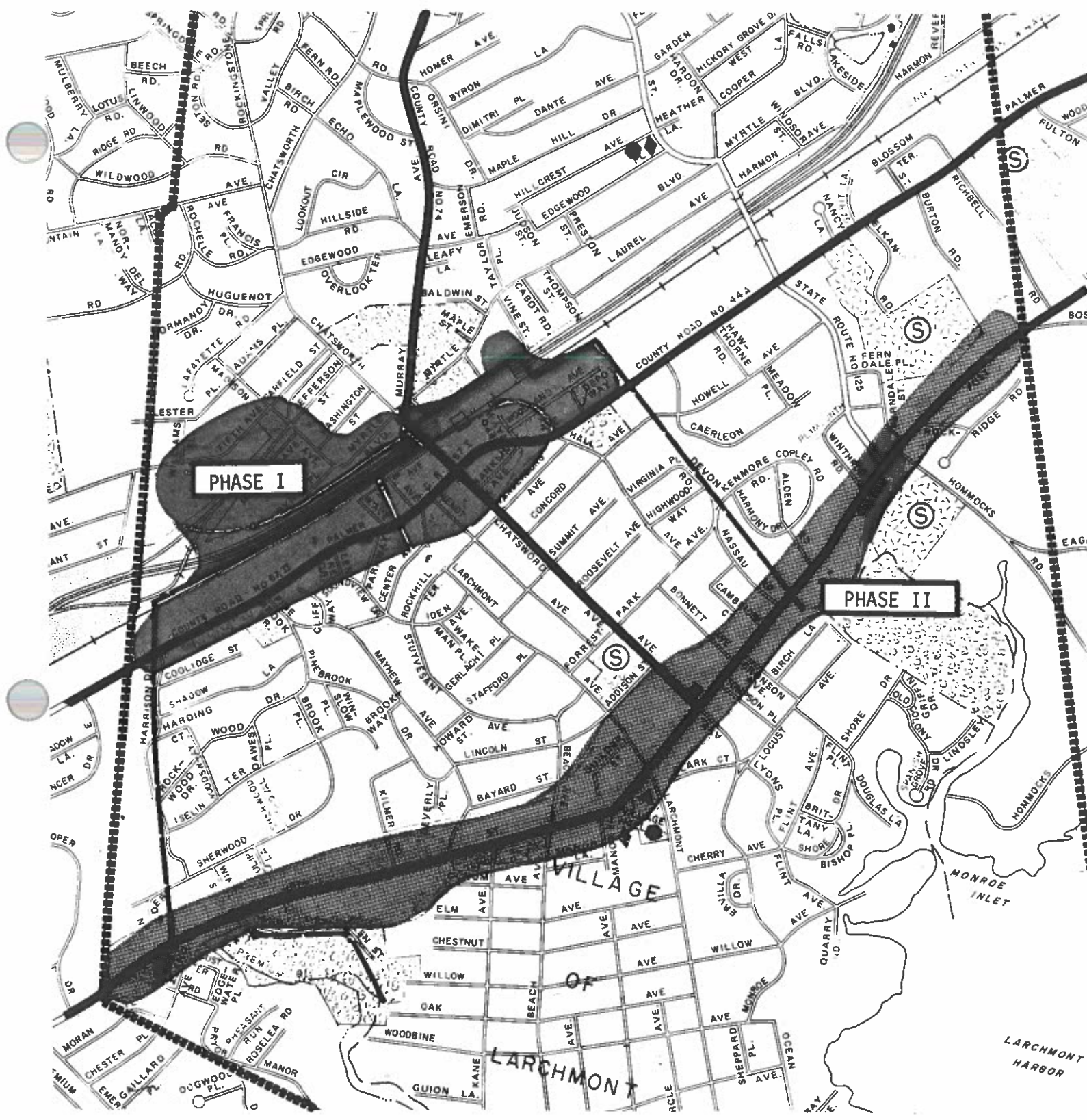
B. Business Area Policy Plan

Based on the above analysis, a plan for the entire business area was developed. At meetings of the Joint Planning Group preliminary recommendations were reviewed and alternatives considered. The Plan, in text and map form deals with five distinct sub-areas, or planning units and includes the following:

1. Land use and development policies to serve as a basis for zoning regulations and other development controls.
2. Parking policies and recommendations for short term and long term actions.
3. Proposals for traffic circulation modifications, including both physical and regulatory actions.
4. Design guidelines and a design concept for the entire Post Road including specific illustrations of treatment of sidewalks, parking lots, open space, public and business signs and other elements.
5. An implementation plan, including recommended priorities for action.

C. Action Program

A program is recommended for implementation of high priority actions--those which are either urgent or can be readily accomplished. Such actions consist of proposed zoning amendments, new parking regulations, improvements to parking facilities and techniques to enhance the visual environment. The action program is in sufficient detail so that the responsible local agency or board can take appropriate actions without further study.



Study Area

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 Traffic Engineering • Parking Studies

Map No.
1

**MASTER
 PLAN
 UPDATE**

**VILLAGE
 OF
 LARCHMONT**

**TOWN
 OF
 MAMARONECK**

PART II
INVENTORY AND ANALYSIS

II. INVENTORY AND ANALYSIS

The Study Area was defined to include the entire length of the Boston Post Road--from the Village of Mamaroneck to the City of New Rochelle--plus the adjacent streets in the Village Hall business area. Consequently, it includes a wide range of business uses plus numerous other land uses, including both single family homes and apartments. As the major regional traffic thoroughfare, the Post Road carries heavy volumes of both local and through automobile traffic, trucks and buses.

For purposes of detailed analysis, the Study Area has been divided into five planning units which are discussed in Part III. The general characteristics of the area, the major issues to be addressed and basic conclusions and groundrules are discussed in this part.

A. Land Use Issues

Land use along the Post Road reflects the evolution of the type and intensity of development in the twentieth century. The core of the business area around the Village Hall consists of dense, multi-story, mixed use development typical of pre-World War II construction. The eastern end of the Post Road, near the Village of Mamaroneck, illustrates more recent development patterns of low density, highway oriented retail "strips". Also included are a mixture of residential and business uses near the New Rochelle line, a concentration of automotive uses east of the Village Hall area, and the only homogeneous residential frontage, west of Village Hall.

The Existing Zoning map (see Map No. 2.) reflects the land use pattern very closely and, except at the western end, the demarcation between business and residential uses is quite clear--both along the Post Road and between the Post Road and the adjacent neighborhood. A detailed survey of the use of every property was completed* and a map made illustrating the various categories of residential uses and a list of the name and address of every use. On the basis of the survey and observations of land use relationships in the Study Area, the following land use issues were identified as warranting the greatest attention during the Master Plan Update (see map No. 3):

1. Sites With Unused Development Potential

While much of the Post Road is developed to the maximum allowed under present business zoning, several sites near either end are underutilized in terms of building area or intensity of use. Included in this category are several auto sales lots and gas stations plus some small non-conforming residential uses on the western end and the VFW and, perhaps, the Larchmont Motel and several others on the eastern end. The impact of more extensive development on these sites must be evaluated.

*See Exhibit B.

2. Location of Automotive Uses

Due to its function as a major traffic artery, the Post Road has attracted a variety of automotive commercial uses, including new and used car sales, gas stations, repair shops and shops selling automotive accessories. These uses generate a high volume of traffic and often detract from the visual environment. Many of these uses have concentrated just east of the Village Hall area and near the New Rochelle line. However, others are scattered elsewhere and permitted--by right or subject to a special permit--in the business districts of both the Town and Village. The appropriateness of restricting the location of automotive uses must be addressed.

3. Intensification of Development in the Village Hall Business Area

Development in the Village Hall area took place in accord with the practices of an earlier time, when a mixture of commercial and residential uses in multi-story buildings, without on-site parking, was common in the suburbs. This area remains a compact, intensive business district. However, the scarcity of parking has become a problem and zoning now requires all uses to provide on-site parking. This requirement virtually eliminates the possibility of existing uses expanding. In some cases, this situation prevents up-grading an existing structure or improving its economic viability. The pros and cons of this policy must be assessed.

4. Future Use of the Nursery Sites

The two nursery properties just west of Pine Brook comprise the largest underdeveloped parcels along the Post Road. This area, however, is part of the only residentially zoned portion of the Post Road and these businesses are non-conforming uses. In addition, they are at the junction of Pine Brook and the Premium River--an area declared a "critical environmental area" (CEA) under the Village's Environmental Quality Review Law. Careful study is needed to identify the appropriate long range use of this sensitive area.

B. Traffic Analysis

The Boston Post Road (U.S. Route 1) through the Town of Mamaroneck and several connecting north/south roads carry traffic bound for the business area as well as through traffic. Traffic conditions in the Study Area were observed on frequent visits from early in the morning until late evening. Counts of traffic on the Boston Post Road were made at both extremes of the study area by means of traffic counting machines in 15-minute increments. The location of these counts is summarized in Table 1 which shows the 24-hour traffic volumes for each location.

Peak period turning movement counts were made at five traffic signal controlled intersections in the study area between Larchmont Avenue and Richbell Road inclusive. These counts were made from 7:00 a.m. to 9:00 a.m. and from 4:00 p.m. to 6:00 p.m. on weekdays and 10:00 a.m. to 2:00 p.m. on Saturday during December of 1986. The intersections which were counted were those of Larchmont Avenue, Chatsworth Avenue, Alden Road, Weaver Street and Richbell Avenue all with the Boston Post Road. See Map No. 4.

All five traffic signals provide for both pedestrian and vehicular movements with separate signal indications.

Levels of Service

An analysis of each of the five intersections was made based on turning movements counted for the morning peak period, the afternoon peak period and the Saturday peak period utilizing the 1985 Highway Capacity Manual*, "planning version" of capacity analysis. This generalized technique indicated that all of the intersections presently operate under capacity during all of the peak periods.

A more detailed operational analysis which provides for full analysis of capacity and level of service was performed for the intersections of Larchmont Avenue and Boston Post Road and Chatsworth Avenue and the Boston Post Road. Level of service is a method used to measure the function of an intersection in terms of the delay experienced by the motorist. A level of service C, or D at the worst, is considered acceptable during the peak hour. A complete description of each level is included in Exhibit A. The Larchmont Avenue and Boston Post Road intersection operates at level of service C during all of the peaks. The intersection of Chatsworth Avenue and Boston Post Road operates at level of service C during the morning peak period and at level of service E during the evening peak period and Saturday peak period. This latter situation indicates a problem which must be addressed.

C. Parking Analysis

The land uses in the study area generate a substantial demand for parking spaces. This demand is being met by both private property owners and public agencies. Within the study area, there are approximately 350 parking spaces provided by private owners and 675 municipal spaces. See Map No 5. Private lots are generally small with only the Post Office parking lot exceeding 30 parking spaces.

*Special Report 209 published by the Transportation Research Board. This is the manual recommended by the New York State DOT and is the most widely used reference in the field.

The surveyed parts of the municipal system include 358 on-street spaces and 317 off-street spaces.

Parking Regulations

The Village regulates its parking in the study area through the issue of permits and through posted regulations concerning duration of parking and prohibition of parking. It issues three types of permits, 24-hour, daytime and nighttime. The posted regulations range from three hours to 15 minutes.

Some parking resources are reserved for resident permit holders while others are made available on a first come first served basis. Some spaces are restricted to short-term parking. No on-street overnight parking is allowed. The Village has a four hour limit on all on-street parking. Off-street overnight parking with a permit is available to residents. See Table 2.

Parking Surveys

Surveys of parking occupancy in all of the public and private spaces in the study area were carried out on Thursday, December 11, 1986, from 8:00 a.m. to 4:00 p.m. and on Saturday, December 13, 1986, from 9:00 a.m. to 7:00 p.m. During these surveys the license plates of vehicles within the municipal parking spaces were also noted so that the rate of turnover would be identified.

Parking studies generally are conducted to identify inadequacies or to develop proposals to improve parking supply in a specific area. A parking inventory, characteristics of current parking usages, parking demand estimates, and administrative factors were all used to study the C.B.D. in Larchmont. The fact that the parking survey was undertaken during the December holiday shopping season may have produced a slightly overstated maximum accumulation and proportion of short-term parkers. In terms of a Master Plan, the data collected is adequate to utilize in the decision making process.

The municipal on-street and off-street parking areas with high occupancy during both the weekday and Saturday surveys are concentrated in the retail business area. See Tables 3A, 3B, 4A, and 4B. The perimeter area, which for the most part involves residential streets, had parking available throughout the survey period. The Saint Augustine parking lot, which is also located on the perimeter of the parking survey area, had 25 or more available spaces during both the weekday and Saturday survey with the exception of 5 p.m. to 6 p.m. on Saturday due to church service. While the Addison and Gilder Street lots were occupied at greater than 85% for only one and three hours, respectively, on Thursday, the occupancy rate exceeded 85% for eight consecutive hours in each lot on Saturday.

The occupancy of the private parking spaces is summarized in Tables 5A and 5B for Thursday and Saturday, respectively. The difference between the Thursday and Saturday surveys is quite clear as represented by the number of locations having an occupancy at some point during the day of 85 percent or greater. On Thursday, there were 12 such locations. On Saturday, there were 6.

During the weekday survey, the private parking providers had few unoccupied spaces in any of the off-street lots. On Saturday there were two locations with parking available in the afternoon, the Bank of New York had a minimum of 12 spaces unoccupied and Larchmont Federal Savings had a minimum of 19 spaces unoccupied.

Given the present demand and concentration of existing parking, an adequate level of parking service can be obtained only with the addition of municipal off-street parking. Upon construction of additional parking areas, short-term, long-term and permit parking can be reassigned to provide the most efficient use of available parking.

D. Design and Landscaping

The Study Area exhibits a diverse visual appearance, partly due to the different time periods during which development has taken place and partly due to inconsistent (or non-existent) attention to architectural and landscape design by both public agencies and private property owners. In general, the older areas in the Village--the Village Hall business area and the residential area to the west--have the most consistent quality, scale and level of design treatment. The other areas reflect the results of haphazard design treatment and "lot-by-lot" development with little or no relationship between adjacent properties. Also, in the older business district, efforts have been made to improve the design of public sidewalks and parking lots. Little evidence of any conscious "streetscape" design exists elsewhere.

Because of the diversity of style, scale and function throughout the length of the Post Road, it would be inappropriate to suggest a single theme or character of design. However, it is important to develop design guidelines to address the various conditions which require attention and to establish appropriate procedures to ensure that the appearance of both public and private property is given serious consideration during the design and review process.

Those elements which can and should be influenced by public design policy include the following:

1. Signs

- a. Business Signs: The size, design and location of business signs contribute greatly to the appearance of a business district. There has been an obvious effort to control inappropriate signs in the Village Hall area, particularly hanging and freestanding ones. Elsewhere, however, the design, location and size of signs (particularly free-standing ones) is chaotic and becomes a dominant part of the visual environment.
- b. Public Signs: The profusion of signs for traffic, parking and directional purposes can be both unsightly and ineffective. A program to improve design, location and coordination of such signs can enhance business district appearance and function.

2. Street Furniture/Sidewalk Paving

There has been an effort in the Village Hall area to provide the visual and functional amenities which enhance an urban business area. Continued attention is required. However, in those areas at the east and west ends of the Post Road, there has been virtually no effort to enhance the public right-of-way. In a number of instances, the lack of demarcation between access driveways and sidewalks is not only a visual problem but a safety hazard as well.

3. Parking Areas and Walkways

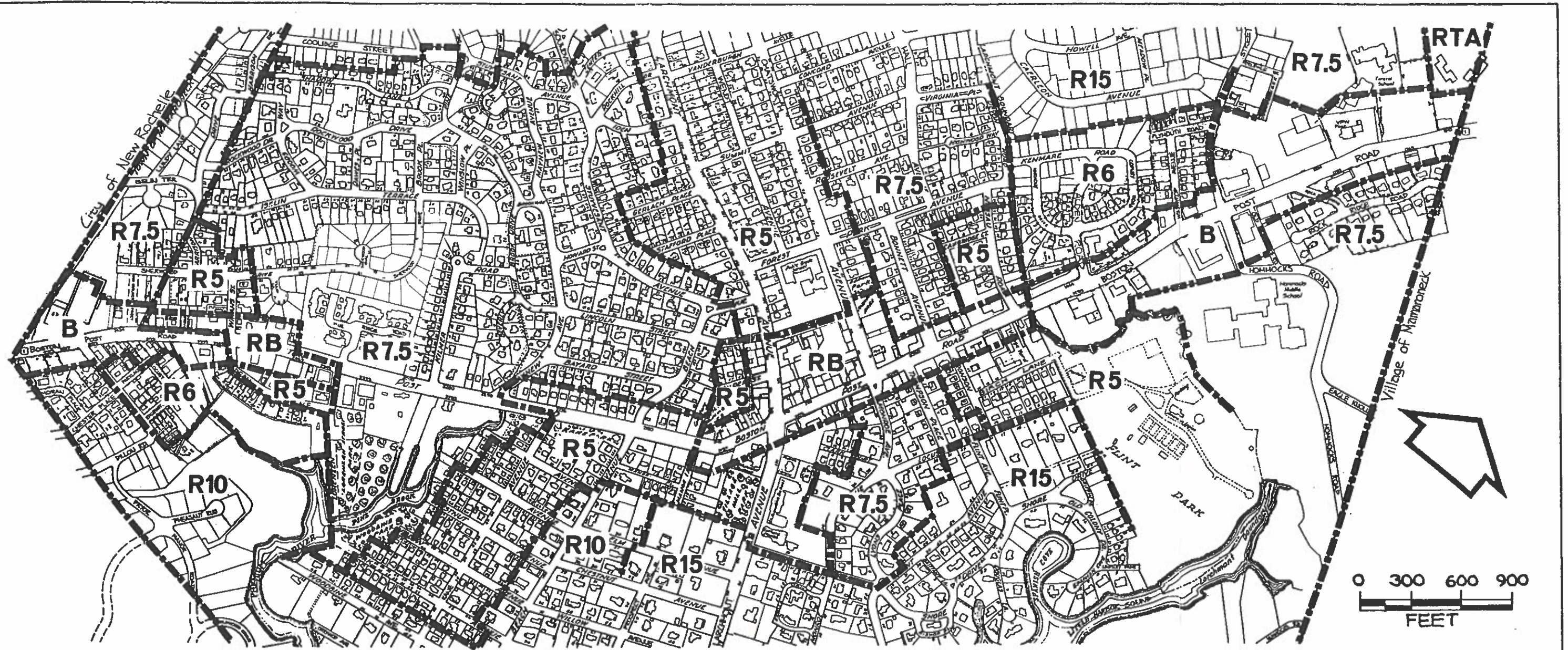
Most people arrive at the business area by car. The parking area and the route they walk to their destination does much to form their impression of the business district. Therefore, parking lot design, landscaping, signage and access must be attractive. While the Village parking lots have been the subject of a concerted effort to improve their appearance, most of the private parking areas along the Post Road are large, bare expanses of asphalt. In part, this may be due to the excessive requirement of one space for each 100 square feet of floor area in the Town Zoning Ordinance.

4. Entrances

The quality of the many entrances to the Town and Village business districts varies. Many are attractive--some are not. A program of common entrance and directional signs could be useful and provide a common design theme.

5. Landscaping

Unlike most of the Town and Village, the Post Road is virtually treeless, except between Bonnet Avenue and Mayhew Avenue in the Village. The width of the Post Road, plus the substantial set-back of most buildings, is accentuated by the lack of trees and other landscape elements on both public and private property.



