

DES MOINES TRANSPORTATION SAFETY COMMITTEE
MAY 13, 2025
MINUTES

The Des Moines Transportation Safety Committee met in the Board Room at the Municipal Service Center, 2nd Floor, 1551 E Martin Luther King Jr Parkway, at 7:30 a.m. on May 13, 2025. Those members in attendance were:

Renda Barbieri-Snyder
Shelby Ebel
Matt Hardin
Andy McCoy

Brianne Sanchez
Julian Van Liew
Tony Filippini
Kristin Hanks-Bents

Members Absent: Jarrett Belden

Staff Present: John Davis, Jen Dakovich, Corey Bogenreif, Molly Long, Sgt. Chip Beardsley, and Emily Voeller

Guests Present: Carol Maher, Tyler Wiles, and Alec Davis

The committee approved by acclamation the May 13, 2025, agenda.

OLD BUSINESS

1. Approval of March Minutes

The committee approved by acclamation the March 11, 2025, minutes.

Matt Hardin welcomed new member, Kristin Hanks-Bents, to the Transportation Safety Committee.

NEW BUSINESS

1. Downtown Peak Hour Parking Restriction Update

Corey Bogenreif, Principal Traffic Engineer, defined the peak hour parking restrictions, identified issues, and reviewed the restriction map and elimination process.

2025 – 8th Street & 9th Street

9th Street

- Realign lanes on 12th Street and Keo Way with new pavement markings
- Close one-way access lanes between Mulberry Street and Cherry Street as part of the SW 9th Street Viaduct Project

8th Street

- Close access lane on south portion of 8th Street
- Remove signs and signals
- New pavement markings

2026 – 2nd Avenue & 6th Avenue

2nd Avenue – Court Ave to I-235

- Reduce lanes
- Add permanent parking
- Add curb Extensions
- Dedicate loading zone by Wells Fargo Arena
- Add on-street parking for Lauridsen Skatepark

6th Avenue – Crocker Street to University Avenue

- 3rd Phase of Streetscape Project

DES MOINES TRANSPORTATION SAFETY COMMITTEE

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- Reduce lanes
- Install sidewalk
- Add bike facility

Committee Discussion

Andy McCoy asked if East Village is considered downtown. Corey Bogenreif said yes. There is a concept plan to convert E 6th Street and Pennsylvania Avenue into two-way traffic. As part of the project parking restrictions would be removed. The project is not currently funded and requires an interchange justification report, which must be submitted to the IDOT to make modifications to the interchange. Funding for the study is in the capital improvement program in FY2029.

Matt Hardin asked if it was possible to put barricades on 8th Street and 9th Street rather than curb extensions to make sure individuals don't enter the parking lanes. Corey Bogenreif stated the lanes would be restriped to keep vehicles in proper lanes and referred to High Street as an example of potential changes. Corey Bogenreif stated the peak hour parking restriction was removed on 7th Street in 2018 or 2019.

Public Comments

Alec Davis supports reducing travel lanes. He would like to see space repurposed to create more protected bike lanes rather than additional parking lanes. Corey Bogenreif stated the 6th Avenue project will include protected bike lanes, with a side path eventually to 5th Avenue and down to Grand Avenue. 9th Street north of Keo Way is on the bike network and is a location we would look to add a bike facility in the future. We are looking for funding to head further north with bike lanes.

2. Vision Zero Transportation Safety Action Plan – 2024 Annual Report

Corey Bogenreif, Principal Traffic Engineer, stated the annual report is in draft form. The committee feedback will be presented to City Council, and a final version will be published online. The Vision Zero Transportation Safety Action Plan was adopted in June 2023. Vision Zero is a strategy to eliminate all traffic fatalities and severe injuries. Vision Zero starts with the ethical belief that everyone has the right to move safely in their communities and that system designers and policymakers share the responsibility to ensure safe systems for travel. The Vision Zero approach recognizes that people will sometimes make mistakes, so the road system and related policies should be designed to ensure those inevitable mistakes do not result in severe injuries or fatalities. Corey Bogenreif provided a summary of the action plan, goals, crash data, progress, and intervention highlights.

The Ingersoll Avenue and 6th Avenue Streetscape projects were highlighted to show how implementing various safety measures directly impacted safety outcomes.

An update was given on the two roundabouts installed last year as part of the Polk County Broadway Avenue Project. While individuals tend to dislike them, the safety benefit has been proven and has resulted in a significant reduction in fatal and injury crashes.

Two projects were completed on the high injury network.

- E University from E 14th Street to Easton Boulevard
- University Avenue from 31st Street

Corey Bogenreif gave an update on the Safe Streets For All (SS4A) implementation schedule and presented upcoming studies.

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Committee Discussion

Matt Hardin asked for clarification on how the speed reduction on 6th Avenue was captured. Corey Bogenreif stated that the data is captured by spot speed, meaning a radar is set in the middle of the block and each vehicle that drives by is collected.

Andy McCoy asked if the volume of traffic on Ingersoll Avenue has changed, to include pedestrian and bicycle traffic. Corey Bogenreif did not have that information available at the meeting but stated the traffic is roughly the same. Jen Dakovich said we can find vehicular traffic. Brianne Sanchez thought she could get pedestrian and bicycle data from various partners. Corey Bogenreif said data reveals an increase in bicycle volume, but he did not have pedestrian volume data.

Renda Barbieri-Snyder stated the changes on E University Avenue are greatly appreciated.

Julian Van Liew thanked staff for their commitment. She stated the Polk County Health Department would love to partner with the City to provide public education materials. Corey Bogenreif said John Davis would reach out to her.

Andy McCoy stated the investment of a dedicated staff member would be a great return on investment. Jen Dakovich and Corey Bogenreif stated they continue to ask for additional funds.

Matt Hardin asked if a similar presentation would be made to Council. Corey Bogenreif said he will work with the City Manager's office to determine how the information will be presented to council members. After the Committee's comments have been presented to the Council, the final Vision Zero Action plan will be published online. Matt Hardin would love to see the Council choose to prioritize the "Priority 1" sidewalk gaps.

Matt Hardin stated he would like the presentation to capture how various street design elements impact the crash data and can build safer environments, rather than overemphasizing the driver's and victim's behaviors. Corey Bogenreif stated the information is intended to provide all factors that play into serious injury and fatal crashes and point out the many factors that create a safe system.

Public Comments

Carol Maher thanked the staff and echoed Matt Hardin's comments regarding focusing the report only on street design elements. She noted there have been no serious injuries or fatalities reported since the Ingersoll Avenue streetscape has been implemented.

NEXT MEETING

Tuesday, June 10, 2025

Adjourned at 8:44 a.m.

Respectfully submitted,

/s/Emily Voeller

Emily Voeller

Recording Secretary

DES MOINES TRANSPORTATION SAFETY COMMITTEE
MAY 13, 2025

	Old Business Item 1	
	Y	N
Renda Barbieri-Snyder	X	
Jarrett Belden		
Shelby Ebel	X	
Juliann Van Liew	X	
Andy McCoy	X	
Tony Filippini	X	
Matt Hardin	X	
Brianne Sanchez	X	
Kristin Hanks-Bents	X	

TRANSPORTATION SAFETY COMMITTEE

GUEST ATTENDANCE

Municipal Service Center--Board Room (2nd Floor)

1551 E Martin Luther King Jr Parkway

Tuesday, May 13, 2025, at 7:30 a.m.

PLEASE SIGN IN IF YOU WOULD LIKE TO BE INCLUDED IN FUTURE MAILINGS ON THE TOPIC OF THIS MEETING.

PLEASE BE ADVISED THAT THIS INFORMATION MAY BE RELEASED TO THE PUBLIC UPON REQUEST.

NAME/BUSINESS	ADDRESS	EMAIL	PHONE	ITEM #
OPRAH MAJOR	Whane			
TYLER LILES				

TRANSPORTATION SAFETY COMMITTEE REGULAR MEETING – MAY 13, 2025

ITEM #1 NEW BUSINESS

ITEM #1: Downtown Peak Hour Parking Restriction Update

BACKGROUND:

Peak hour parking restrictions are restrictions that allow travel lanes to be used for parking for part of the day and as vehicular lanes the other portion of the day to create extra capacity to support rush hour (peak hour) traffic. This type of parking restriction is located mostly downtown in Des Moines. While peak hour parking restrictions may seem to provide efficient use of limited street space, they have unintended consequences on streets where there is more street capacity than needed for existing traffic volumes. For under capacity streets, they result in underutilized parking throughout the day and create extra wide streets that support higher speeds and detract from the pedestrian environment. Based on existing and projected traffic patterns, these restrictions are largely unnecessary from a traffic flow perspective on most downtown streets.