Existing Zoning

Map
No. 2

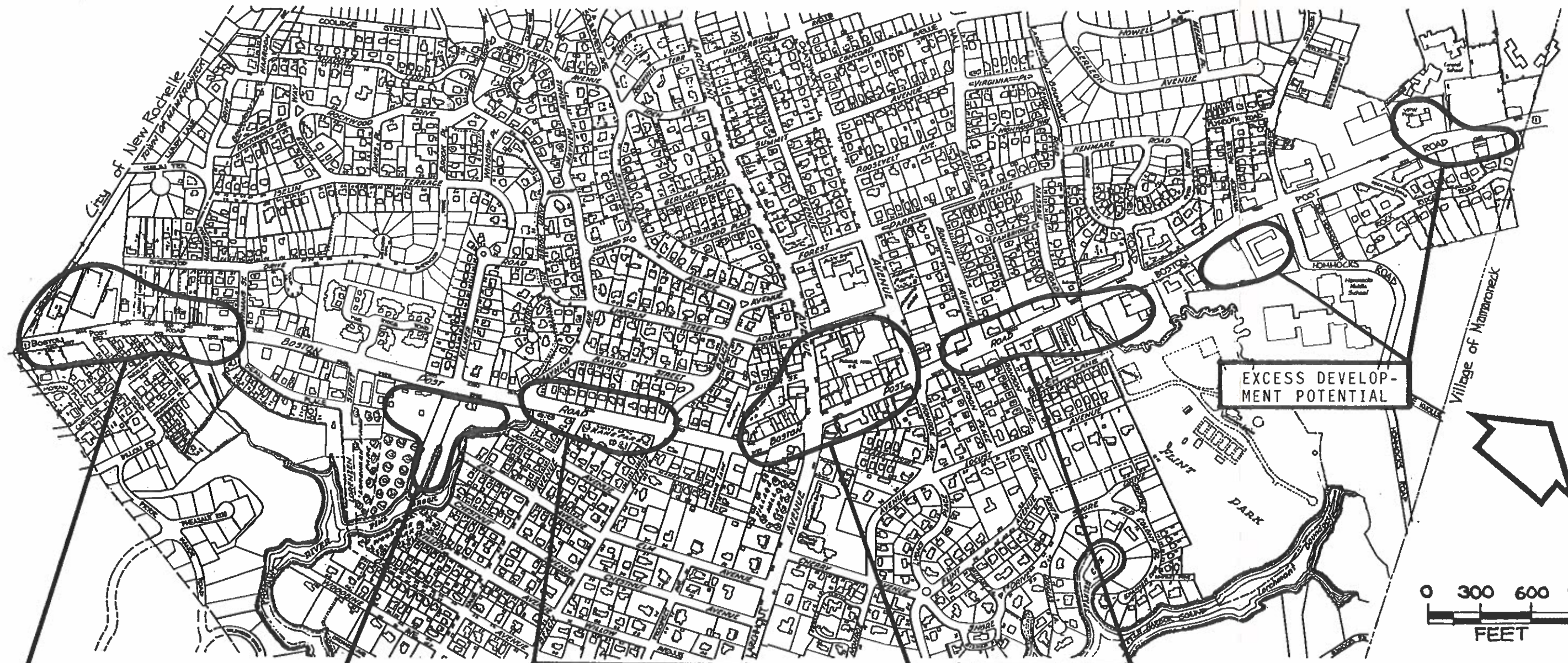
MASTER
PLAN
UPDATE

VILLAGE
OF
LARCHMONT

TOWN
OF
MAMARONECK

SHUSTER ASSOCIATES
RD 1, Box 259 Stone Ridge, New York 12484

ALLAN DAVIS ASSOCIATES, INC.,
Traffic Engineering • Parking Studies



EXCESS DEVELOPMENT POTENTIAL

MIXED USES AND EXCESS DEVELOPMENT POTENTIAL

FUTURE USE OF NURSERIES

PRESERVE RESIDENTIAL CHARACTER

IMPACT OF DEVELOPMENT ON PARKING DEMAND/SUPPLY

CONTROL AUTOMOTIVE USES

Land Use Issues

Map No. 3

MASTER PLAN UPDATE

VILLAGE OF LARCHMONT

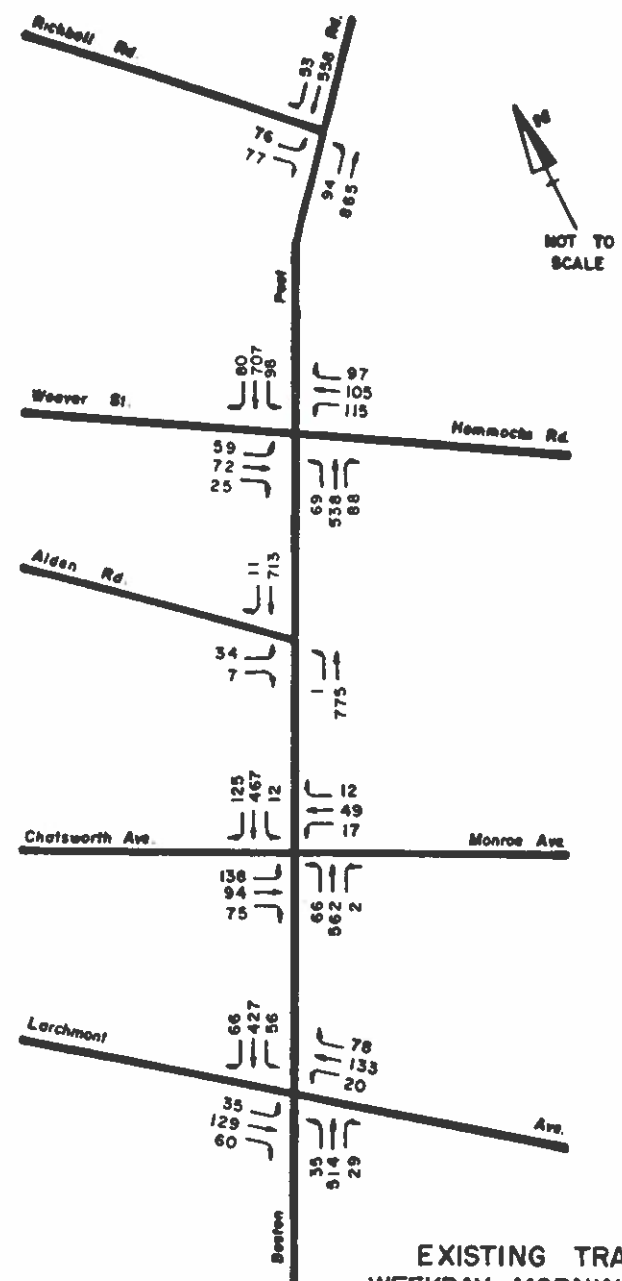
TOWN OF MAMARONECK

SHUSTER ASSOCIATES

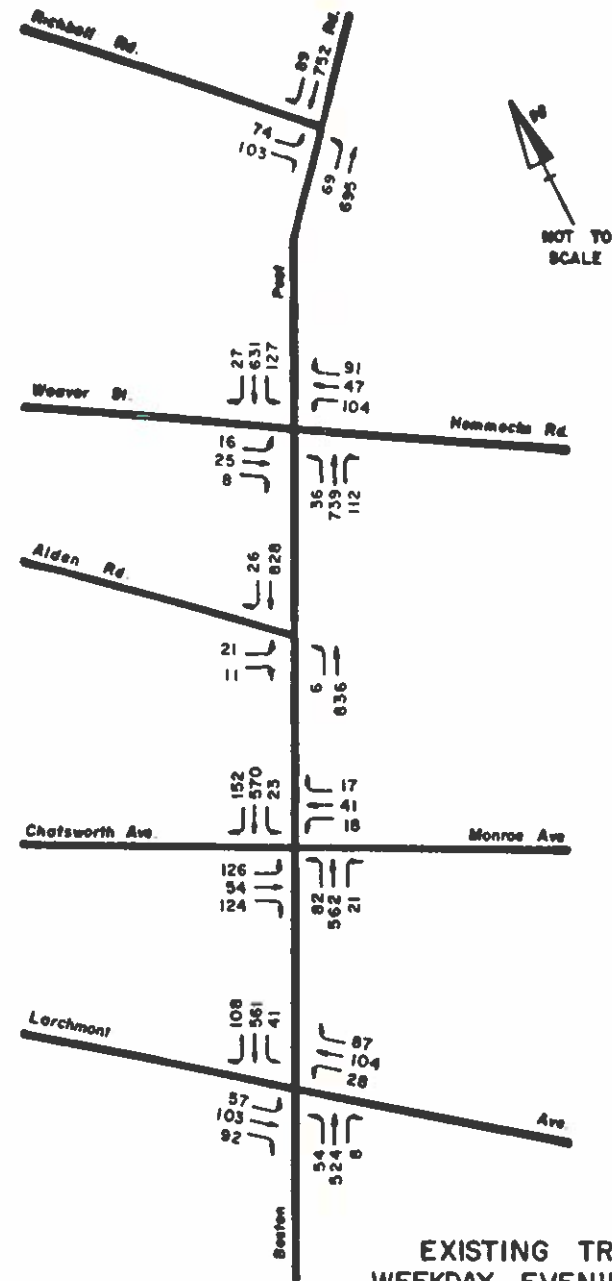
RD 1, Box 259 Stone Ridge, New York 12484

ALLAN DAVIS ASSOCIATES, INC.,

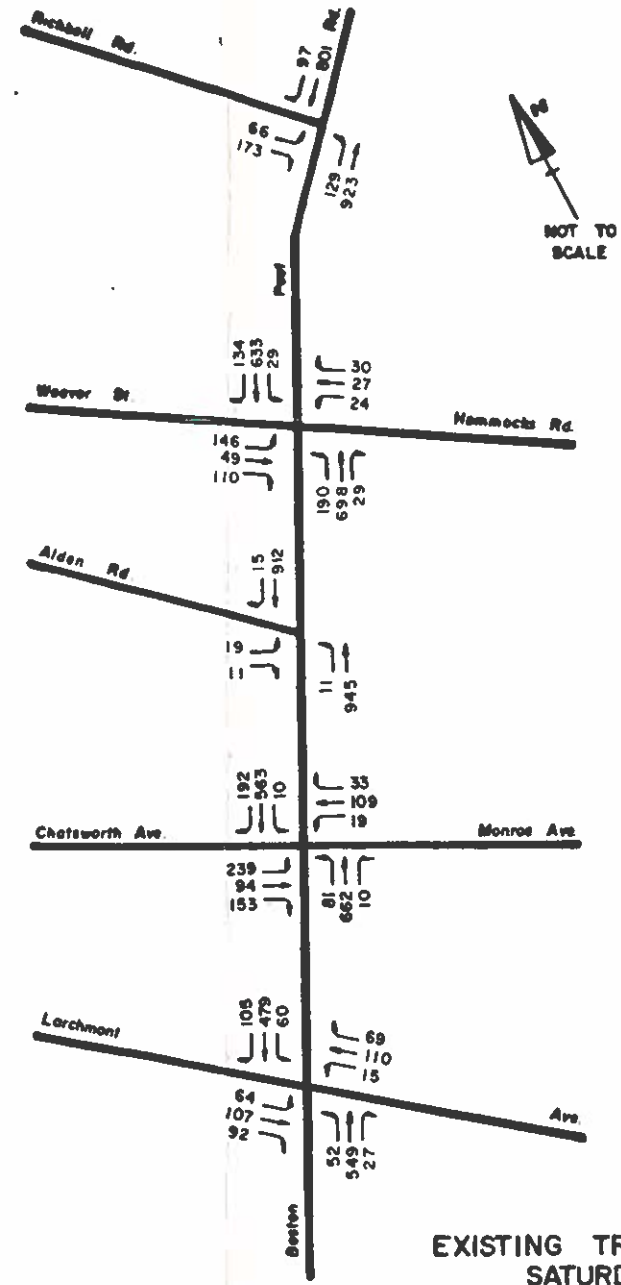
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EXISTING TRAFFIC VOLUMES
WEEKDAY MORNING PEAK HOUR



EXISTING TRAFFIC VOLUMES
WEEKDAY EVENING PEAK HOUR



EXISTING TRAFFIC VOLUMES
SATURDAY PEAK HOUR

Traffic Volumes

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Map
No.

4

MASTER
PLAN
UPDATE

VILLAGE
OF
LARCHMONT

TOWN
OF
MAMARONECK

LEGEND

- 15min. - 15 Minute Parking
- 30min. - 30 Minute Parking
- 1 hr. - 1 Hour Parking
- 2 hr. - 2 Hour Parking
- 3 hr. - 3 Hour Parking / Permit
- B/H - Bus / Handicapped
- Emer. - Emergency
- None - No Regulation

- 45** Number of Spaces in Public Lot
- (00)** Number of On-street Spaces
- Private Lot



Parking Inventory

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| | |
|----------------------------|--------------------------|
| Map No. 5 | MASTER PLAN UPDATE |
| VILLAGE OF LARCHMONT | TOWN OF MAMARONECK |

PART III
PLANNING ANALYSIS AND POLICY DEVELOPMENT

III. PLANNING ANALYSIS AND POLICY DEVELOPMENT

This section summarizes the analysis of existing conditions and formulates policies and specific actions to implement each policy. The specific land use policies are based on several basic principles derived from past planning efforts, discussions with Town and Village officials and Joint Planning Group members, and consultant observations, as follows:

1. Business use and development should be confined within the present boundaries delineated by the existing Zoning Map and the sharp distinction between business and residential zones maintained.
2. The general scale and character of the business areas should be preserved and adjacent residential neighborhoods must be protected from any adverse impacts of business uses.
3. The areas within which automotive use are permitted should be confined to those locations where such uses already exist.
4. Land use policies, and the development regulations to implement them, should reflect the specific characteristics of the varied sub-areas within the study area.
5. Policies for land use must be coordinated with the findings and conclusions concerning traffic flow and parking supply.

The remainder of the section is divided into two parts. The first deals individually with five identified planning units; the second sets forth a concept for design treatment of public and private property along the Post Road.

A. PLANNING UNITS

For the purpose of analysis and detailed planning, the study area has been divided into five planning units which each have common characteristics and/or defined physical boundaries (see Map No. 6). Set forth below is a discussion of each planning unit in terms of the following:

1. Existing land uses.
2. Traffic and Parking considerations (Planning Unit No. 3 only).
3. Current development policies based on the existing master plan and zoning.
4. Development issues.
5. Proposed land use policies

6. Specific design or site planning considerations.
7. Proposed implementation actions.

Planning Unit #1

1. Existing Land Use

This area encompasses the northeast end of Boston Post Road in the Town of Mamaroneck, between the Villages of Larchmont and Mamaroneck, and exhibits the low density characteristics of post-war "highway strip" development, with large general retail stores and restaurants predominating. Most buildings are fairly new, of one story in height and have ample off-street parking.

Distinct changes between commercial and residential land uses form the boundaries between Post Road development and adjacent areas on either side, while the northeast and southwest boundaries coincide with the boundaries of the two villages, into which similar patterns of land use continue. This area also contains two shopping plazas, two banks, two four story apartment houses, a single family residence on Weaver Street, two car dealers, an office building and the only motel on the Boston Post Road in the study corridor.

Properties are generally well maintained, with the more extensive landscaping provided for the most recently constructed buildings. The uses and adjacent streets and sidewalks are, generally, well maintained. Some of the larger parking lots have wide curb cuts that virtually eliminate the sidewalks and pose potential safety hazards for pedestrians. The least intensively developed parcel is the lot adjacent to the International House of Pancakes, which is occasionally used for overflow parking for that restaurant and the neighboring auto parts store.* Other properties capable of accommodating more intensive uses are the VFW site and, perhaps, the Larchmont Motel.

2. Current Development Policies

Almost all of this area is zoned B (Business). In spite of minimal lot size and setback requirements, development is at a low density reflecting off-street parking requirements for offices, restaurants and retail establishments. The 1966 Master Plan and the 1976 Update call for continuation of the existing type of uses in this area. A portion of this area, containing the rear portion of the apartment house parcels, is zoned R-6 (Residential, 6,000 square foot minimum lot size).

Both of the apartment houses are nonconforming uses within both the B and R-6 Districts, which bisect their sites. The VFW post and the single family house on Weaver Street are nonconforming uses within the B District.

*It is understood that a plan to use this site for offices has recently been submitted.

3. Development Issues

There are two issues of concern in this area:

- a. The existing Business District Zoning permits a broad range of uses including retail and service uses, offices, commercial recreation and auto sales, plus additional uses subject to a special permit. The potential impact of some of these possible uses on adjacent residential areas, traffic flow and appearance may well be adverse, particularly in that some lots are vacant or have additional development capacity.
- b. The appearance of this area suffers from a lack of a defined streetscape. There is little landscaping or street furniture along the right-of-way, unsightly utility lines, and excessive curb opening--all of which detract from its visual character.

4. Proposed Land Use Policy

This area should be maintained as the location of large, self contained retail or office complexes. Further subdivision of parcels should be prevented and creation of additional curb cuts discouraged. A coordinated public/private program should be pursued to enhance the appearance of the street and abutting private property by establishing a landscaping theme. Mixed use development on large parcels, as proposed in Phase I of this Master Plan Update, is also appropriate and consistent with this policy.

5. Site Planning and Design Considerations

Site planning for any new or expanded development on the south side of the Post Road, where many lots are quite shallow, must be carefully evaluated in terms of impact on adjacent residential areas and the streetscape.

6. Proposed Implementation Actions (see Part IV)

- a. Amendment of the zoning regulations to establish a minimum frontage for business uses and to refine the list of permitted uses to exclude automotive uses.
- b. Development of a detailed design concept for the public right-of-way and private frontage to be implemented in a staged, cooperative program by the Town, the State DOT and adjacent property owners.
- c. Application of the special permit provisions for mixed use development proposed in the Phase I report by reducing the minimum site area to 60,000 square feet.

Planning Unit #2

1. Existing Land Use

This unit consists of the eastern portion of Boston Post Road in Larchmont and is fairly uniform in character, consisting primarily of low density commercial uses, the majority of which are tied to automobile sales and service. These retail uses consist of three new car dealers, two gasoline stations with repair facilities, a tire warehouse, and an electric and electronic service shop. Also in this area are two public service facilities (the Village garage and a Con Edison substation), a restaurant and a bank. Most of these parcels contain single story structures and some off-street parking. A few rely on on-street parking or the public lots in the older business core at Chatsworth Avenue. Although most of the automotive uses have on-site parking areas, the sites are too small to effectively accommodate the parking demands of customers, employees and inventory. Consequently, virtually every available square foot is used for this purpose, including adjacent streets.

Like Planning Unit 1, a distinct boundary is formed on either side of the business area by solid residential areas of high quality. The northeast boundary follows the Larchmont boundary at East Creek across Boston Post Road. The southwest boundary is drawn at Thompson Place, beyond which is the older, more densely developed business district.

2. Current Development Policies

Most of the area in Planning Unit 2 lies within the Village's RB (Retail Business) District. However, several parcels within this zone extend into the two R-5 (Residential, 5,000 square foot lot) districts immediately adjacent. The 1966 Master Plan does not specifically address this area in detail, although it focuses on the more densely developed "core" of the Boston Post Road business area which comprises Planning Unit 3. No distinction is made in the Master Plan between this area and the higher density core. Nonconforming uses consist of business properties which extend into the R-5 District, although a clear demarcation between residential and commercial uses has been maintained in most instances.

Recently, policies aimed at restoring the ecological values of East Creek have been promulgated in the joint Larchmont/Mamaroneck Local Waterfront Revitalization Program. These policies must be considered as the impacts of various land use policies and proposals are reviewed.

3. Development Issues

The primary issue in this area is to control potential adverse impacts from the automotive uses on adjacent residential properties, traffic flow and appearance. Since there is virtually no vacant or under-utilized land in this area, development pressures will be in the form of intensification of use or infringement on the adjacent residential districts.

4. Proposed Land Use Policy

Most of the automotive uses are on sites which are inadequate by today's standards. The extent of such existing uses precludes eliminating them from the zoning district. However, the area within which they are allowed should be kept to a minimum and all regulations dealing with use, access and design strictly enforced so as to protect adjacent residential areas and minimize traffic conflicts and visual blight.

5. Site Planning and Design Considerations

- a. Screening adjacent to residential properties must be maintained.
- b. The impact of business uses on residential side streets in terms of vehicular access and visual intrusions must be considered during site plan review.
- c. The impact of off-street parking and outside storage of automobiles which contributes to the clutter in this area should be addressed.

6. Proposed Implementation Actions

- a. Include auto sales as a special permit use, rather than a permitted use in the RB District and redelineate the district to confine it to the area between Bronson Avenue and the Village line.
- b. Require site plan review as part of any external change.
- c. Develop detailed design approach for street furniture and parking lot landscaping.

Planning Unit #3

1. Existing Land Use

This area is the commercial core of Boston Post Road, situated near the geographical center of the Village. It includes the properties on both sides of Boston Post Road, Chatsworth Avenue and Larchmont Avenue, and on the southeast side of Addison Street. Compared to the other areas along Boston Post Road, this area reflects a higher density of development and a large amount of mixed commercial and residential uses. Principal non-residential uses include general retailing, professional and general offices and personal services. Principal residential types are the apartments located above many of the non-residential uses, with three buildings providing five or more apartments. Buildings are often two or more stories tall, on-site parking is limited to only a few locations, and often several different types of commercial uses occur on the ground floors of these buildings.

The largest building in the unit is the 5 story Albee Court, a multi-family apartment building on Boston Post Road and Larchmont Avenue with a variety of office and service uses on the ground floor frontage

on Post Road. Other large parcels in the unit include the Grand Union supermarket and the Post Office. These three buildings have off-street parking for the needs of residents, customers, and employees, respectively. Another large parcel contains the Village Municipal Building. There are also a synagogue, a bank, and several small eating and drinking establishments, along with two public parking lots to the rear of the Post Road properties. Automotive retail uses are limited to a gas station and body repair shop and a tire dealer, both located on adjacent properties in the south west end of this area near Manor Place.

An abrupt transition from commercial to residential uses occurs along most of the boundary on either side of the Post Road and at Chatsworth and Larchmont Avenues at their intersection with Addison Street. The northeast boundary is defined at Thompson Place, where lower density commercial uses, dominated by automotive retail, begin; on the southeast, Manor Place and the cemetery form the boundary between the retail core and the Post Road's only residential area.

2. Traffic Considerations

Capacity analysis and field observations indicate that the peak hour volumes of traffic through the intersections of these major roads are not the primary cause of congestion in this vicinity. Factors adversely impacting traffic are the exclusive pedestrian phases with long crosswalk lengths, large intersection widths requiring longer clearance intervals, conflicts from parking maneuvers, and misalignment of lanes. These elements cause delays and substantially reduce the levels of service of the intersections.

Crosswalk length, lane alignment and width of the two intersections can be improved through redesign of the lanes and sidewalks within the existing right-of-way. Upon completion of these physical improvements, signal timing can be optimized to provide the highest possible level of service.

3. Parking Considerations

Planning Unit #3 is a densely developed retail area which was the primary subject of the parking study. Approximately 675 on-street and off-street municipal parking spaces are located within this area. Many of the businesses offer some off-street parking, although the majority of these spaces are utilized by employees.

Municipal parking lots are controlled with a combination of permits available only to Village or Town residents and a 3 hour parking restriction for vehicles without a permit. Because these municipal lots are surrounded by private businesses with limited parking accessed through the municipal lots, private and municipal space are not easily distinguished. Although 157 municipal spaces were inventoried in the three lots, 256 spaces were categorized as municipal spaces in these lots for the purpose of the survey.

The parking survey found that on-street and off-street parking is heavily utilized on both weekdays and Saturdays, with the exception of the Saint Augustine Lot. There is an apparent need for the provision of additional municipal parking facilities within the Planning Unit #3 area.

It is important that the parking supply include a sufficient "cushion" in excess of the necessary spaces to allow for the dynamics of vehicles moving in and out of parking stalls and to reduce the time to search for the last few available spaces. This cushion, or level of parking service, also allows for spaces lost by restricting lots to certain users, misparked vehicles, snow cover, and minor construction. Thus, a supply of parking operates at peak efficiency when occupancy is 85% to 95%. When occupancy exceeds this level there may be delays and frustration in finding the last few available spaces. Thus, the parking supply may be perceived as inadequate even though there are spaces available.

For this reason it is normally acceptable to size the parking supply approximately 5% to 15% over actual parking demand. This cushion of parking is obtained by comparing the parking demand with an "effective" parking supply instead of the actual supply. The "effective" parking supply is assumed to be 90% of the surface lot spaces, and 95% of on-street spaces.

Municipal parking in the heart of the C.B.D. exceeded capacity during the Saturday survey. These 460 spaces were approximately 108% occupied, falling short of the 10% "cushion" desired for an adequate level of parking service by 18%. Approximately 80 additional parking spaces are necessary to ease the demand on the existing municipal parking system.

To accommodate new development, approximately 20 additional off-street parking spaces are also needed.

4. Current Development Policies

This area is almost entirely zoned RB, with some parcels along the southeast side of Boston Post Road split into the RB and R5 Districts. The 1966 Master Plan and current zoning anticipated continued commercial uses in this area. The current zoning, particularly in regard to setback requirements, reflects the older, more densely developed nature of this business area. The impact of recently imposed parking requirements is revealed by the lower development density of two newer buildings, the Larchmont Savings Bank and the Grand Union supermarket. The Post Office is the other less densely developed parcel. Several of the uses permitted in residential districts and also allowed in the RB District are present within this study area. The cemetery and the automobile body shop are non-conforming uses in the RB District, and the rear of the tire shop and the off-street parking lot serving Albee House are non-conforming uses in the R5 District. Ten parcels exceed height and/or density maximums.

Since the 1966 Master Plan, a number of recommended improvements have been completed which enhance the appearance of the area. However, two suggestions for expansion of the two public parking lots have not yet been implemented.

5. Development Issues

The major issue in this area involves the impact of potential development of additional floor space on the already tight parking situation. Although the zoning regulations require on-site, off-street parking for each use, few parcels are large enough to meet this requirement since most of the area was developed under earlier standards. Thus, expansion of existing structures has been effectively precluded, even when such construction might upgrade the existing building or provide more efficient operation. There are no vacant parcels affected by this provision. On the other hand, in the absence of this restraint, a substantial amount of new floor area could be developed. For example, if all of the one-story buildings (except Grand Union) were to add a second story, as permitted under the existing RB regulations, some 65,000 square feet of floor area could be developed. Under the existing parking regulations, this floor area would require 260 parking spaces. The total development potential, based on a floor area ratio (FAR) of 1.6 (two stories at 80% coverage) would add 188,000 square feet of floor area requiring 752 parking spaces. The latter potential is unrealistic, as is strict application of the parking requirement. However, there is no doubt that any substantial increase in floor area will adversely affect the parking situation. Unlike the case in the railroad station business district, there are few underutilized parking spaces that can be used to meet future demands.

It should be pointed out that the actual ratio of parking spaces to floor area is only two-thirds that required under the zoning regulations. There are approximately 1,100 parking spaces serving a grand total of 414,000 square feet of floor area on all levels--a ratio of 2.65 spaces per 1,000 square feet as opposed to the required 4.0 spaces.

Although the existing parking situation is somewhat tight, this suggests a lower ratio may be appropriate in this area.

6. Proposed Land Use Policy

The core business area should remain primarily a retail/personal service center which encourages internal pedestrian circulation. The present intensity of development should be maintained in terms of height and floor area. A major portion of the responsibility for providing necessary off-street parking must be assumed by the Village to preserve the character of the business area and to provide parking in the most efficient manner. However, any waiver of on-site parking requirements should be tied to the provision of additional public parking spaces or should only apply to very minor additions of floor space.