Based on the data collected and analyzed as part of Connect Downtown, the downtown mobility study adopted in 2018, it was recommended that the peak hour parking restrictions on downtown streets be eliminated.

Peak hour parking restrictions on several streets have already been removed, including most recently on Center Street, High Street, Grand Avenue, and 10th Street in 2024. Additional changes are planned to several streets, shown in the map below, in 2025 and in 2026.

This agenda item will provide a summary of the background, summary, challenges, and plans to eliminate peak hour parking restrictions on downtown streets.



STAFF RECOMMENDATION:

This item is for informational purposes only. No action is required.

Downtown Peak Hour Parking Restriction Update

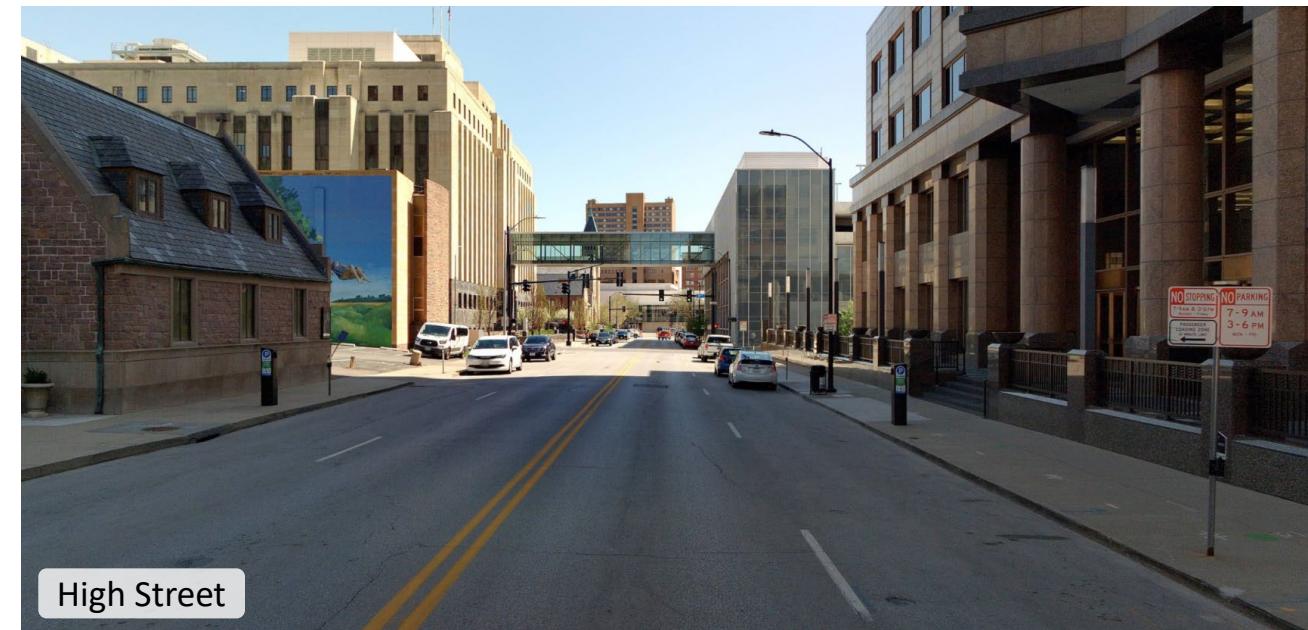
Transportation Safety Committee
May 13, 2025

Corey Bogenreif, PE – Principal Traffic Engineer



What is a Peak Hour Parking Restriction?