7. Site Planning Considerations

Due to the diversity of the core area, there are many small scale site planning factors for each potential development which are applicable to the particular site. In general, the continuity of ground level frontage should be maintained while parking and site access should be oriented to side streets. The design of sidewalks, street furniture, storefronts and signs is particularly important in a business area where pedestrian traffic is high.

8. Proposed Implementation Actions

Zoning provisions similar to those recommended in Phase 1 for the proposed RC District should be implemented to deal with parking as well as design guidelines. However, the criteria under which on-site parking can be waived should be revised to reflect the different nature of parking supply and demand.

Planning Unit #4

1. Existing Land Uses

This unit is primarily an area of homogeneous, low density residential dwellings, with a more open character than any of the other planning units, due to two cemeteries, a park, three vacant residential parcels and two horticultural nurseries within the study area.

The abrupt transition from residential to non-residential uses at Manor Lane marks the northeast boundary of this unit. The southwest boundary occurs where more intensive commercial uses begin beyond the nurseries. The boundaries on either side of the Post Road correspond to the rear lot line of the parcels facing the Boston Post Road.

The majority of the developed parcels consist of single family homes. Two such structures are currently used as professional offices. Two other professional offices are accessories to existing residential uses. The two nurseries are nonconforming uses which occupy the largest parcels in this unit. Another large parcel is Kane Park. On-street parking is minimal, and well maintained parks and private properties add to the attractiveness of the area.

At the center of recent controversy in this area is the continued operation of the two nurseries on Post Road. Erosion and siltation of the Pine Brook and Premium River, and accompanying ecological and visual blight, are two principle issues which were identified in the 1986 Larchmont-Mamaroneck Local Waterfront Revitalization Program. Erosion problems were identified as resulting from an unstable, improperly designed vertical embankment that lines the rear and side of one of the nursery properties along the Pine Brook. The erosion results in excessive siltation in the Pine Brook and the Premium River into which it flows. This contributes to the degradation of the Premium Wetland and Wildlife Habitat Complex, which has been designated by local law as a "critical environmental area", and the visual quality of the area.

2. Current Development Policies

This section of Boston Post Road is within either the Village's R-5 or R-7.5 Districts. The 1966 Master Plan examined this area with respect to the possible development of the vacant parcels and the two nursery properties, without sacrificing the "suburban quality and landscaped character". The plan acknowledges that new single family dwellings may not be desirable if located along the Post Road, given the traffic volumes. Alternatives to this were put forth for consideration, although rezoning was not specifically proposed. Office buildings were suggested for the smaller vacant parcels and part of the nursery properties, subject to lot coverage, off-street parking and strict landscaping requirements. It was suggested that the nursery properties could also be incorporated into the proposed Premium River Preserve.

In the 1986 Local Waterfront Revitalization Program (LWRP), residential development of the nursery properties is encouraged, in the event that these parcels become available for development, under conditions imposed by the Village to ensure that any resultant adverse impacts upon the Pine Brook and Premium River will be properly identified and mitigated so that the quality of these ecologically important habitats would be maintained or improved. The Village thus can require site plan review and environmental impact statements to insure that any proposed development of these parcels is consistent with policies adopted concerning wildlife habitats, wetlands, flood control, siltation and erosion control, and scenic quality, as specified in the LWRP.

3. Development Issues

There are two distinct issues in this unit:

- a. How can the integrity of the residential area of the Post Road be maintained and are any changes in its boundaries appropriate.
- b. What nature and intensity of use is appropriate for the nursery properties should the present use be discontinued.

4. Proposed Land Use Policy

The low density residential/park section of the Post Road is a sound area which provides important visual and functional relief from the otherwise continuous business development. No new non-residential uses should be permitted. Clustered-residential development is appropriate on the vacant parcels, including the nursery parcels, subject to careful review of environmental impacts. Such clustering on the nursery parcels, as permitted in the R-7.5 District, will allow the more sensitive portions of the site, adjacent to the Pine Brook and Premium River, to be preserved and restored as part of the natural habitat area while concentrating townhouses near the Post Road.

5. Site Planning and Design Considerations

The existing residential scale and character of this area must be retained, if cluster development is undertaken, by adhering to the same scale of buildings and landscaping.

6. Proposed Implementation Actions

- a. The cluster provisions of Local Law No. 6 of 1977 should be made applicable along the Post Road by reducing the minimum site area to two acres.
- b. The Zoning Law should be amended to more clearly spell out the conditions under which home occupation offices are permitted to prevent the gradual conversion of existing residential structures to non-residential use.

Planning Unit #5

1. Existing Land Use

Planning Unit #5 covers the southwestern end of Boston Post Road in both the Village and Town, and consists of a mixture of residential and commercial uses developed at varying densities. Several parcels have residences in the same structures as non-residential uses. Detached structures, housing from one to three families, are isolated by intervening commercial uses and vacant lots. In some instances lots in use for either residential or business uses have frontage on both Boston Post Road and streets parallel to it. Multi-family housing exists at two locations. At Dillon Road there is a five story apartment house with a ground level commercial use along the Post Road. Across from the Lorenzen Street intersection is a new, lower density, multi-building, two story apartment/condominium complex built on a large parcel which was formerly a part of one of the nursery properties. In the Town, automotive retail and servicing are the principal business uses, while offices and restaurants are the dominant business uses in the Village.

Several vacant lots exist along with an undeveloped street right-of-way (Van Guilders Street). A number of these lots, which may be owned by adjacent property owners, along with the rear of the double frontage residential lots on Post Road, appear neglected or poorly maintained. This invites littering and outright dumping of household debris and trash, such as old mattresses. In one vacant lot are two cars which, if not abandoned, appear as if they are incapable of being moved under their own power.

This planning unit is bounded on the southwest by a continuation of the automotive commercial land uses into the City of New Rochelle. The northeast boundary, drawn between the nursery and the automotive repair shop, marks the transition into commercial uses from the single family residential uses of Planning Unit #4. The boundary is defined along the rear lot lines of the parcels fronting on the Post Road. In this area, changes between commercial and residential land uses are distinct in some places and indistinct in others. The latter situation occurs in the lots with double frontage. As a result, residential and commercial uses can extend into districts where they are not permitted.

2. Current Development Policies

This section of Boston Post Road crosses through the Town and Village, and six zoning districts. Most of Post Road is within either the Town's B District or the Village's RB District. The rear of properties on the northwest side of Post Road in the Town are in the R-7.5 District. In the Village, the rear portions of the properties between Winans Street and the Van Guilders Street right-of-way are in the R-5 District, as is the portion of the Roy Rogers Restaurant property fronting on Dean(e) Place. In the northeast end of this area, both sides of Boston Post Road are within the Village's R-7.5 District.

The 1966 Master Plan for the Town briefly describes the commercial uses in this area, whereas the 1976 Update provides a more detailed examination of land uses and aesthetic characteristics. Continuation of commercial land uses is supported in both documents. The Village's 1966 Master Plan briefly describes this area and recommends continued commercial land uses. That document suggested office buildings and parkland for the 6.5 acre parcel now occupied by the Pine Ridge apartment/condominium development.

The rear portion of the bowling alley property is a non-conforming use within the Town's R-7.5 District, and several residential structures (with or without first floor retail uses) are non-conforming uses within the Town's B District. The offices at the Post Road-Lorenzen Street intersection along with the building housing the automotive repair and service shop, the house services shop and the real estate office in the adjacent parcel are non-conforming uses within the Village's R-7.5 District. The rear portions of other commercial properties between the Van Guilder Street right-of-way and Lorenzen Street also extend into the R-5 or R-7.5 Districts of the Village.

3. Development Issues

Development issues in this area relate to its diversity in uses and property size.

- a. The block on the south side of the Post Road, east of Dillon Road, is occupied by residential uses on narrow, steep lots although it is zoned for business use. Most of these individual lots are not suitable for business uses but the general environment is not residential in nature.
- b. The mixture of auto related business uses on generally small lots creates special problems in terms of appearance, traffic flow and impacts on the surrounding neighborhood.

4. Proposed Land Use Policy

Business development on small parcels should be discouraged and opportunities for multi-family residential use, as an alternative, should be provided instead. Incentives should be developed to encourage assemblage of small lots for appropriate business or intensive residential use.

5. Site Planning and Design Considerations

- a. Reduce or eliminate new curb cuts on the Post Road.
- b. Provide buffers from adjacent residential areas.
- c. Develop landscaping and street furniture along the street edge that requires minimal maintenance.

6. Proposed Implementation Actions

- a. Redesignate all or part of this area as SB Service Business, as suggested in Phase 1, and permit multi-family residential development subject to a special permit.
- b. Carry out various actions proposed in the Post Road Design Concept to enhance the visual quality of the area.

B. POST ROAD DESIGN CONCEPT AND POLICIES

To enhance the existing visual quality of the Post Road and improve the quality of future design is a complex effort. It involves the following elements:

- Agreed upon guidelines and policies for the design of both public and private facilities.
- Preparation of illustrative material to demonstrate the techniques that can be used to accomplish the desired objectives.
- A comprehensive concept which sets forth a total design plan for the Post Road.
- Legislation which assigns responsibility for design review, sets forth procedures for such review, and establishes design standards to serve as review criteria.
- An allocation of public funds for a phased implementation of public design elements.
- Competent review of plans prepared by private developers and public agencies, including negotiations to achieve specific objectives.

1. Design Guidelines

The concern of community design is not with the architecture of individual buildings but rather with the relationship between one building and another, between buildings and the street and adjacent open space, and with all of the various elements which together establish the character of an area, including the following:

a. Scale

The scale of an area is established by the height of the buildings, the distance between them, the width of streets and the relationship of open spaces. In general, radical departures from the basic scale of an area should be avoided since they tend to disrupt its basic character. Occasionally such a change may be used to emphasize an important focal point, but in such cases particular care should be taken so that it complements rather than competes with the general scale. The marked difference between the easterly end of the Post Road (Planning Unit 1) and the older Village Hall business area (Planning Unit 3) is a result of the different scale of distance between and height of buildings. The taller buildings and smaller distance between them results in a much more urban character in the latter area.

b. Materials

The predominant materials of construction are a major factor in establishing the image of an area. Brick, wood stone, etc. are warm materials, often associated with residential neighborhoods and suited to small scale development. Steel, glass and concrete are cold materials, more urban in character. They should be used sparingly so as not to overwhelm the small, basically residential scale of development in the Village core area.

A second area where choice of materials plays a vital role is in the "floorscape". Too often, the choice of materials here is limited to asphalt or concrete. A great deal of visual interest can be created by occasional use of the varied floor textures provided by brick, cobblestone, asphalt block, and exposed aggregate or broom-brushed concrete. These materials provide a relief from the usual flatness by breaking up the surface into small scaled units and providing interesting shadow lines. However, because of their added expense, they are usually used sparingly, in areas of particular importance. This technique has been used somewhat in the Village center area and is most appropriate where pedestrian traffic is high.

c. Color

Some of the colors used in buildings, particularly in highway oriented portions of the Post Road, are so bright that they violate the fundamental rule of subordinating the individual elements to the street as a whole. There is almost no way that extensive areas of orange, purple, bright red, yellow, aqua, and other loud or garish colors can become part of a unified whole, except, possibly, at an amusement park. If these colors are used at all, they should be used only as very occasional accents.

The one obvious exception to this rule is in the use of flowers--these small bits of bright color can make a street "come alive" or accent a specific focal point. Otherwise, only colors which will blend well should be used as the dominant theme on the street: white, greys, blacks, natural wood shades, light and dark earth colors, such as ochres, tans, and browns, and darker reds or greens. The same color considerations apply to buildings, signs, and all other elements of the streetscape.

d. Horizontality

The Post Road streetscape is horizontal, and any element which interrupts this horizontal feeling tends to disrupt the street's unity. Unnecessary verticals only add visual chaos to the streetscape and should be removed. Specific offenders include

telephone and electric poles, freestanding lights and signs which could be attached to a building or to another pole, thus reducing the forest of verticals. Telephone and electric poles, lines and miscellaneous equipment are serious offenders. Even if other items such as signs and colors are improved, these element alone can seriously disrupt the visual beauty of the street.

e. Signs

In general, signs should, be a subordinate part of the landscape rather than a dominant feature, and adjacent signs should be harmonious in color and scale. At present, many signs clash with each other and thus detract from the street's basic unity. This is particularly true towards either end of the Post Road where signs are also more prominent. While there may be nothing inherently wrong with most types of signs (even freestanding traffic signs or overhanging signs, if carefully handled, can be given their place in the streetscape), the problem is one of subordinating these elements to the rest of the street's development. If each store simply puts up the biggest and most colorful sign it can, the result, aside from visual disorder, is a diminution of the impact of the individual signs. Yet properly designed signs could add interest to the street without destroying and overpowering the whole composition. Signs need not be of the same size or design, even on adjacent buildings, but should be designed so as not to drastically clash with each other or the building to which they relate.

f. Landscaping

The appropriate use of trees, shrubs, and ground cover can provide continuity, character and warmth to even the most drab street. Its absence can prevent even outstanding building design from achieving its potential affect. Effective landscaping does not have to be elaborate. A simple grass strip planted with hardy trees does wonders for almost any street. The trees enclose the street and provide an interesting "roof" in scale with most low-rise development. Such a treatment east of the Village would dramatically change the streetscape which is now wide open with virtually no enclosure.

A tree or planter will catch the eye and supply identity to a significant location. Shrubs and foundation planting, while varying in species and size, provide a basic character and add interest to the streetscape. Landscaping can be particularly useful to screen and soften large parking lots such as those in the shopping centers east of the Village.

2. Design Techniques

The following design techniques are applicable in various locations along the Post Road and are illustrated on the following pages.

Parking Lot Buffer: The wide open, barren aspect along the street at the eastern end of the Post Road can be substantially relieved by a combination of a row of trees to provide an "edge" to the street and low shrubs to screen large parking lots. Recommended species of street trees are also illustrated.

Parking Lot Planting: Visual relief in large parking lots can be provided by trees spaced to provide shade and add vertical scale.

Curb-cut Definition: Improvements in traffic safety and visual appearance can be achieved by narrowing overly wide curb-cuts and defining the vehicular space with planting.

Entrance Signs: Entrances to the Town and Village can be suitably identified with an attractive sign that establishes a design theme for all public signs.

Auto Dealer Landscaping: Auto dealers represent a difficult design challenge. In most cases, the lots are small and crammed with cars almost on the sidewalk. The appearance of the street suffers and the sales message of the dealer is lost in a sea of vehicles. A design which addresses both problems is to highlight several cars on a raised platform while screening the rest of the lot with low landscaping. Both aspects of this approach are required--as opposed to just elevating a few cars as at the Volkswagen dealer on the New Rochelle line.

Crosswalk Identification: Well identified crosswalks serve as a safety measure and can, in selected locations, be used as a design feature as well. The safety feature is especially important at mid-block locations where sidewalk design encourages a crossing, such as opposite the Post Office entrance. Special paving can be used at locations of heavy pedestrian use and focal points, such as on the Post Road adjacent to the Village Hall.

Village Sidewalk Design: In the area of heaviest pedestrian traffic, the older core business area adjacent to Village Hall, a number of measures have already been taken to improve its appearance. Additional design elements such as tree grates, brick paving along the curb, benches at key locations and colorful flowers in the planters will enhance the previous efforts.

Bus Stop Design: The appearance and function of bus shelters and bus stops can be enhanced by paving the loading area between the sidewalk and curb, perhaps with a decorative material, and providing integrated lighting and route information standards.

Village Hall and Park: The grassy area on the corner adjacent to Village Hall and Village Hall Park* on Larchmont Avenue comprise the only open space of any size in the core business area. The corner can be enhanced by more intensive landscaping and flowers, an expanded sitting area and a well designed bus shelter. Access to Village Hall Park directly from the Post Road, by defining a walkway at the rear of Village Hall, would encourage pedestrians to use the park and increase activity within it.

Kane Park: This passive park provides visual relief along the Post Road. Its function would be enhanced by lighting along the interior pathway and installation of another path just inside the trees on the Post Road to accommodate pedestrians along the street.

3. Post Road Design Concept Plan

Map No. 7 illustrates an overall plan for the Post Road indicating the locations where various design techniques are appropriate to deal with existing problems and further specific objectives. Implementation of the entire plan would require a vast effort by both public agencies and private property owners. The cost of the public portion alone would be over \$400,000, as estimated in the following table. While it is unlikely (and not necessarily desirable) that a major portion of the plan be undertaken at one time, it is important that a commitment be made to initiate the plan, especially if owners of private property are to be convinced to participate. Therefore, it is recommended that a four year program to undertake high priority improvements be adopted by both communities. Such a program is described in Part IV.

*Recently renamed "Constitution Park".

Preliminary Cost Estimate

DESIGN IMPROVEMENT PROGRAM (See Map No. 7)
 (Public Costs Only, in 1987 dollars)

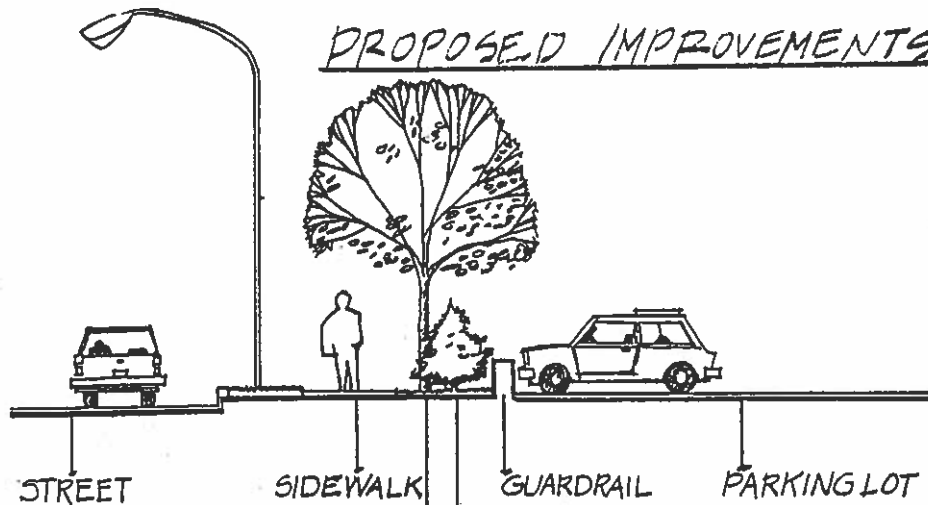
| Item | Unit Cost | Village | | Town | |
|---|-----------|---------|-----------|-------|-----------|
| | | Qty. | Amt. | Qty. | Amt. |
| a. Street Trees: w/grate w/o grate grate only | \$600ea. | 20 | \$12,000 | - | - |
| | 325ea. | 78 | 25,350 | 126 | \$40,950 |
| | 300ea. | 40 | 2,000 | - | - |
| b. sidewalk: concrete brick | 4.65/s.f. | 2,600 | 12,090 | 4,600 | 21,390 |
| | 8.50/s.f. | 7,800 | 66,300 | - | - |
| c. Curb: granite concrete | 20/l.f. | 1,000 | 20,000 | - | - |
| | 10/l.f. | 650 | 6,500 | 1,150 | 11,500 |
| d. Crosswalk: Pavers Painted (inc. sign) | 20/s.f. | 2@ 700 | 28,000 | - | - |
| | 1/s.f. | 3@ 400 | 1,950 | - | - |
| | +sign | | | | |
| e. Lighting (pedestrian) | 1,000ea. | 42 | 42,000 | - | - |
| f. Bus Shelter: New Redesigned | 5,500ea. | - | - | 1 | 5,500 |
| | 2,500ea. | 1 | 2,500 | 1 | 2,500 |
| g. Benches | 500ea. | 10 | 5,000 | - | - |
| h. Signs: directional entrance | 150ea. | 10 | 1,500 | 5 | 750 |
| | 500ea. | 2 | 1,000 | 1 | 500 |
| I. Shrubs and flowers | lump sum | - | 10,000 | - | - |
| SUBTOTAL | | | \$246,190 | | \$83,090 |
| Design and Contingencies @ 25% | | | 61,547 | | 20,772 |
| TOTAL (rounded) | | | \$308,000 | | \$104,000 |



①. PARKING LOT/
LANDSCAPE BUFFER

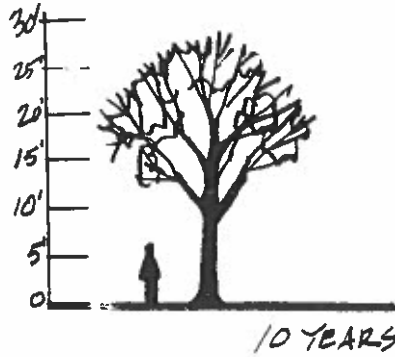
EXISTING CONDITIONS

- OVERHEAD UTILITY LINES DOMINATE AREA,
- NO ATTENTION PAID TO PEDESTRIAN/ DRIVER VIEW.
- OPEN VIEW TO PARKING LOT.
- NO STREET TREES.



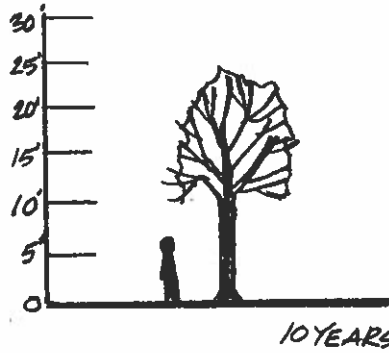
PLANTING WITH EVERGREEN SHRUBS TO FORM DENSE SCREEN BETWEEN PARKED CARS AND PEDESTRIAN.

TALL DECIDUOUS TREE PROVIDES OVERHEAD CANOPY PEDESTRIAN SCALE.



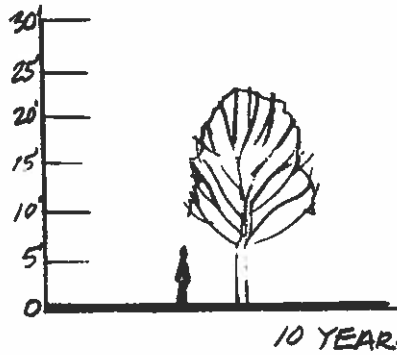
VERY HIGH-HEADED, FINE TEXTURE
 FILTERS LIGHT
 HEIGHT - 50-80'
 SPREAD - 40'

HONEY LOCUST, GLEDITSIA TRIACANTHOS



RESISTANT TO LOW MOISTURE IN SOIL
 BLOOMS IN SPRING W/WHITE FLOWERS
 FALL FOLIAGE - RED/ORANGE
 HEIGHT - 30-50'
 SPREAD - 20-35'

BRADFORD PEAR, PYRUS CALLERYANA



BEAUTIFUL ROUND SHAPE
 EXCELLANT SHADE TREE

HEIGHT - 50-60'
 SPREAD - 20-35'

LITTLE-LEAF LINDEN, TILIA CORDATA

RECOMMENDED STREET TREES

2.

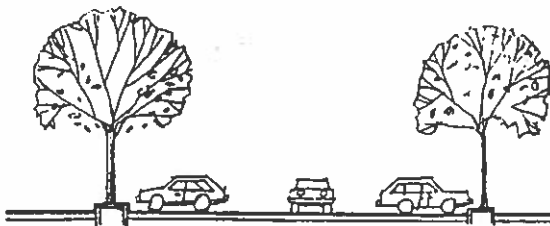
PARKING LOT -
TREE PLANTING
LAYOUT



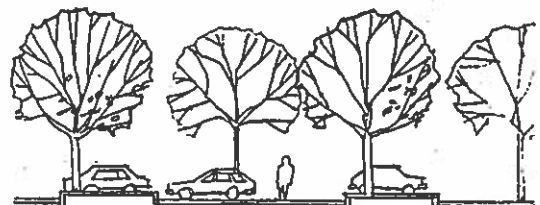
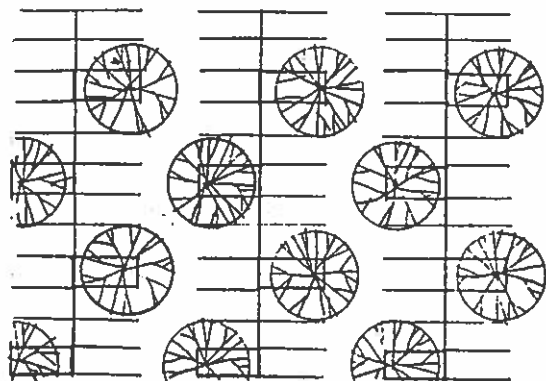
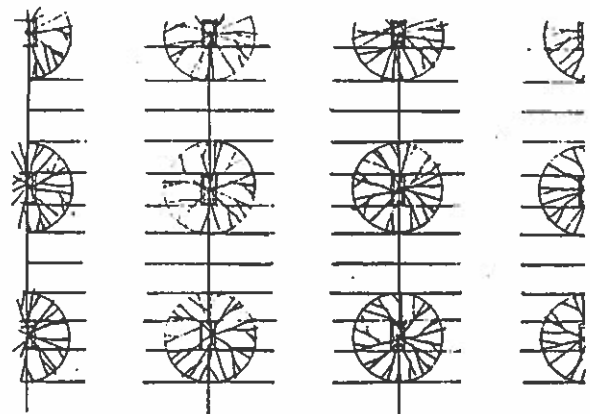
EXISTING CONDITIONS

VAST PARKING LOT - NO VISUAL RELIEF

PROPOSED IMPROVEMENTS



50' x 60' SPACING, PLANTING IN CUT
OUT SQUARES, NO LOSS OF
PARKING SPACES.



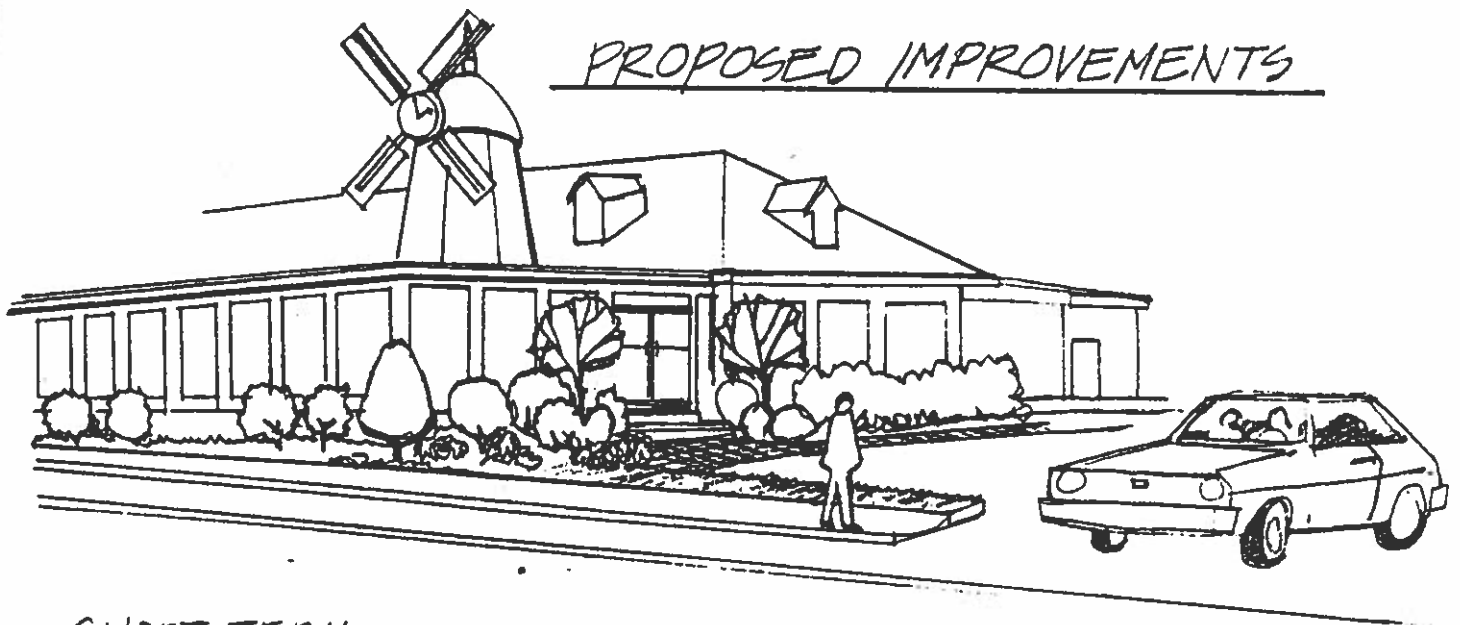
40' x 40' SPACING,
VERY UNIFORM COVERAGE.

3. CURB CUT



EXISTING CONDITIONS

- CURB CUT CREATES CONFUSION AND TRAFFIC HAZARD.
- NO DEFINITION OF ENTRANCE
- LACK OF AMENITIES - WALKWAY, TREES



PROPOSED IMPROVEMENTS

SHORT TERM:

EXTEND CURBS, KEEP PARKED CARS AWAY FROM BLDG. ENTRANCE

LONG TERM:

ENHANCE ENTRANCE WITH INTERESTING PAVING MATERIALS, LANDSCAPING.



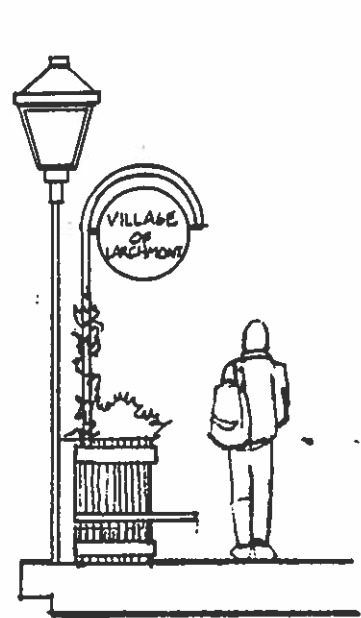
4A,

VILLAGE
SIGNAGE
AND
LIGHTING

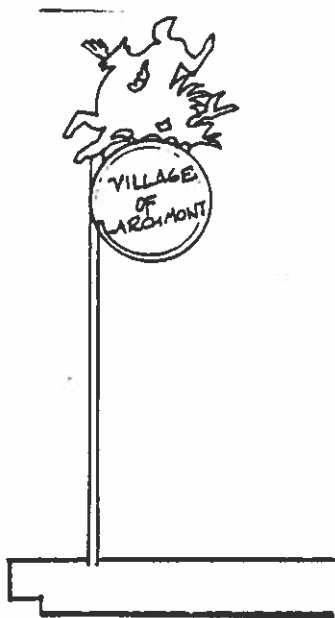
EXISTING CONDITIONS

- SIGN OVERLOAD
- POORLY MARKED PEDESTRIAN CROSSING
- CAR DEALER LACKS ADEQUATE SCREENING (SEE NEXT PAGE)

PROPOSED IMPROVEMENTS



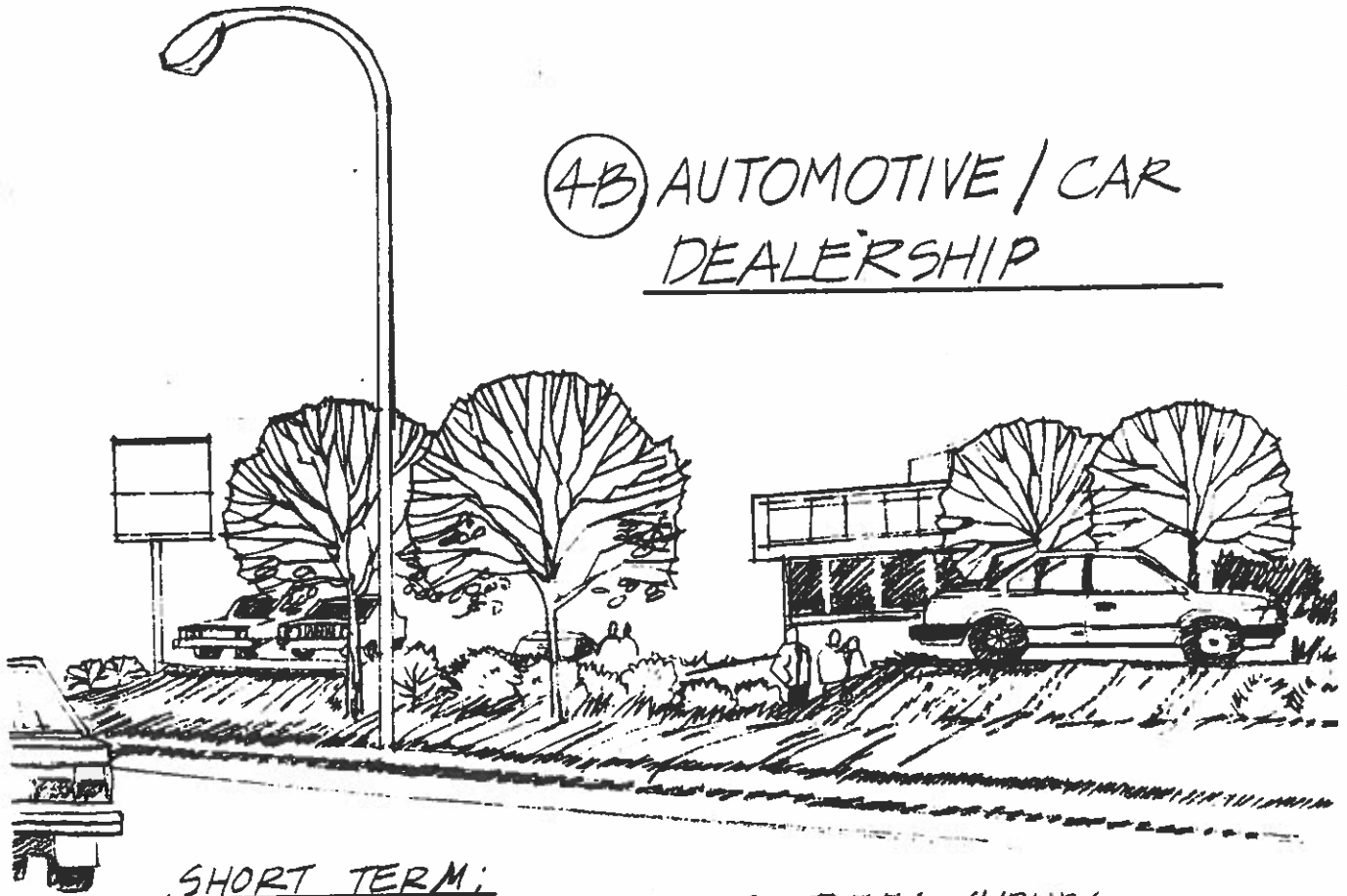
STREET FURNITURE UNIT
SEATING, LIGHT, SIGN.



USE EXISTING VILLAGE
DESIGN THROUGHOUT SIGNAGE



(4B) AUTOMOTIVE / CAR
DEALERSHIP



SHORT TERM:

ADD MINIMUM PLANTINGS OF TREES, SHRUBS

LONG TERM:

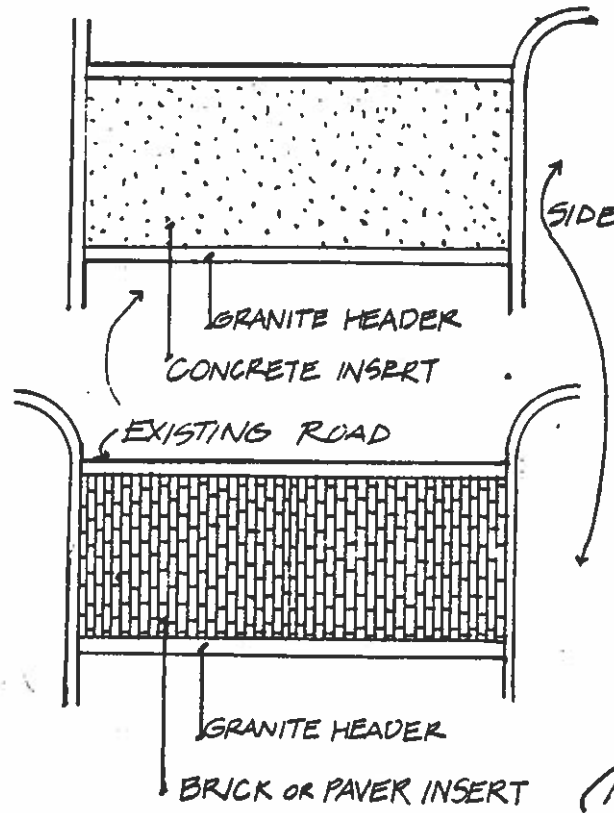
ISOLATE 2-3 CARS FOR DISPLAY ENHANCED WITH
RAISED BERM, LOW SPOT LIGHTING, LANDSCAPING.

5. CROSSWALK MODIFICATIONS



- EXISTING CONDITIONS
- POORLY DEFINED PEDESTRIAN CROSSING.
 - STREET LIGHTS DO NOT ENCOURAGE PEDESTRIAN USE.

PROPOSED IMPROVEMENTS



- SHORT TERM:
PAINT NEW PEDESTRIAN CROSSWALKS
- LONG TERM:
CHANGE PAVING MATERIAL IN CROSSWALK TO BE COMPATIBLE WITH NEW SIDEWALK TREATMENT

(PLAN VIEW DRAWINGS)

6. VILLAGE STREETScape



EXISTING CONDITIONS

- STREET LIGHTS OUT OF SCALE WITH BLDG. & PEDESTRIAN.
- LACK OF PEDESTRIAN-ORIENTED AMENITIES
- SIGNS DO NOT COMPLIMENT ARCHITECTURE.

PROPOSED IMPROVEMENTS.

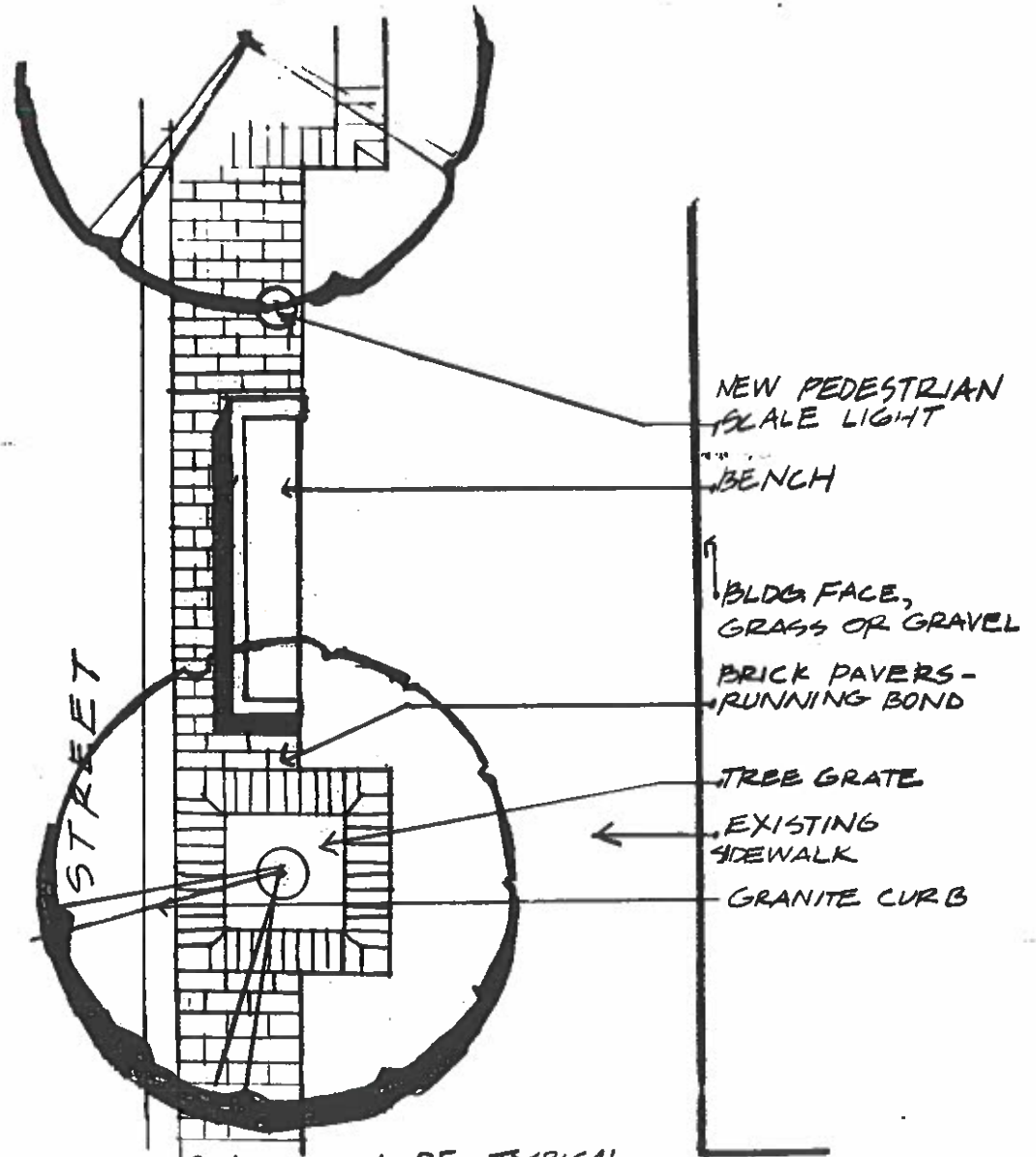


SHORT TERM:

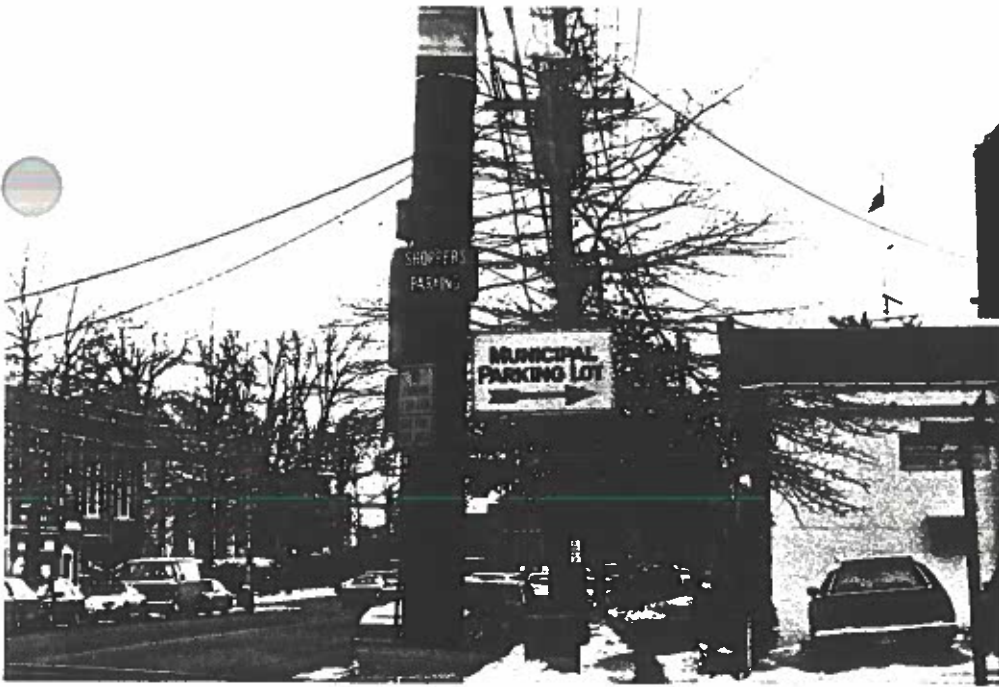
IMPROVE CURB. ADD TREES & NEW LIGHTING.
ADD UNIFYING PAVING SURFACE
ALONG CURB. REPLACE SIGNS
WITH COMPATIBLE STYLE.

LONG TERM:

BURY UTILITY LINES
IN VILLAGE RETAIL.



PLAN VIEW OF TYPICAL
 VILLAGE AREA SIDEWALK IMPROVEMENT
 NOT TO SCALE

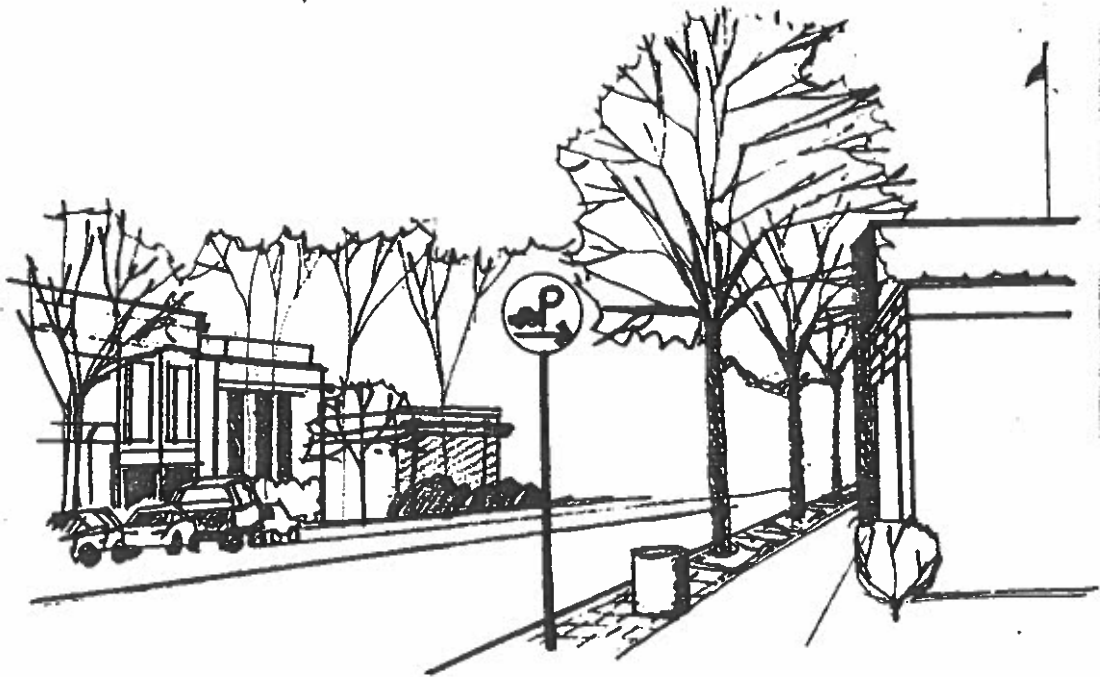


7. PARKING SIGNAGE

EXISTING CONDITIONS

- PARKING SIGNS ARE REDUNDANT AND ADD CONFUSION AND CLUTTER.
- UTILITY LINES AND POLES DOMINATE AREA.

PROPOSED IMPROVEMENTS



SHORT TERM:

INTRODUCE UNIFORM SIGNS FOR PARKING

LONG TERM:

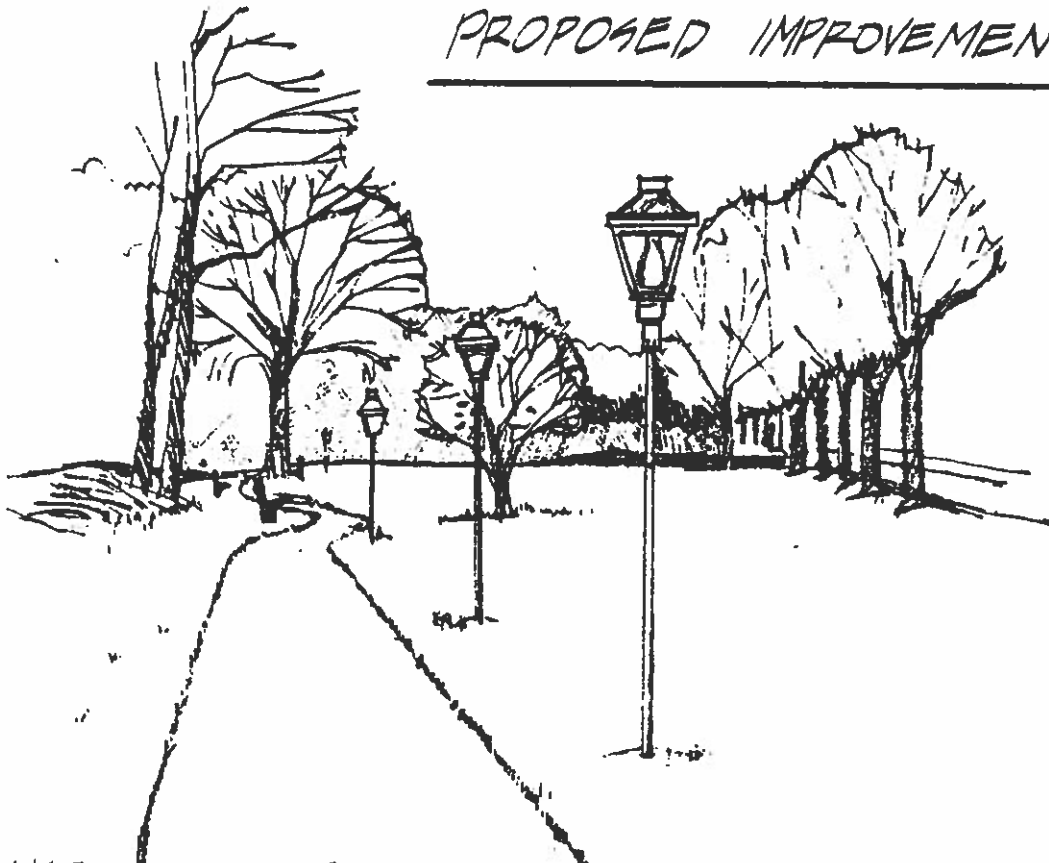
BURY UTILITY LINES

8. KANE PARK

EXISTING CONDITIONS
BEAUTIFUL STROLLING
PARK BUT FOR DAY-
USE ONLY.



PROPOSED IMPROVEMENTS



MAKE PARK SAFER AT NIGHT
INCLUDE PEDESTRIAN SCALE LIGHTS

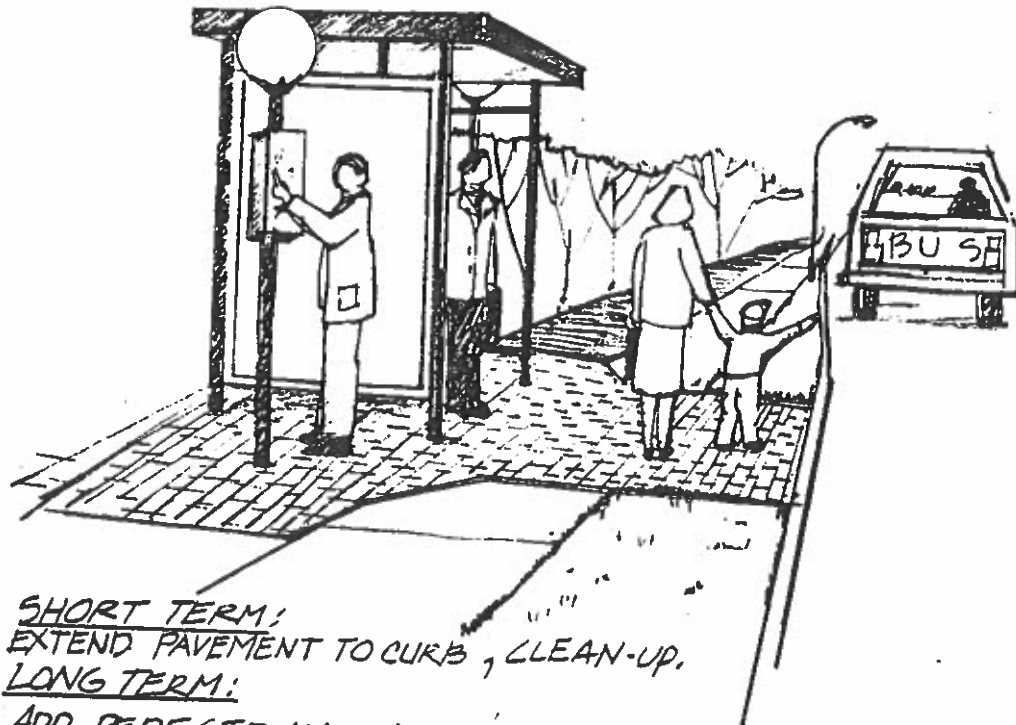


9. BUS SHELTER

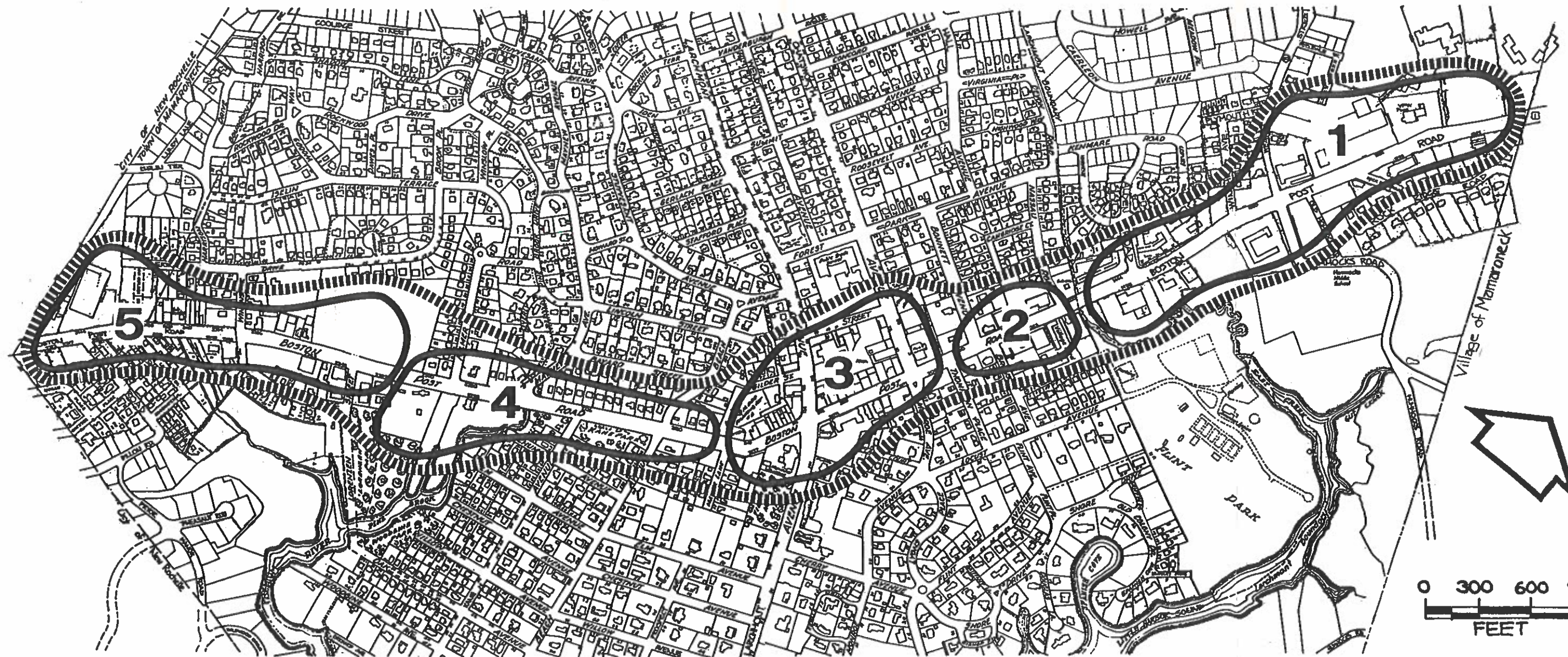
EXISTING CONDITIONS

- BUS SHELTER AND SURROUNDING AREA ARE IN POOR CONDITION.
- GRASS STRIP GETS TRAMPLED ON BY THOSE USERS BOARDING BUS.

PROPOSED IMPROVEMENTS



- SHORT TERM:
 EXTEND PAVEMENT TO CURB, CLEAN-UP.
- LONG TERM:
 ADD PEDESTRIAN SCALE LIGHTS
 CHANGE PAVING MATERIAL IN HEAVY USE AREA



Planning Units

SHUSTER ASSOCIATES

RD 1, Box 259 Stone Ridge, New York 12484

ALLAN DAVIS ASSOCIATES, INC.,

Traffic Engineering • Parking Studies

Map
No.


















6

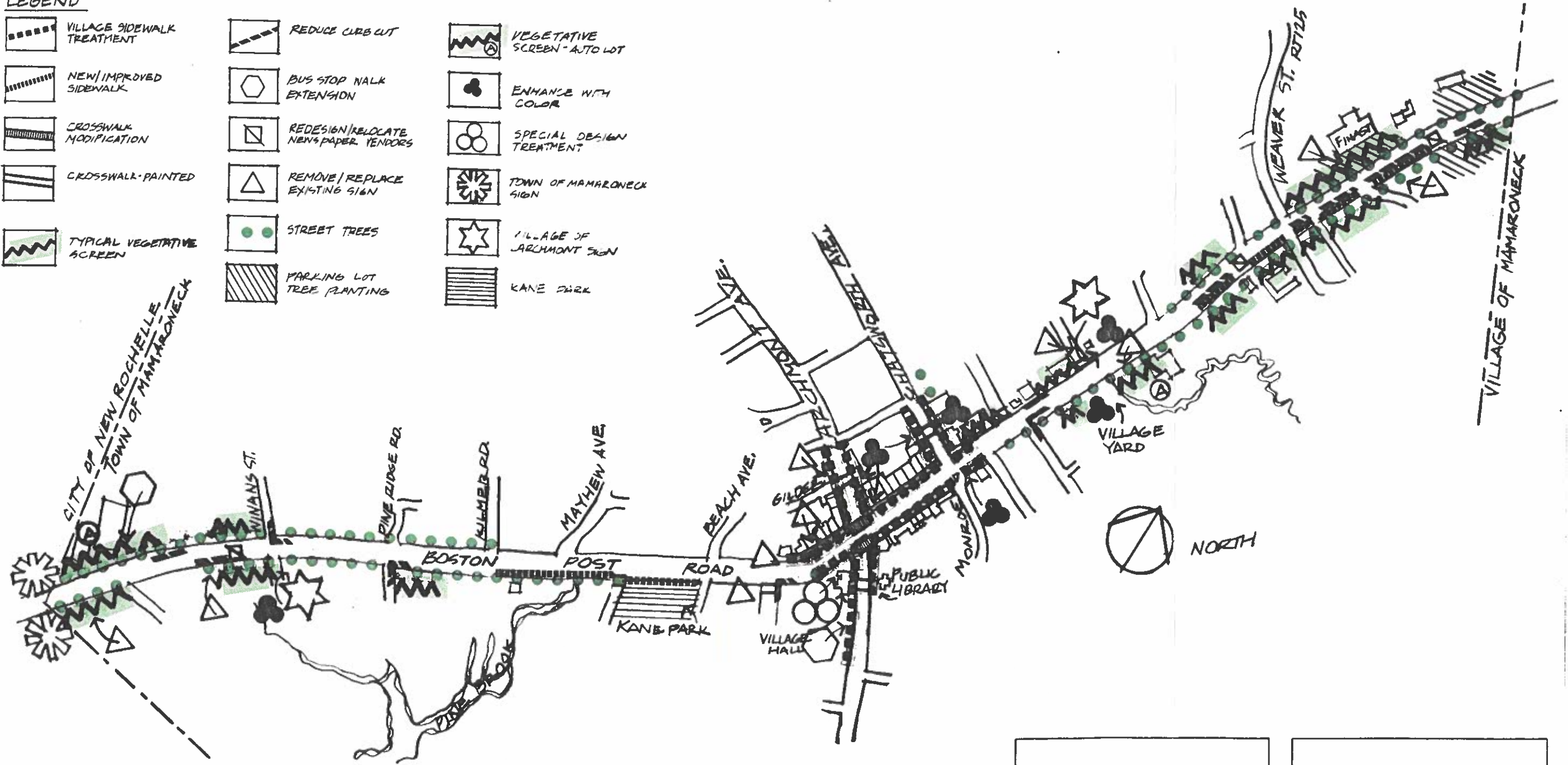
MASTER
PLAN
UPDATE

VILLAGE
OF
LARCHMONT

TOWN
OF
MAMARONECK

LEGEND

- | | | |
|--|--|--|
|  VILLAGE SIDEWALK TREATMENT |  REDUCE CURB CUT |  VEGETATIVE SCREEN - AUTO LOT |
|  NEW/IMPROVED SIDEWALK |  BUS STOP WALK EXTENSION |  ENHANCE WITH COLOR |
|  CROSSWALK MODIFICATION |  REDESIGN/RELOCATE NEWS PAPER VENDORS |  SPECIAL DESIGN TREATMENT |
|  CROSSWALK - PAINTED |  REMOVE/REPLACE EXISTING SIGN |  TOWN OF MAMARONECK SIGN |
|  TYPICAL VEGETATIVE SCREEN |  STREET TREES |  VILLAGE OF LARCHMONT SIGN |
| |  PARKING LOT TREE PLANTING |  KANE PARK |



Post Road Design Concept

SHUSTER ASSOCIATES
 RD 1, Box 259 Stone Ridge, New York 12484
 ALLAN DAVIS ASSOCIATES, INC.,
 Traffic Engineering • Parking Studies

Map
 No. 7

MASTER
 PLAN
 UPDATE

VILLAGE
 OF
 LARCHMONT

TOWN
 OF
 MAMARONECK

PART IV
ACTION PROGRAM

IV. ACTION PROGRAM

This section describes the specific legislative, administrative and physical actions necessary to implement land use, traffic, parking and design policies, recommended in Part III.

A. Land Use

Several amendments to the zoning regulations of both the Town and Village are proposed to implement the recommended land use policies. These are set forth below and illustrated on Map No. 8, Proposed Zoning.

1. Amendments to the Town Zoning Ordinance

a. Amendments to the Business (B) District

In order to achieve the several objectives for the Post Road between Larchmont and the Village of Mamaroneck, three changes are proposed in the Town's only (as of now) business zoning district regulations, as follows:

- (1) The minimum lot width requirement shall be established as 150 feet; there is currently no width requirement.
- (2) The section on permitted uses shall be amended to delete, under Principal Uses, item (5) "sale or hire of new or used motor vehicles. . . ." and, under Special Uses, item (1) "gasoline filling stations and garages" and item (7) "automatic car-washing facilities".
- (3) The provisions for Mixed Use Development which were recommended in Phase I, if enacted, would also be applicable to several sites in this area if the minimum site area were reduced to 60,000 square feet (see page 35 of the Phase I report). These are proposed as an amendment to the B District.

b. Change in Parking Requirements

As in the Phase I report, it is recommended that §98-66 shall be amended so that the minimum required parking for retail or service establishments shall be one space for each 200 square feet of gross leasable floor area. Recent traffic and parking studies indicate that the present requirement of one space for each 100 square feet is excessive. ("Parking Generation" ITE, 1985)

Such a change could lead to two positive results. Additional retail space could be added to existing shopping centers which comply with the current parking requirement,

without additional parking. Also, some of the existing parking spaces could be replaced by landscaping as part of the effort to enhance the appearance of the Post Road. In fact, in some cases, the second provision may be made a prerequisite for the first.

c. Establish New Service Business (SB) District

A new service oriented business district was recommended in the Phase I Report (see page 33). This recommendation is reaffirmed and it is recommended that the Zoning Map be amended to designate the area of the Post Road between the Village of Larchmont and the City of New Rochelle as Service Business (see Map No. 8).

In addition it is recommended that the SB District permit, as a special use, multi-family dwellings in accord with the requirements of the R-A District which allow development at a density of 17 units per acre. This would provide an option to encourage assemblage of some of the smaller properties which are not suitable for business use.

d. Regulation of Home Occupations

See item 2.c. below.

2. Amendments to the Village Zoning Law

- a. The new RC District proposed in Phase I (page 36) for the railroad area is equally applicable to the Village Hall area (Planning Unit 3) in terms of design guidelines and, land use. It is proposed that the area from Manor Lane to Bronson Avenue be changed from RB to RC thereby incorporating the design controls and eliminating automotive uses (see Map No. 8). However, in that there is less opportunity to meet parking demands by reallocation of the existing parking supply, the waiver provisions applicable to new floor area would only exacerbate the tight parking situation and are not appropriate at this time.

At such time as additional parking is provided, as per C. below, the parking waiver should be re-evaluated. For instance, the total floor area possible if every existing structure which complies with all bulk requirements were allowed to expand by 500 square feet or 25% (up to a floor area ratio of 1.6), whichever is less, is approximately 15,000 square feet. The existing parking regulations would require 60 parking spaces for this sum. Under the actual ratio of parking to floor area which exists today, about 40 spaces would be required. In that it is virtually impossible for this entire floor area to be realized, the waiver would not significantly increase the demand for parking but would allow some property improvements and minor expansions to take place.

b. Amend Townhouse Provisions

A 1977 amendment to the Zoning Law permits "Townhouse Developments" in the R-7.5 District, at a density of seven units per acre, upon approval of a special permit by the Village Board. Among the requirements for such development is that the site have a minimum contiguous area of six acres. This provision would preclude town houses on the nursery properties where such development could be used to preserve the sensitive portions of the property. Since use of this option is subject to special permit, site plan review and environmental impact review, there is ample opportunity to evaluate each proposed project in considerable detail. The minimum area requirement should be reduced to two acres, but use of these provisions confined to properties with direct access from the Post Road or Palmer Avenue which are designed to accommodate any traffic impacts.

c. Refine Home Occupation Requirements

The standards for a "professional office" as a permitted accessory use in residential districts should be amended to establish the maximum floor area and/or proportions of the building that may be devoted to such use, that the professional must be the primary resident of the structure and to ensure that the basic residential character of the structure and lot will be preserved. Such standards should include the following provisions.

- (1) There shall be no indication of the home occupation from the exterior of the building, except for a sign of no more than three square feet.
- (2) No modification to the structure shall alter its residential character in terms of window or door openings, materials, or similar characteristics.
- (3) A home occupation shall be conducted only within the principal building on the lot.
- (4) The floor area designed for and allotted to the home occupation shall not exceed 30 percent of the total floor area of the dwelling unit or 500 square feet, whichever is less.
- (5) No more than one person who is not a resident of the dwelling unit shall be employed in the home occupation.
- (6) No materials or equipment used in the home occupation shall be stored or displayed outside the dwelling unit.

- (7) At least one but no more than three off-street parking spaces shall be provided for a home occupation, in addition to those required for the principal residential use. Such space may be provided in the driveway but not in any required front or side yard.
- (8) No commercial vehicle shall be used in connection with the home occupation or parked on the property.
- (9) The home occupation shall not result in traffic, noise, vibration, odor, smoke, glare or electrical interference beyond that normally generated by permitted uses in the same zoning district.

B. Traffic

- 1. The improvements to the geometry of the intersections of Larchmont Avenue (Monroe Avenue) and Chatsworth Avenue with the Boston Post Road, illustrated on Figures 1 and 2, will increase traffic capacity and reduce delays while improving pedestrian safety. Cost of these improvements are estimated below. Three parking spaces will also be lost.

LARCHMONT AVENUE & POST ROAD

Approximately: 6,000 S.F. Concrete Sidewalk
 360 L.F. Concrete Curb
 Stripe Lanes & Crosswalks

Estimated Cost: \$50,000 - \$80,000

CHATSWORTH AVENUE & POST ROAD

Approximately: 350 S.F. Concrete Sidewalk
 50 L.F. Concrete Sidewalk
 Remove & Restore Island
 Stripe Lanes & Crosswalks

Estimated Cost: \$8,000 - \$12,000

- 2. Modification of the traffic control system along the Boston Post Road at the Larchmont Avenue and Chatsworth Avenue intersections will reduce delays and improve the quality of travel through the area. Several options exist, all of which must be approved by the New York State Department of Transportation:
 - a. The most effective measure is to install a master traffic signal controller interconnected between the Chatsworth Avenue, Larchmont Avenue and Beach Avenue intersections with the Boston Post Road. This system would be traffic responsive with signal timing adjusted with changes in traffic flow rather than on a time basis.

*Exhibit A illustrates the improved level of service if both the physical changes and optimum traffic signal upgrading were undertaken. A definition of level of service is also provided.

- b. An improvement to the Chatsworth Avenue and Boston Post Road intersection to reduce delays at this location requires the traffic signal be upgraded to be fully-actuated.
 - c. The minimum measure to be taken to reduce delays at these intersections would be to adjust the signal timing to the optimal timing as determined by the 1985 Highway Capacity Manual, signalized intersection capacity calculations. The signal timing should reflect any changes to the intersection geometry and traffic volumes.
3. With construction of the traffic signal at the intersection of Chatsworth Avenue and Addison Street, the operation of the Grand Union driveways pose two problems. At the entrance driveway, at the south end of the frontage, the signal exacerbates the "interlock" between left-turning Grand Union customers southbound on Chatsworth which existed before but which was not subject to the coincident discharge imposed by the signal.

Queues southbound on Chatsworth at the traffic signal stop line obstruct left-turning shopping center customers exiting from the parking lot.

Several options are available to deal with these problems:

- a. Prohibit left turns into and out of Grand Union. This would be a great inconvenience to shopping center customers and would introduce circuitous routings for those who did not choose to shop elsewhere in response to the restrictions.
- b. Reverse the internal circulation in the Grand Union parking lot so that the conflicts at the entrance would be eliminated and exiting vehicles would enter the intersection in front of the northbound stop line, presumably on the Addison green phase. Exiting vehicles could line up ahead of the stop bar and await a northbound green or make the left turn in gaps in the oncoming Addison traffic, as from any uncontrolled intersection/driveway, as happened prior to signalization. Reversal of the operation of the parking lot complicates the use of the service area. Commercial vehicles would be unable to reverse within the lot to back into the loading dock.
- c. Cut a new entrance into the parking lot opposite Addison Street and close the two existing driveways. This would reduce the number of parking spaces within the lot, require the relocation of the traffic signal mast arm which has been newly installed opposite Addison Street and would complicate, if not make impossible, the use of the loading dock.
- d. Perhaps the simplest solution is to introduce a new stop bar north of the exit driveway with a sign which says "Stop here on red" for southbound traffic. This would allow a reservoir for vehicles exiting the Grand Union driveway wishing to turn left. This solution does not resolve the left-turning interlock at the entrance drive. Until that becomes an operational problem its solution may be deferred.

C. Parking

1. Introduce long-term parking in the Saint Augustine Lot (Municipal Lot No. 11).
2. As discussed in Part III, it will be necessary to increase the parking supply by approximately 100 additional parking spaces, in the correct locations, to ease the demand on existing parking in the Village Hall business area. Such an increase would be in line with industry practice for providing an adequate level of parking service in the short term, and would provide some reserve to accommodate new development. Possible areas for parking expansion are discussed below and summarized in the following table.
 - a. Expansion of the Gilder Municipal Lot to the east through acquisition of the parcel of land between the existing lot and Post Alley (see Figure 3) to add 39 parking spaces.
Total Cost Estimate: \$550,000.
 - b. Construction of a parking facility under the school yard on Addison Street (see Figures 5A and 5B) to create 66 or 128 parking spaces and restoring the playground above the garage. When complete, there would be no impact on the school or playground. However, particular care would have to be taken during construction to minimize disruption of school property and the length of time the school yard is unavailable.
Total Cost Estimate: \$528,000 or \$1,056,000 (excluding any land cost which may be required).
 - c. Construct a parking area in front of Village Hall Park by relocating the sidewalk into the park to allow perpendicular parking on Larchmont Avenue (see Figure 6) to add about 17 spaces.
Total Cost Estimate: \$77,000.

SUMMARY

| LOCATION | PARKING SPACES GAINED | PRIMARY CONSIDERATIONS | | |
|-------------------------------------|-----------------------|------------------------|----------------|------------------|
| | | LOCATION | TRAFFIC IMPACT | COST (Per Space) |
| a. Gilder Lot Expansion | 39 | Good | Small | \$14,100 |
| b. Addison Street (School Property) | 66/128 | Good | Moderate | \$8,000 |
| c. Village Hall Park Frontage | 17 | Good | Small | \$4,500 |

3. Several options to provide additional parking that were considered and dismissed include:
 - a. Construction of a parking deck over the existing Addison Municipal Lot.

This option was dismissed because it would not be possible to obtain sufficient parking spaces after providing access for truck deliveries and parking in private parking areas on the perimeter of the lot. Additional property would also be required along Addison Street for the access ramps.

- b. Construction of a parking deck over the Grand Union parking lot.

This option was dismissed because of the same problems with access for deliveries and ramps as the Addison Lot.

- c. Acquisition of two vacant parcels of land on the south side of the Boston Post Road on the corner of Manor Lane (see Figure 4) to create 37 parking spaces.

This alternate was rejected because of its intrusion into a residential area and its distance from the business area.

D. DESIGN

Implementation of the various design recommendations depends on a long range effort involving effective use of regulatory tools, direct public action and creating a spirit of public/private cooperation and concern for the visual environment.

Both Town and Village have the basic regulatory tools to review proposed development in terms of site planning, building design, landscaping and signage. The design guidelines and concept plan developed in this report are intended to highlight areas of concern, to establish specific objectives and criteria to be followed by the various reviewing bodies, to provide a framework for governmental actions and to encourage participation by owners of private property on the Post Road. Set forth below are a variety of specific early actions to implement the guidelines and policies in Part III.

1. Hold a series of workshops for members of the various agencies involved in design review to explain the design concept and to develop a consensus on objectives, procedures and criteria to be followed during design review.
2. Institute an awards program in which a designated municipal panel (such as the Larchmont Beautification Commission) would issue a "good design certificate" to private property owners who construct or maintain improvements which substantially contribute to the design and appearance of the community. Good media coverage will help emphasize design in the community.

3. Develop a detailed "design vocabulary" of approved street furniture (trash receptacles, benches, bike racks, planters, lights, etc.) paving materials, trees and shrubs for use on both public and private property. These elements should be selected on the basis of initial cost and the ease and cost of maintenance and should be required by reviewing agencies as part of plan approval.
4. Revise the sign regulations in the Town and Village to distinguish between the several business districts. The existing single set of standards does not reflect the differences between strip commercial areas and the concentrated pedestrian oriented shopping core.
5. Establish a four year capital improvement program to begin implementation of the design plan. The proposed program is described below with cost estimates summarized in the following table. This program would accomplish all of the proposed public design improvements with the exception of a number of high cost, longrange items in the Village Hall business area--brick sidewalks, granite curbs and pedestrian scale lighting.

FOUR YEAR PROGRAM

First Year

Town: Sidewalk and curb improvements from Village of Mamaroneck to Hommocks Road (south side only).

| | |
|----------------------------|----------|
| 3,400 sq. ft. sidewalk | \$15,810 |
| 600 lin. ft. concrete curb | 6,000 |

Village: Street trees; mid-block crosswalks; flowers shrubs--Village Hall area.

| | |
|--------------------|----------|
| 10 trees w/o grate | \$ 3,250 |
| 10 trees w/grate | 6,000 |
| 3 crosswalks | 1,950 |
| flowers/shrubs | 10,000 |

Second Year

Town: Street trees from Weaver Street and Hommocks Drive to Village of Mamaroneck; entrance sign from New Rochelle

| | |
|----------|----------|
| 56 trees | \$18,200 |
| 1 sign | 500 |

Village: Street trees from Town line to Kilmer Road and to Lorenzen Street; entrance sign from west.

| | |
|----------|----------|
| 46 trees | \$14,950 |
| 1 sign | 500 |

Third Year

Town: Street trees from Hommocks Drive and Weaver Street to Larchmont line; bus shelter.

| | |
|-----------------------------|----------|
| 43 trees | \$13,975 |
| 1 bus shelter (new) | 5,500 |
| 1 bush shelter (redesigned) | 2,500 |

Village: Tree grates at existing trees in Village Hall area; trees from Lorenzen Street to Pine Brook.

| | |
|-----------|----------|
| 40 grates | \$12,000 |
| 22 trees | 7,150 |

Fouth Year

Town: Street trees form City of New Rochelle to Deane Place and to Larchmont line (north side).

| | |
|----------|---------|
| 27 trees | \$8,775 |
|----------|---------|

Village: Street trees (in-fill) in Village Hall area; sidewalk and curb along Kane Park.

| | |
|--------------------------------|----------|
| 10 trees w/grate @ \$600 | \$ 6,000 |
| 2,600 sq.ft. sidewalk @ \$4.65 | 12,090 |

SUMMARY

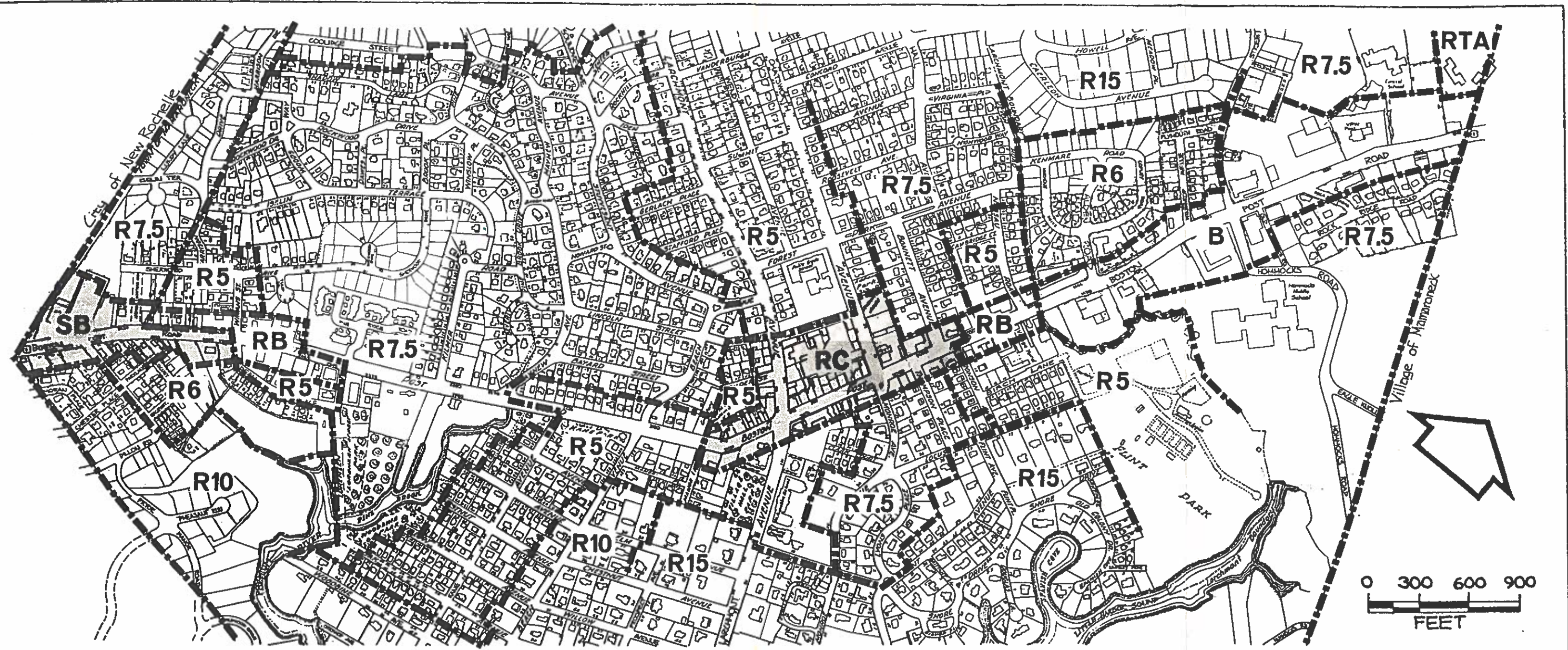
Town of Mamaroneck

Year

| Item | 1 | 2 | 3 | 4 |
|--------------------------|-----------------|-----------------|-----------------|-----------------|
| a. Tree Planting | | \$18,250 | \$13,975 | \$ 8,775 |
| b. Entrance Sign | | 500 | | |
| c. Bus shelters | | | 8,000 | |
| d. Sidewalks | \$15,800 | | | |
| e. Curbs | 6,000 | | | |
| Contingencies/ Design | 5,450 | 4,675 | 5,493 | 2,193 |
| TOTAL | \$27,250 | \$23,375 | \$27,468 | \$10,968 |

Village of Larchmont

| Item | Year | | | |
|------------------------------------|----------|----------|----------|----------|
| | 1 | 2 | 3 | 4 |
| a. Flowers and Shrubs | \$10,000 | | | |
| b. Entrance Signs | | 500 | | |
| c. Sidewalks | | | | \$12,090 |
| d. Tree Planting and/ or grates | 9,250 | 14,950 | \$19,150 | 6,000 |
| e. Mid-block cross- walks | 1,950 | | | |
| Contingencies/ Design | 5,300 | 3,862 | 4,787 | 4,522 |
| <hr/> | | | | |
| TOTAL | \$26,500 | \$19,312 | \$23,937 | \$22,612 |
| <hr/> | | | | |



Area of proposed zone change

Proposed Zoning

Map No. 8

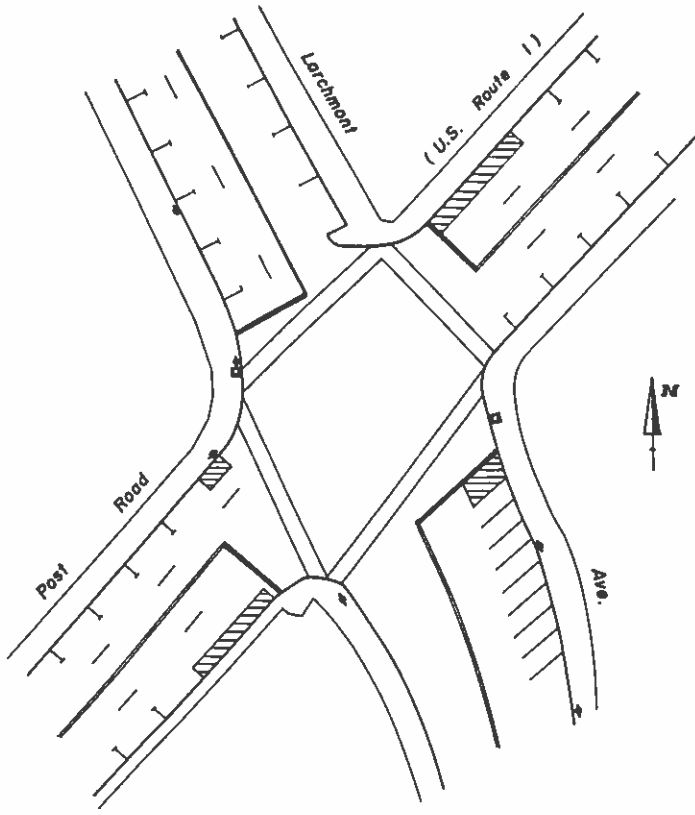
MASTER PLAN UPDATE

VILLAGE OF LARCHMONT

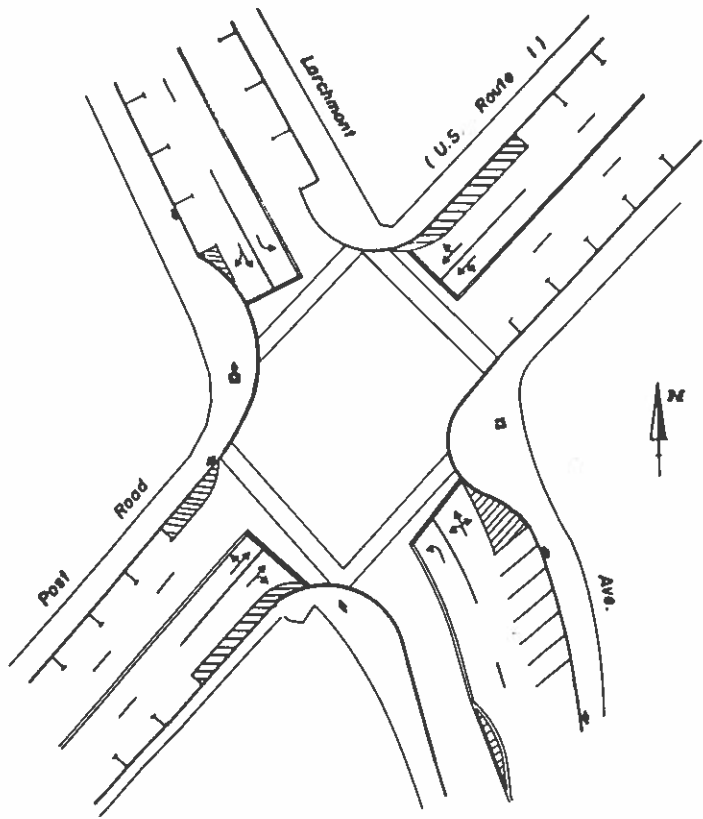
TOWN OF MAMARONECK

SHUSTER ASSOCIATES
 RD 1, Box 259 Stone Ridge, New York 12484
 ALLAN DAVIS ASSOCIATES, INC.,
 Traffic Engineering • Parking Studies

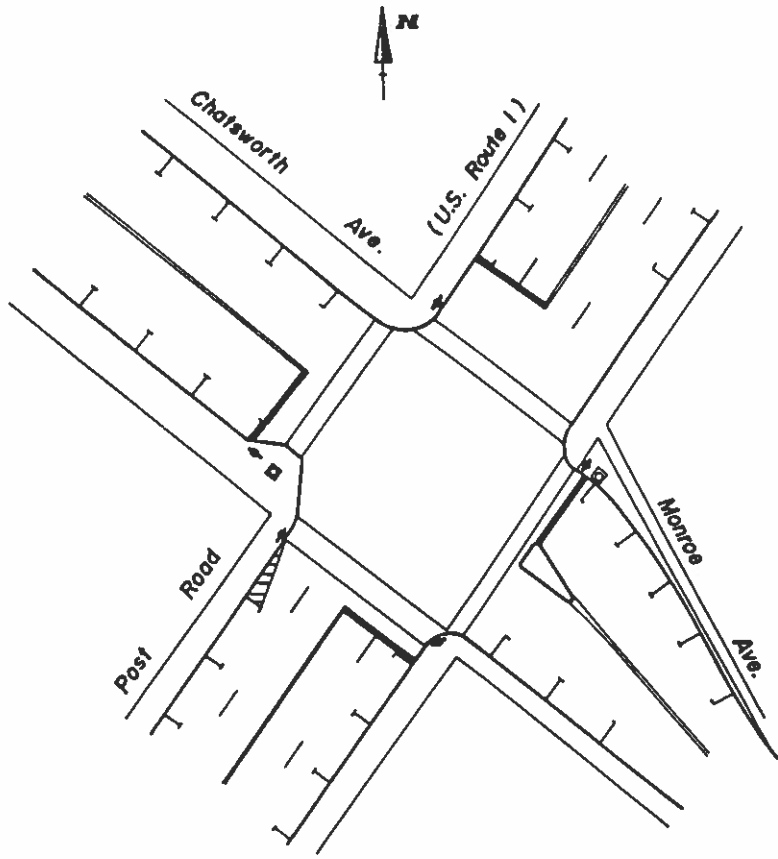
FIGURES



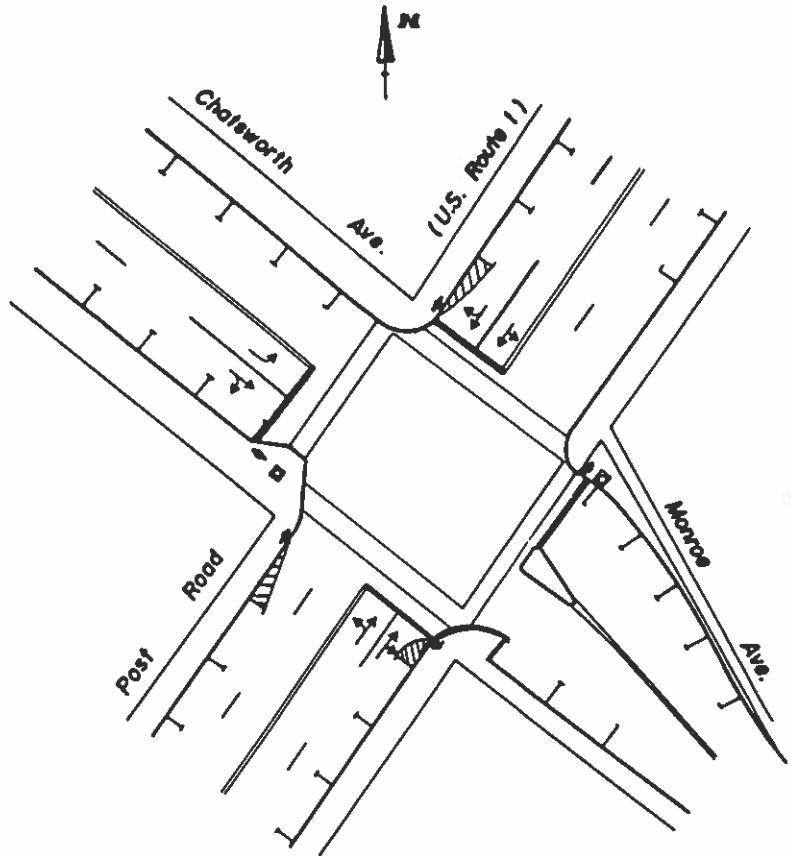
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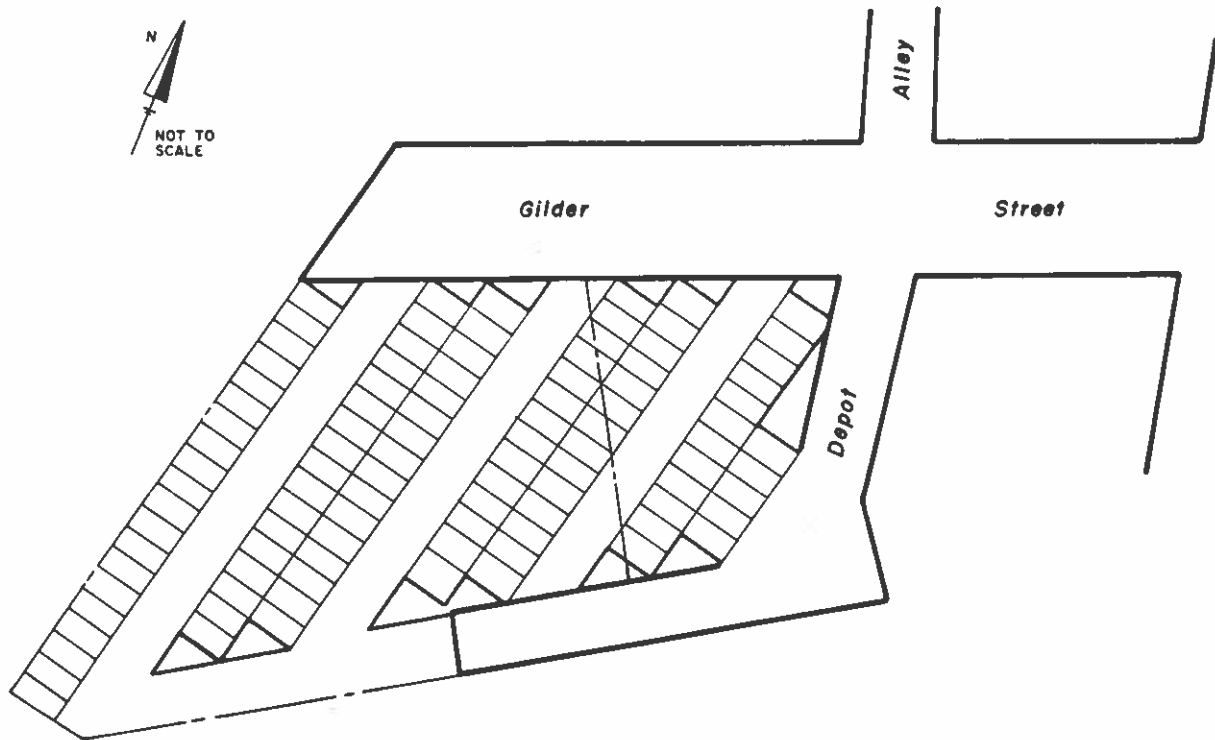
IMPROVED CONDITIONS



EXISTING CONDITIONS



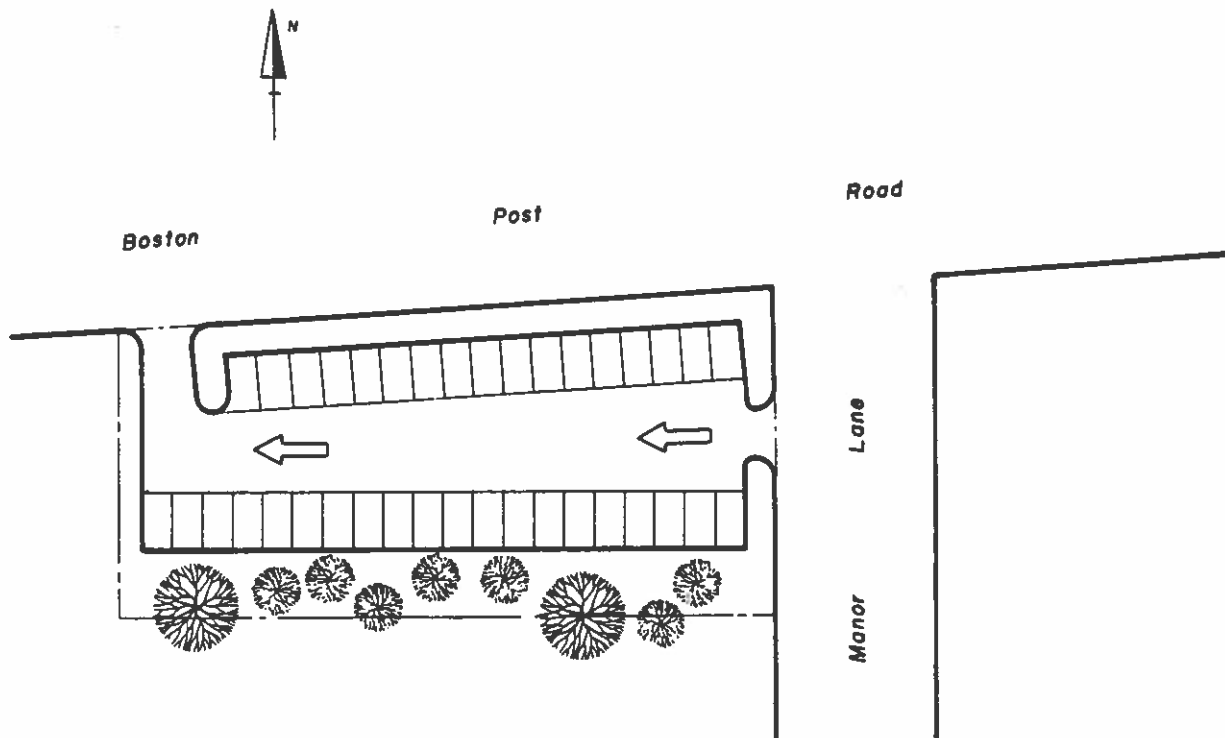
IMPROVED CONDITIONS
FIGURE 2



PROPOSED CONDITIONS
98 SPACES



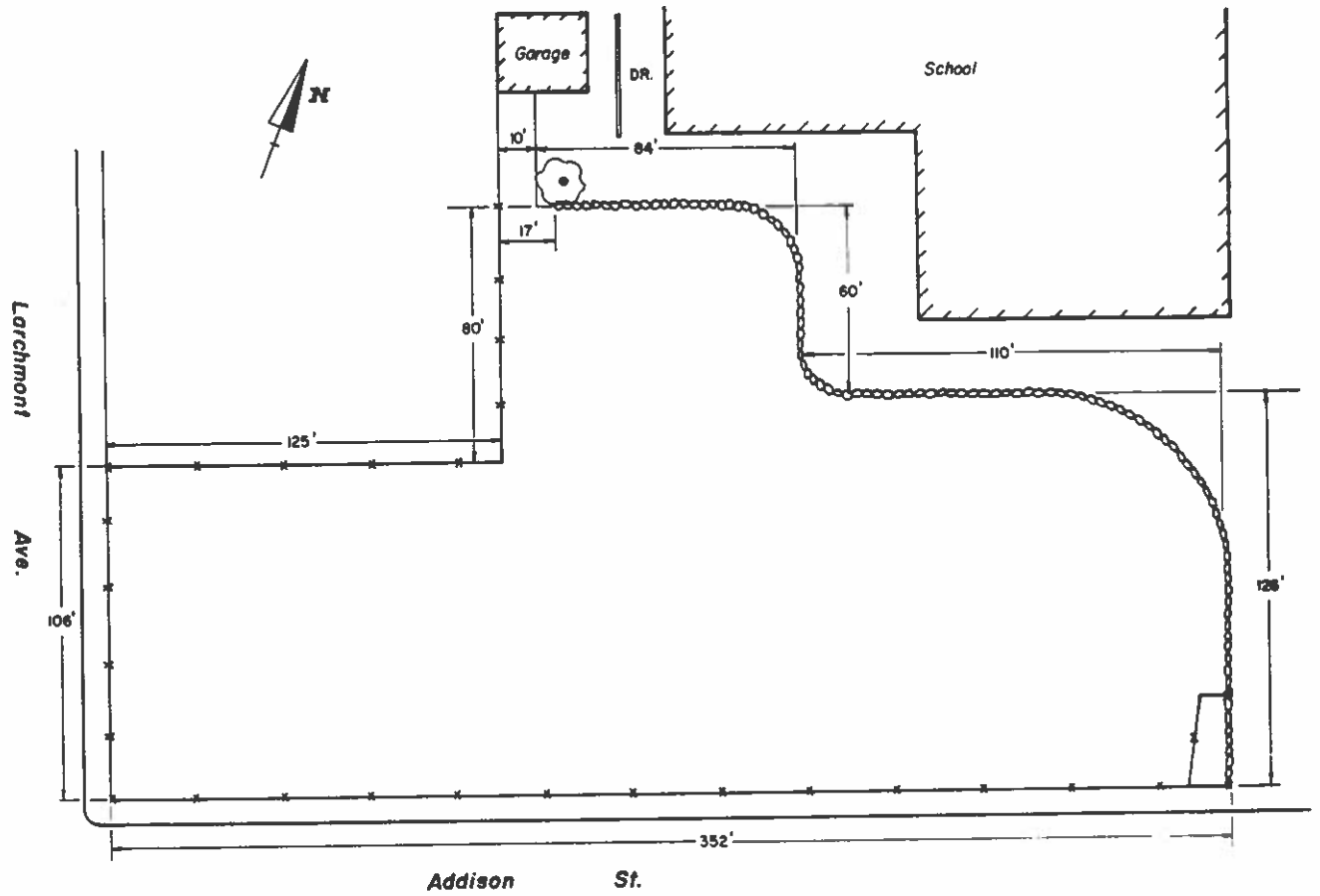
FIGURE 3



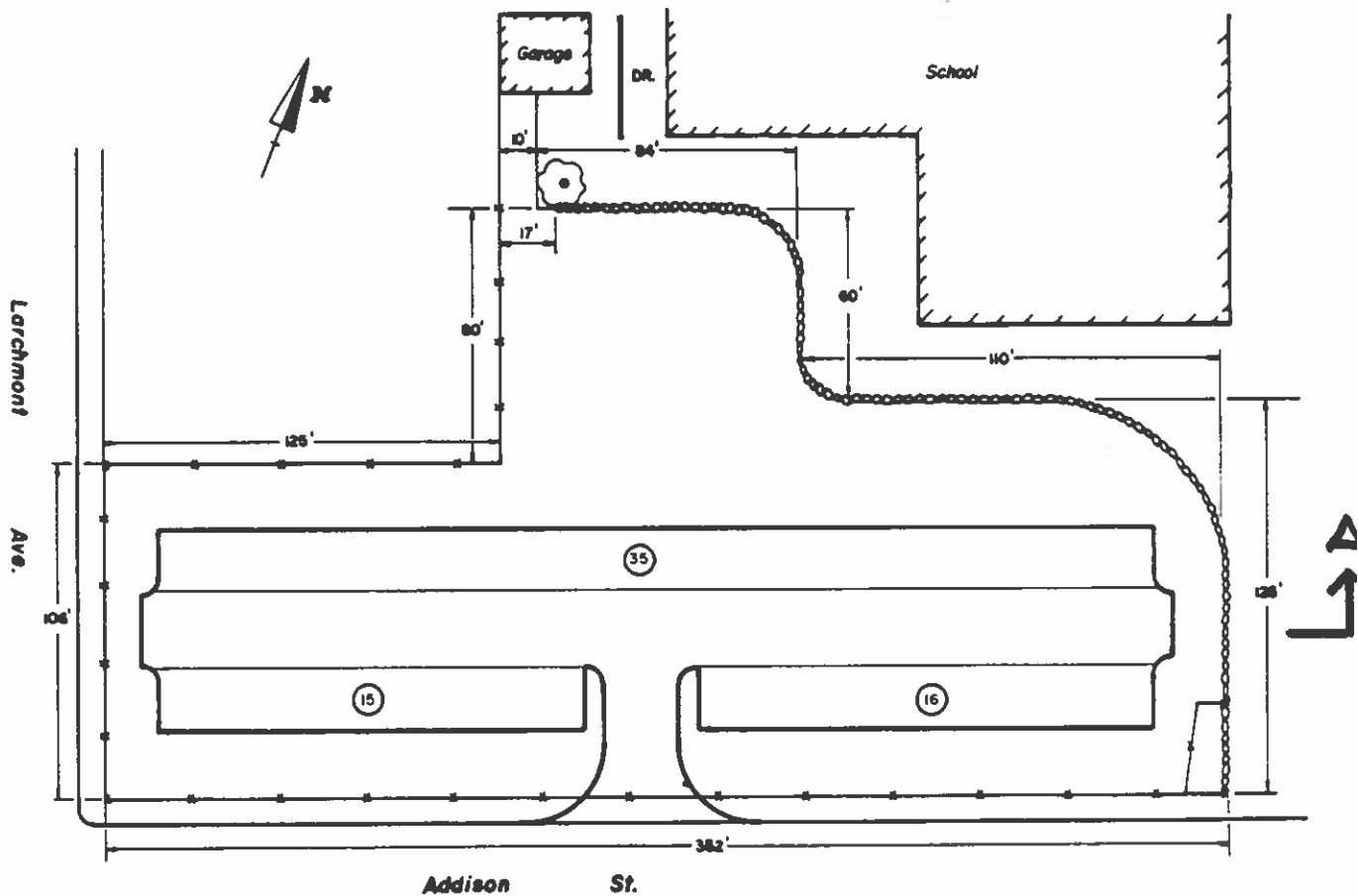
PROPOSED CONDITIONS
37 SPACES



FIGURE 4



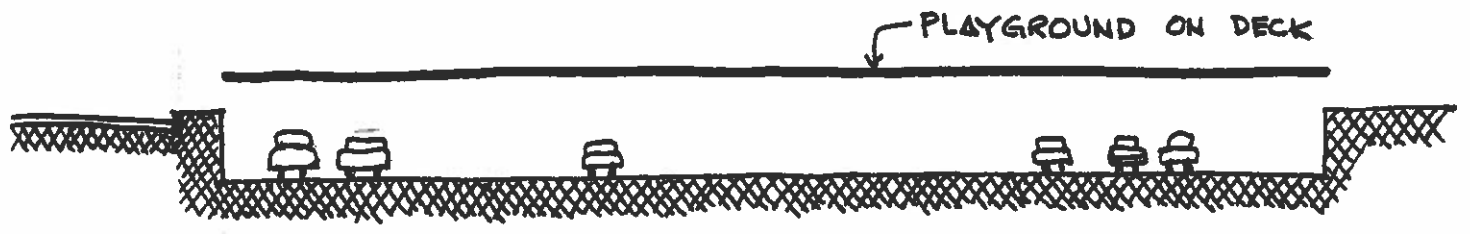
EXISTING CONDITIONS



PROPOSED CONDITIONS
66 SPACES

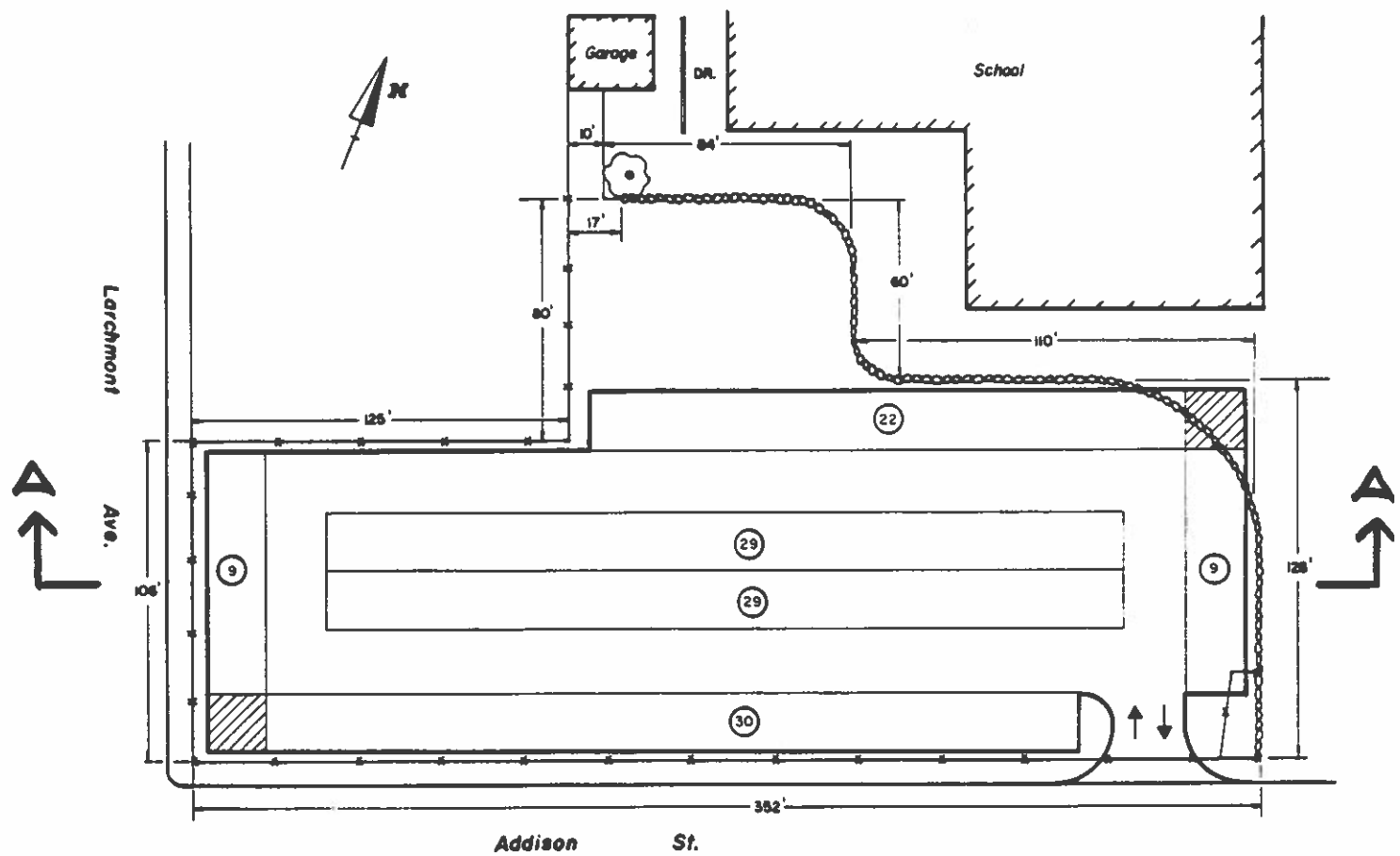
ADA ALLAN DAVIS ASSOCIATES, INC.

FIGURE 5A



SECTION A-A

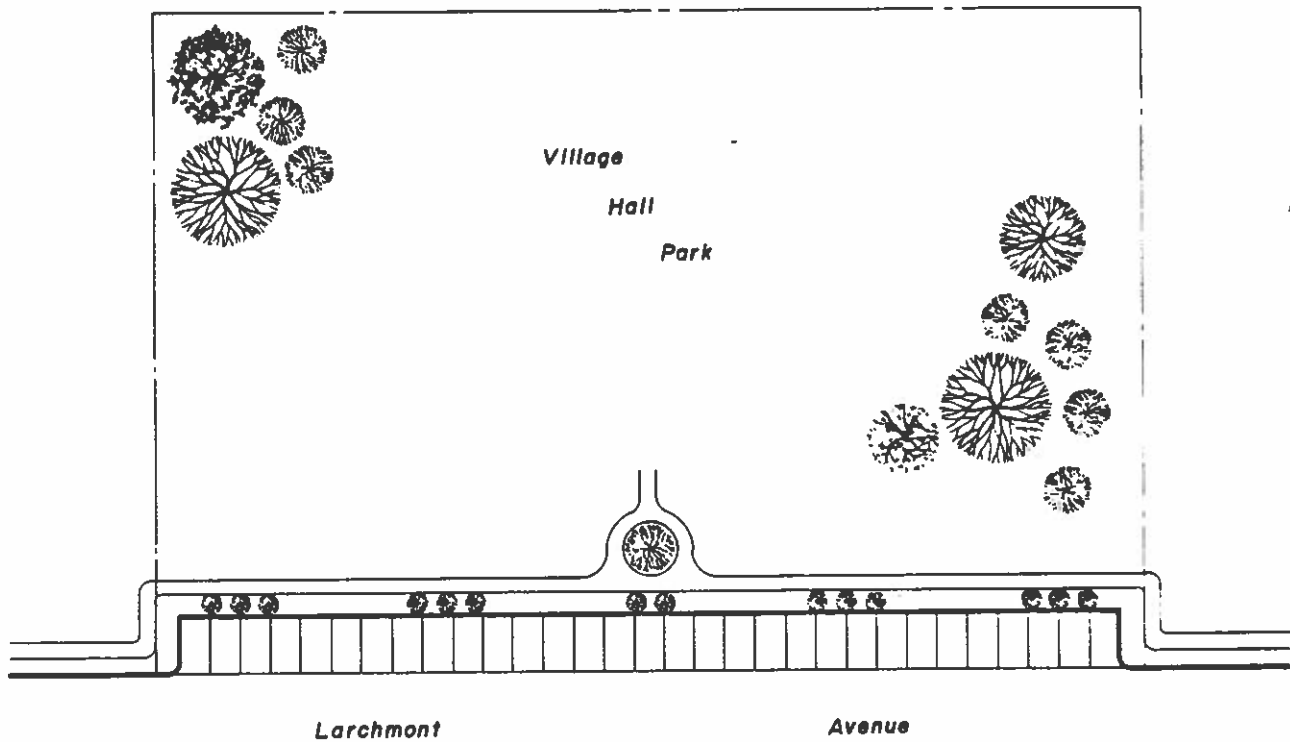
N.T.S.



PROPOSED CONDITIONS
128 SPACES



FIGURE 5B



PROPOSED CONDITIONS
31 SPACES



FIGURE 6

TABLES

TABLE 1
TRAFFIC VOLUMES

| <u>LOCATION</u> | <u>TWO WAY DAILY TRAFFIC</u> |
|-----------------------------|------------------------------|
| Boston Post Road @ Ferndale | 11,089 |
| Boston Post Road @ Mayhew | 8,676 |
| Chatsworth @ Summit | 8,069 |
| Larchmont @ Summit | 6,710 |

TABLE 2

MUNICIPAL OFF-STREET PARKING
INVENTORY AND FEE SCHEDULE

| | <u>Number of Spaces</u> | | | Total |
|---|-------------------------|-----------|--------|-------|
| | Metered | Transient | Permit | |
| Gilder Street Lot (Municipal Lot No. 5) | 0 | 24 | 21 | 45 |
| Addison Street Lot (Municipal Lot No. 6) | 0 | 48 | 15 | 68 |
| Saint Augustine's Lot (Municipal Lot No. 11) | 0 | -- | -- | 44 |

| <u>Fee Schedule</u> | <u>Day Permits</u> | <u>Night Parking</u> |
|---|--------------------|----------------------|
| Yearly (January thru December Only) | \$135.00 | \$75.00 |
| Semi-Annually (January thru June; July thru December) | \$ 75.00 | \$45.00 |
| Quarterly (January thru March; April thru June; July thru September; October thru December) | \$ 40.00 | \$25.00 |

Parking Meter Decal, Village of Larchmont:

| | | |
|-------------------------------------|----------|----------|
| Village of Larchmont Residents..... | \$ 2.00 | Annually |
| Town of Mamaroneck Residents | \$ 5.00 | Annually |
| Other | \$100.00 | Annually |

TABLE 3A
MUNICIPAL ON-STREET AND OFF-STREET PARKING
THURSDAY, 12/18/86

| <u>Location</u> | <u>Number Of Spaces</u> | <u>Maximum Number Of Occupied Spaces</u> | <u>Maximum Occupancy</u> |
|-----------------------|-----------------------------|--|------------------------------|
| Addison Municipal Lot | 145 | 125 | 86 |
| Gilder Municipal Lot | 59 | 55 | 93 |
| Saint Augustine Lot | 52 | 27 | 52 |
| Addison Street | 36 | 20 | 56 |
| Beach Avenue | 20 | 8 | 40 |
| Bonnett Avenue | 41 | 7 | 17 |
| Boston Post Road | 105 | 66 | 63 |
| Bronson Avenue | 16 | 6 | 38 |
| Chatsworth Avenue | 23 | 28 | 122 |
| Gilder Street | 4 | 4 | 100 |
| Larchmont Avenue | 71 | 74 | 104 |
| Monroe Avenue | 6 | 6 | 100 |
| Thompson Place | 36 | 12 | 33 |
| Grand Union | 61 | 61 | 100 |
| | <u>675</u> | <u>499</u> | |

TABLE 3B

MUNICIPAL ON-STREET AND OFF-STREET PARKING

SATURDAY, 12/20/86

| <u>Location</u> | <u>Number Of Spaces</u> | <u>Maximum Number Of Occupied Spaces</u> | <u>Maximum Occupancy</u> |
|-----------------------|-----------------------------|--|------------------------------|
| Addison Municipal Lot | 145 | 152 | 105 |
| Gilder Municipal Lot | 59 | 62 | 105 |
| Saint Augustine Lot | 52 | 44 | 85 |
| Addison Street | 36 | 20 | 56 |
| Beach Avenue | 20 | 1 | 5 |
| Bonnett Avenue | 41 | 6 | 15 |
| Boston Post Road | 105 | 77 | 73 |
| Bronson Avenue | 16 | 5 | 31 |
| Chatsworth Avenue | 23 | 29 | 126 |
| Gilder Street | 4 | 5 | 125 |
| Larchmont Avenue | 71 | 78 | 110 |
| Monroe Avenue | 6 | 10 | 167 |
| Thompson Place | 36 | 13 | 36 |
| Grand Union | 61 | 72 | 118 |
| | ----- | ----- | |
| | 675 | 574 | |

TABLE 4A

MUNICIPAL ON-STREET AND OFF-STREET PARKING

Thursday, December 18, 1986

PERCENT OCCUPANCY

| Location | Number Of Spaces | 8-9 | 9-10 | 10-11 | 11-12 | 12-1 | 1-2 | 2-3 | 3-4 |
|-----------------------|---------------------|------|------|-------|-------|------|------|------|------|
| Addison Municipal Lot | 145 | 45% | 66% | 83% | 86% | 83% | 78% | 80% | 77% |
| Gilder Municipal Lot | 59 | 51% | 83% | 85% | 93% | 90% | 83% | 81% | |
| Saint Augustine Lot | 52 | 12% | 37% | 40% | 40% | 42% | 50% | 52% | 40% |
| Addison Street | 36 | 6% | 25% | 22% | 28% | 39% | 39% | 56% | 47% |
| Beach Avenue | 20 | 0% | 40% | 40% | 25% | 30% | 35% | 30% | 35% |
| Bonnett Avenue | 41 | 17% | 17% | 15% | 15% | 10% | 12% | 12% | 12% |
| Boston Post Road | 105 | 29% | 48% | 47% | 52% | 59% | 63% | 60% | 63% |
| Bronson Avenue | 16 | 31% | 31% | 38% | 31% | 38% | 38% | 38% | 38% |
| Chatsworth Avenue | 23 | 109% | 104% | 100% | 104% | 113% | 96% | 113% | 122% |
| Gilder Street | 4 | 75% | 75% | 25% | 50% | 100% | 75% | 75% | |
| Larchmont Avenue | 71 | 37% | 90% | 93% | 103% | 92% | 104% | 93% | 69% |
| Monroe Avenue | 6 | 0% | 83% | 50% | 100% | 83% | 83% | 67% | 50% |
| Thompson Place | 36 | 17% | 25% | 28% | 31% | 33% | 28% | 36% | 28% |
| Grand Union | 61 | 54% | 90% | 85% | 93% | 100% | 93% | 85% | 85% |

TABLE 5A

PRIVATE PARKING PROVIDERS

PERCENT OCCUPIED SPACES

THURSDAY, DECEMBER 18 1986

| LOT | LOCATION | 8:00 | 9:00 | 10:00 | 11:00 | 12:00 | 1:00 | 2:00 | 3:00 | Total Number Spaces |
|-------|------------------------------|------|------|-------|-------|-------|------|------|------|------------------------|
| LOT A | Flower Shop | 67% | 67% | 56% | 67% | 67% | 72% | 67% | 56% | 18 |
| LOT B | Manor Park Deli & Graffiti | 67% | 78% | 83% | 100% | 100% | 100% | 67% | 83% | 18 |
| LOT C | Village Paint & Private lots | 20% | 47% | 60% | 93% | 60% | 73% | 60% | 53% | 15 |
| LOT D | Post Alley | 54% | 77% | 77% | 77% | 85% | 85% | 69% | 69% | 13 |
| LOT E | Jayson & Alberts Realty | 33% | 56% | 56% | 89% | 56% | 100% | 89% | 89% | 9 |
| LOT F | Post Alley-West | 30% | 40% | 60% | 50% | 60% | 40% | 60% | 60% | 10 |
| LOT G | Sutton & Whittenmore Inc. | 54% | 62% | 88% | 69% | 69% | 81% | 73% | 100% | 26 |
| LOT H | Amoco Gas | 63% | 63% | 60% | 67% | 63% | 77% | 73% | 67% | 30 |
| LOT I | Mavis Tire & Woodpecker Shop | 50% | 100% | 83% | 67% | 67% | 50% | 50% | 50% | 6 |
| LOT J | Police Lot | 71% | 79% | 68% | 61% | 61% | 64% | 71% | 68% | 28 |
| LOT K | Albee Court | 52% | 52% | 44% | 56% | 44% | 52% | 48% | 44% | 27 |
| LOT L | Arrow lamp | 25% | 100% | 50% | 75% | 75% | 100% | 75% | 75% | 4 |
| LOT M | Midnight Serv. & leasing | 67% | 67% | 50% | 83% | 100% | 100% | 83% | 83% | 6 |
| LOT N | Interior Shop | 11% | 11% | 5% | 21% | 32% | 32% | 16% | 11% | 19 |
| LOT O | Larchmont Federal Savings | 4% | 43% | 54% | 82% | 82% | 54% | 57% | 54% | 28 |
| LOT P | La Cafe' | 11% | 16% | 21% | 37% | 42% | 79% | 58% | 11% | 19 |
| LOT Q | The Bank of New York | 33% | 47% | 53% | 73% | 67% | 67% | 100% | 47% | 15 |
| LOT R | Larchmont Chrysler Plymouth | 47% | 11% | 68% | 116% | 105% | 95% | 89% | 95% | 19 |
| LOT S | Post Office Lot | 94% | 97% | 86% | 64% | 72% | 47% | 42% | 39% | 36 |
| LOT T | Funeral Home | 22% | 22% | 44% | 78% | 78% | 78% | 89% | 89% | 9 |

TABLE 5B

PRIVATE PARKING PROVIDERS

PERCENT OCCUPIED SPACES

SATURDAY, DECEMBER 20, 1986

| LOT | LOCATION | 9:00 | 10:00 | 11:00 | 12:00 | 1:00 | 2:00 | 3:00 | 4:00 | 5:00 | 6:00 | Total Numb of Spaces |
|-------|------------------------------|------|-------|-------|-------|------|------|------|------|------|------|-------------------------|
| LOT A | Flower Shop | 89% | 89% | 78% | 78% | 83% | 67% | 67% | 72% | 56% | 50% | 18 |
| LOT B | Manor Park Deli & Graffiti | 67% | 122% | 106% | 106% | 89% | 78% | 83% | 78% | 67% | 50% | 18 |
| LOT C | Village Paint & Privatelots | 80% | 60% | 73% | 73% | 60% | 60% | 67% | 27% | 27% | 27% | 15 |
| LOT D | Post Alley | 46% | 31% | 46% | 38% | 69% | 77% | 69% | 46% | 38% | 38% | 13 |
| LOT E | Jayson & Alberts Realty | 33% | 44% | 44% | 44% | 67% | 56% | 56% | 56% | 44% | 0% | 9 |
| LOT F | Post Alley-West | 90% | 100% | 130% | 140% | 140% | 120% | 120% | 120% | 120% | 100% | 10 |
| LOT G | Sutton & Whittemore Inc. | 31% | 35% | 42% | 46% | 35% | 31% | 27% | 27% | 23% | 15% | 26 |
| LOT H | Amoco Gas | 72% | 69% | 63% | 59% | 50% | 59% | 53% | 56% | 50% | 50% | 32 |
| LOT I | Mavis Tire & Woodpecker Shop | 44% | 78% | 56% | 67% | 89% | 89% | 100% | 100% | 67% | 33% | 9 |
| LOT J | Police Lot | 50% | 61% | 68% | 61% | 50% | 50% | 50% | 43% | 39% | 36% | 28 |
| LOT K | Albee Court | 41% | 48% | 52% | 48% | 56% | 59% | 56% | 48% | 44% | 44% | 27 |
| LOT L | Arrow lamp | 50% | 75% | 100% | 100% | 75% | 75% | 100% | 100% | 75% | 0% | 4 |
| LOT M | Midnight Serv. & leasing | 67% | 83% | 83% | 83% | 100% | 83% | 83% | 83% | 83% | 83% | 6 |
| LOT N | Interior Shop | 16% | 16% | 42% | 26% | 32% | 53% | 26% | 26% | 11% | 21% | 19 |
| LOT O | Larchmont Federal Savings | 39% | 46% | 50% | 75% | 32% | 25% | 11% | 11% | 7% | 4% | 28 |
| LOT P | La Cafe' | 21% | 16% | 26% | 37% | 63% | 53% | 16% | 16% | 37% | 63% | 19 |
| LOT Q | The Bank of New York | 20% | 27% | 33% | 27% | 13% | 7% | 7% | 7% | 7% | 0% | 15 |
| LOT R | Larchmont Chrysler Plymouth | 47% | 53% | 53% | 47% | 42% | 53% | 47% | 53% | 37% | 37% | 19 |
| LOT S | Post Office Lot | 39% | 47% | 47% | 47% | 33% | 14% | 17% | 6% | 0% | 0% | 36 |
| LOT T | Funeral Home | 43% | 43% | 57% | 57% | 57% | 57% | 43% | 29% | 57% | 57% | 7 |

EXHIBIT A
TRAFFIC CAPACITY ANALYSIS

INTERSECTION DELAY AND LEVEL OF SERVICE

CALCULATED USING
1985 HIGHWAY CAPACITY MANUAL

LARCHMONT AVENUE AND BOSTON POST ROAD

| | <u>Existing Operation</u> | <u>With Improvements</u> |
|-----------------------|-------------------------------|------------------------------|
| Morning Peak Hour | 23.39 sec* LOS = C | 17.76 sec LOS = C |
| Evening Peak Hour | 24.41 sec LOS = C | 18.20 sec LOS = C |
| Saturday Peak Hour | 24.21 sec LOS = C | 18.03 sec LOS = C |

CHATSWORTH AVENUE AND BOSTON POST ROAD

| | <u>Existing Operation</u> | <u>With Improvements</u> |
|-----------------------|-------------------------------|------------------------------|
| Morning Peak Hour | 24.11 sec LOS = C | 21.83 sec LOS = C |
| Evening Peak Hour | 44.32 sec LOS = E | 15.60 sec LOS = C |
| Saturday Peak Hour | 58.95 sec LOS = E | 24.48 sec LOS = C |

*Average stopped delay per vehicle

LEVELS OF SERVICE
(Traffic Signals)

The Highway Capacity Manual (1985) is the basic guide for determining the level of service of roads, streets and intersections.

LEVEL OF SERVICE FOR SIGNALIZED INTERSECTIONS

Level of service for signalized intersections is defined in terms of delay. Delay is a measure of driver discomfort, frustration, fuel consumption, and lost travel time. Specifically, level-of-service criteria are stated in terms of the average stopped delay per vehicle for a 15-min analysis period. The criteria are given in the Table.

Delay may be measured in the field, or may be estimated using procedures presented in the Highway Capacity Manual. Delay is a complex measure, and is dependent on a number of variables, including the quality of progression, the cycle length, the green ratio, and the v/c ratio for the lane group or approach in question.

Level-of-service A describes operations with very low delay, i.e., less than 5.0 sec per vehicle. This occurs when progression is extremely favorable, and most vehicles arrive during the green phase. Most vehicles do not stop at all. Short cycle lengths may also contribute to low delay.

LEVEL-OF-SERVICE CRITERIA FOR SIGNALIZED INTERSECTIONS

| Level of Service | Stopped Delay Per Vehicle (SEC) |
|------------------|---------------------------------------|
| A | ≤ 5.0 |
| B | 5.1 to 15.0 |
| C | 15.1 to 25.0 |
| D | 25.1 to 40.0 |
| E | 40.1 to 60.0 |
| F | > 60.0 |

Level-of-service B describes operations with delay in the range of 5.1 to 15.0 sec per vehicle. This generally occurs with good progression and/or short cycle lengths. More vehicles stop than for LOS A, causing higher levels of average delay.

Level-of-service C describes operations with delay in the range of 15.1 to 25.0 sec per vehicle. These higher delays may result from fair progression and/or longer cycle lengths. Individual cycle failures may begin to appear in this level. The number of vehicles stopping is significant at this level, although many still pass through the intersection without stopping.

Level-of-service D describes operations with delay in the range of 25.1 to 40.0 sec per vehicle. At level D, the influence of congestion becomes more noticeable. Longer delays may result from some combination of unfavorable progression, long cycle lengths, or high v/c ratios. Many vehicles stop, and the proportion of vehicles not stopping declines. Individual cycle failures are noticeable.

Level-of-service E describes operations with delay in the range of 40.0 to 60.0 sec per vehicle. This is considered to be the limit of acceptable delay. These high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent occurrences.

Level-of-service F describes operations with delay in excess of 60.0 sec per vehicle. This is considered to be unacceptable to drivers. This condition often occurs with oversaturation, i.e., when arrival flow rates exceed the capacity of the intersection. It may also occur at high v/c ratios below 1.00 with many individual cycle failures. Poor progression and long cycle lengths may also be major contributing causes to such delay levels.

RELATING CAPACITY AND LEVEL OF SERVICE

Because delay is a complex measure, its relationship to capacity is also complex. The levels of service in the Table have been established based on the acceptability of various delays to drivers. It is important to note that this concept is not related to capacity in a simple one-to-one fashion.

The lower bound of LOS E was defined to be capacity, in the 1965 Manual; i.e., the v/c ratio was, by definition, 1.00. This is not the case for the procedures of the 1985 Manual. It is possible, for example, to have delays in the range of LOS F (unacceptable) while the v/c ratio is below 1.00, perhaps as low as 0.75-0.85. Very high delays can occur at such v/c ratios when some combination of the following conditions exists: (1) the cycle length is long, (2) the lane group in question is disadvantaged

(has a long red time) by the signal timing, and/or (3) the signal progression for the subject movements is poor.

The reverse is also possible: a saturated approach or lane group (i.e., v/c ratio = 1.00) may have low delays if : (1) the cycle length is short, and/or (2) the signal progression is favorable for the subject movement. Thus, the designation of LOS F does not automatically imply that the intersection, approach, or lane group is overloaded, nor does a level of service in the A to E range automatically imply that there is unused capacity available.

The procedures and methods of the 1985 Highway Capacity Manual require the analysis of both capacity and level-of-service conditions to fully evaluate the operation of a signalized intersection. It is important to recognize the unique relationship of these two concepts as they apply to signalized intersections.

EXHIBIT B

DETAILED LAND USE INVENTORY

LARCHMONT-MAMARONECK MASTER PLAN UPDATE, PHASE 2
 BOSTON POST ROAD STUDY CORRIDOR

KEY TO LAND USES BY STREET ADDRESS (as of December 1986)

BOSTON POST ROAD

- 1235 McDonald's restaurant
- 1240 Unicorn Diner restaurant
- 1265 Cook's Restaurant
- 1260 EAST: Access to 6 story apartment buildings
 WEST: Village Square shopping plaza (from EAST to WEST):
 Videocassette rental shop
 Tano handbag shop
 J. Roberts Hair Salon
 Enjou Choclatier and ice cream shop
 Rainbow Color Lab and photography shop
 Kitty's Hallmark card shop
 La Hacienda restaurant
 Marisa's Cuisine restaurant
 Computerland
- 1280 Access to Central School
- 1289 Castro Convertables furniture
- 1300 David Potts VFW Post
- 1324 Finast Plaza (from EAST to WEST):
 Tung Hoy Chinese Restaurant
 Home Fair variety store
 Excelsior Cleaners
 Finast supermarket
- 1325 Dunkin' Donuts restaurant
- 1329 Kenny's retail carpets, rugs, linoleum and tile
- 1330 Barclay's bank
- 1340 Hory Chevrolet dealer
- 1345 EAST: Competition Bike and Sport Shop
 WEST: Seasons Too seasonal home furnishings
- 1350 Manufacturers Hanover bank
- 1361 Larchmont Motel
- 1370 Kentucky Fried Chicken restaurant
- 1375 International House of Pancakes restaurant
- 1380 Sentinel Leasing auto leasing

- 1395 Larchmont Auto Parts Distributors
- 1420 1st & Front: Casey's Restaurant
 2nd-6th & Rear: Alden House apartment house
- 1425 Eco Leasing Corp. 2 story office building

- 1435 Crabtree Ford/AMC/Renault dealer
- 1444-1456 Larchmont Gables 4 story apartment house

- 2000 Consolidated Edison substation
- 2001 Cleaning supplies sales and rentals
- 2015 Larchmont Village Garage and Public Works Buildings
- 2020 Mayer Volvo dealer
- 2021-2027 Tire dealer/warehouse
- 2030 Alfredo's Foreign Cars Alpha Romeo/Maserati dealer
- 2031 Auto Sound Specialists
- 2040 Bank of New York
- 2041 Hennessy Freda auto repair and gas station

BOSTON POST ROAD, Continued

- 2047 La Cafe d'argent restaurant
 2050 Larchmont Chrysler Plymouth dealer
 2055 Mobil gas station and repair shop
 2056 Dualities antiques
 2065 Larchmont Federal Savings
 2070-2072 1st: 2070: Larchmont Pharmacy
 2072: Kiderifics children's clothes
 2nd: SEE 4-6 Chatsworth Avenue
 2071-2075 Donaghy's Carriage Inn restaurant
 2074-2076 1st: 2074 Village Antiques
 2076 Decoration Day home furnishings, gifts, antiques
 2nd: Office, vacant
 2079 Michelle's Art Gallery
 2080 Fox Funeral Home
 2081 The Interior Shop
 2082-2084 1st: 2082 Adelfi Bros. Fuel Co. office
 2084 Village Paint Supply
 2nd & 3rd: 5 apartments
 2085-2087 1st: 2085 Nail and Skin care shop
 2087 Clock shop
 REAR Leasing services offices/storage
 2nd & 3rd: 4 apartments
 2086-2088 Foley's Hardware store
 2089 1st: Cartoon's bar
 2nd: Offices
 2090 Weichert Realtors
 2091 Lamp and lighting shop
 2094-2096 1st: 2094 PK's bar and restaurant
 2096 Stationery store
 2nd: A Dance Studio
 2095-2107A (ALBEE COURT BUILDING)
 1st: 2095-2097 Burbank realtors
 2097A Lingerie shop
 2099 Larchmont Chamber of Commerce
 2101 R. J. Plaut realtors
 2103 Fashion Studio
 2105 EAST Antique shop
 2105 WEST Hearing Aid Services and sales
 2107 Beauty shop
 2107A Santini Bros. Movers office
 2nd- 5th: Apartment house
 2100 EAST Women's apparel
 WEST Stained glass shop
 2104 1st: EAST Rug and Antique shop
 WEST Interior decorators
 2nd-4th: 8 apartments
 2106 1st: Drug store
 2nd: Office, vacant

BOSTON POST ROAD, Continued

3.

- 2110-2112: 1st: 2110 EAST Shoe repair shop
WEST Chinese Hand Laundry
2112 EAST Athletic Wear and Sporting Goods
WEST Liquor store
2nd: SEE 133-135 Larchmont Avenue
- 2111 Congregation Beth Emeth Synagogue
- 2114 1st: Children's Exchange used goods
2nd: SEE 124-132 Larchmont Avenue
- 2116 1st: My Hero sandwich shop
2nd: SEE 124-132 Larchmont Avenue
- 2118 1st: Carlyn's Cove bar and restaurant
2nd: 2 apartments
- 2120 1st: Karate instruction
2nd: 1 apartment
- 2122 1st: EAST vacant
WEST Links fabric and wallpaper shop
2nd: Westfair Realtors
- 2124 1st: EAST Children's Exchange used goods
WEST Oak and Acorns collectables, antiques
2nd: 2 apartments
- 2126 Larchmont Hair Stylists
- 2128 Robert Bahssin painting and sculpture gallery
- 2130-2136: 1st: 2130 Yesterday's used books and collectables
2130A Garson Realty
2134A Merle Norman cosmetics shop
2136 vacant
2nd-3rd: 16 apartments
- 2137 EAST Mavis tire store
WEST Woodpecker Shop furniture and custom woodworking
- 2138-2140 1st: 2138 EAST John Charles' Manor Place bar and restaurant
WEST Woolf's Den photo studio & antique shop
2140 The Shirt Place
2nd: 2138 2 apartments
2140 Marketing service & microfilm publisher offices
- 2147-2159 Cirillo Auto Body and Amoco gas station
- 2148 Barker Cemetery
- 2150 Quaker Cemetery
- 2155 vacant
- 2160 vacant
- 2165 vacant
- 2166 1 family residence
- 2172 1 family res.
- 2179 EAST P. Rigano insurance
WEST Sutton and Whittemore realtors
- 2180 Real estate appraisal office

BOSTON POST ROAD, Continued

4.

2212 1 family residence w/ dentist's office
2214 1 family res.
2216 1 family res.
2218 1 family res.
2220 1 family res.
2221 Kane Park
2222 1 family res.
2224 1 family res.
2226 1 family res.
2228 1 family res.
2230 1 family res. w/ doctor's office
2231 Kane Park
2232 1 family res.
2234 1 family res.
2250 1 family res.
2252 1 family res.
2254 1 family res.
2280 1 family res.
2285-2295 Tony's Nursery
2300 1 family res.
2315 Larchmont Nurseries
2325 EAST Great Bear automotive repair
WEST 1st: County Home Services- painting, house cleaning, etc.
2nd: Harmony realtors
2335 FRONT Robert Shwab Associates office
REAR (Facing Lorenzen Street) Hochhauser Leather Co. offices
2340 Pine Ridge multifamily condominiums
2349 Foster Trent Co. offices
2353 1 family residence
2362 vacant
2364 Owen Mandeville realtors
2365 Offices 1st: Metropolitan Life Ins., PMS Inc., Bruhnke & Silver
2nd: F. I. Zabriskie, Sandpiper Brokerage
2372 Thomas K. Salese Antique store
2375 Roy Rogers restaurant
2384 EAST vacant
CENTER vacant
WEST L. R. Loffredo kitchen and bath construction offices
2385 Getty gas station
2386 La Riserva restaurant
2388 C & E Auto Body
2394 ATI gas station and car repair
2399 Village Hut restaurant
2406 1st: Lumia interior decorators
2nd: 1 apartment
2408 1st: Willet vacuum sales and service
2nd: 3 apartments

BOSTON POST ROAD, Continued

- 2410-2412 1st: 2410 G & S TV repair
 2412 B & D Motor repair
 2nd: 1 apartment
- 2415 Parking for Stefanelli's restaurant
- 2416 Con Ed substation
- 2417 1st: Stefanelli's restaurant
 2nd: 3 apartments
- 2419-2421 1st(2419): G & R Marine Parts & hardware
 2nd(2421): Real estate office
- 2420 Larchmont Supply Co. plumbing, heating fixtures
- 2425 3 family residence
- 2428 Cartronics/Red Top car storage
- 2429 2 family residence
- 2430 Cartronics auto electric & Red Top auto body and glass repair shop
- 2432 Van Guilders Street Right Of Way
- 2434 Car wash and gas station
- 2441 vacant
- 2444 Loyal Inn Bowling Alley
- 2445 3 family residence
- 2451-2453 1st: Jimmy's Deli
 2nd-5th: Apartment house (entrance on Dillon Road)
- 2517 Northville gas station
- 2523 County Automotive rental and used car lot & office
- 2530 County Automotive Volkswagen dealer
- 2533 Alfredo's Imported Cars used car lot & office
- 2540 Lincoln-Mercury auto dealer
- 2553 AAMCO transmission repair shop

ADDISON STREET

- 1 1st: SEE 26-28 Chatsworth Avenue
- 2nd-4th: 14 apartments
- 7 1st: EAST Toni Gales' handbags
WEST Marketplace Gifts
- 2nd 3 apartments
- 9-11 FRONT- 1st: 9 EAST Mademoiselle Hair Design
WEST Larchmont Meateria deli and meat shop
- 11 EAST Stefis gifts
WEST Flower Bar florist
- 2nd:11 6 apartments
- REAR 11 1 family residence
- 13 1 family residence
- 17 1st: SEE 159-161 Larchmont Avenue
- 2nd-5th: 28 apartments

CHATSWORTH AVENUE

- 3 U.S. Post Office
- 4-6 1st: 4 Florist
- 6 SOUTH Dog Grooming salon
- NORTH Pizzeria
- 2nd: Craftsman's Crucible and engineer's offices
- 10-12 1st: 10 Shoe repair shop
- 12 SOUTH Fish market
- 12 NORTH From Me To You Services: copying, packaging and mailing
- 2nd-4th: 22 apartments
- 14 Imperial Milk Mart convenience store
- 16 Betty's Cheese Store
- 18 SOUTH Luncheonette
- CENTER Liquor store
- NORTH Jewelry store
- 19 SOUTH Green Mountain Sportswear athletic clothes
- NORTH Camera and film shop
- 22A Chatsworth Pastry Shop
- 26-28 1st: 26 Larchmont Healthmart
- 26A Italian-American Deli
- 28 The Checkerboard Gallery antiques and fine art goods
- 2nd-4th: SEE 17 Addison Street
- 29 Grand Union Supermarket

MONROE AVENUE

- 48 3 family residence

LARCHMONT AVENUE

- 100 Village Hall Park
- 101 Saint Augustine Roman Catholic Church
- 105 St. Augustine Elementary School
- 119 Village Center Building
- 121 Larchmont Library
- 122 Larchmont Municipal Building
- 124-132 1st:(f/ 2114 Post Road to 136 Larchmont Ave.)
- Abbott Travel
- Houlihan-Lawrence Real Estate
- Kitchens by Vigliotti
- D. Masters Jeweler
- Hair salon
- Videocassette store

LARCHMONT AVENUE, Continued

7.

124-132, Continued

- 2nd: (2216) Larchmont Employment Agency
- (2214) Dentist's Office
- (124-132) Diet Center
- 133-135 1st: 133 Chiropractor's Office
- 135 Pretty Package gifts
- 2nd: Lawyer's offices
- 136 Tailor shop
- 137-139 1st: 137 Manor Park Deli and sandwich shop
- 139 Graffitti women's clothing
- 2nd-3rd: 6 apartments
- 138 Men's clothing and tailoring
- 140 Real estate office
- 141 The Golden Bowl decorating materials and antiques
- 141A Plumbing contractor's office
- 142 Parking for #136-140
- 144 Playtime toy store
- 145-149: 145 Pancho Villa's Mexican restaurant
- 149 Poster and print shop
- 146 Hat shop
- 150 Mamaroneck Artists' Guild gallery
- 151-157 1st: 151 Paint and wallpaper store
- 153 Delicatessen
- 155 SOUTH Frame shop
- NORTH Needlework shop
- 157 Travel agency
- 2nd-3rd: 16 apartments
- 152A Silver Canvas Yarn and Fabric Shop
- 152B Vienna Hair Stylist
- 154 Morgans Bar
- 156-158 1st: 156-158 Marimo Japanese restaurant
- 2nd: 4 apartments, 1 w/ accessory use (dressmaker's shop)
- 159-161 1st: 159 Stepping Stones gifts
- 161 Oriental rugs
- 2nd: SEE 17 Addison Street

WEAVER STREET

- 24 2 family residence
- 30 Finast Plaza parking lot