- Peak hour parking restrictions create parking for part of the day while creating extra travel lanes to support rush hour (peak hour) traffic
- Mostly located downtown but does exist on other streets

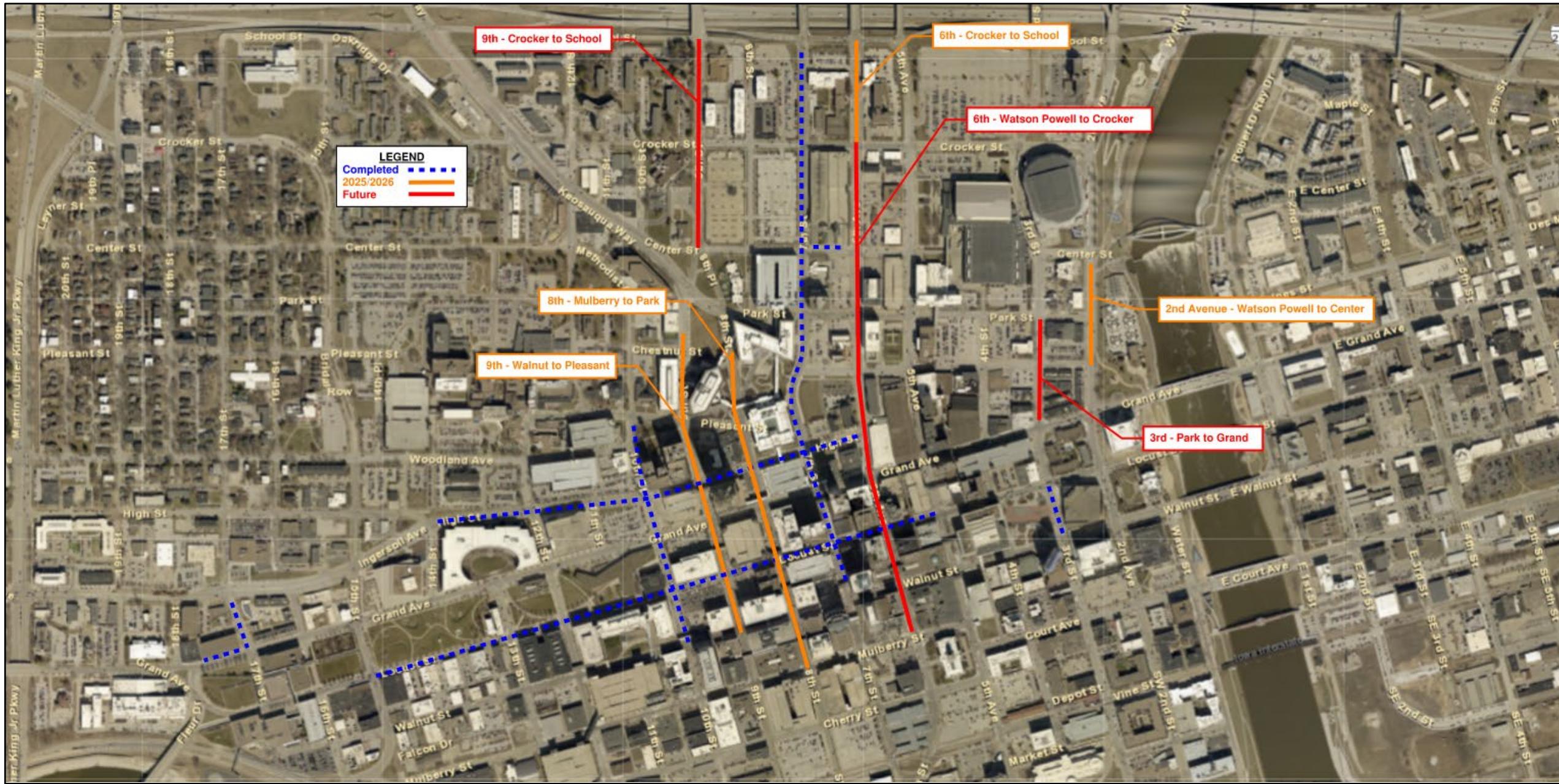


What's the issue?

- Traffic patterns change. Most streets downtown have enough capacity without utilizing the parking lanes during peak hours
- Results in underutilized parking throughout the day
- Create weaving issues during peak hours if a car is “illegally” parked
- Driver confusion/driver expectancy
- Create extra wide streets that support higher speeds & detract from pedestrian environment
- Connect Downtown (2018) recommended eliminating all peak hour parking restrictions



Peak Hour Parking Restrictions Map

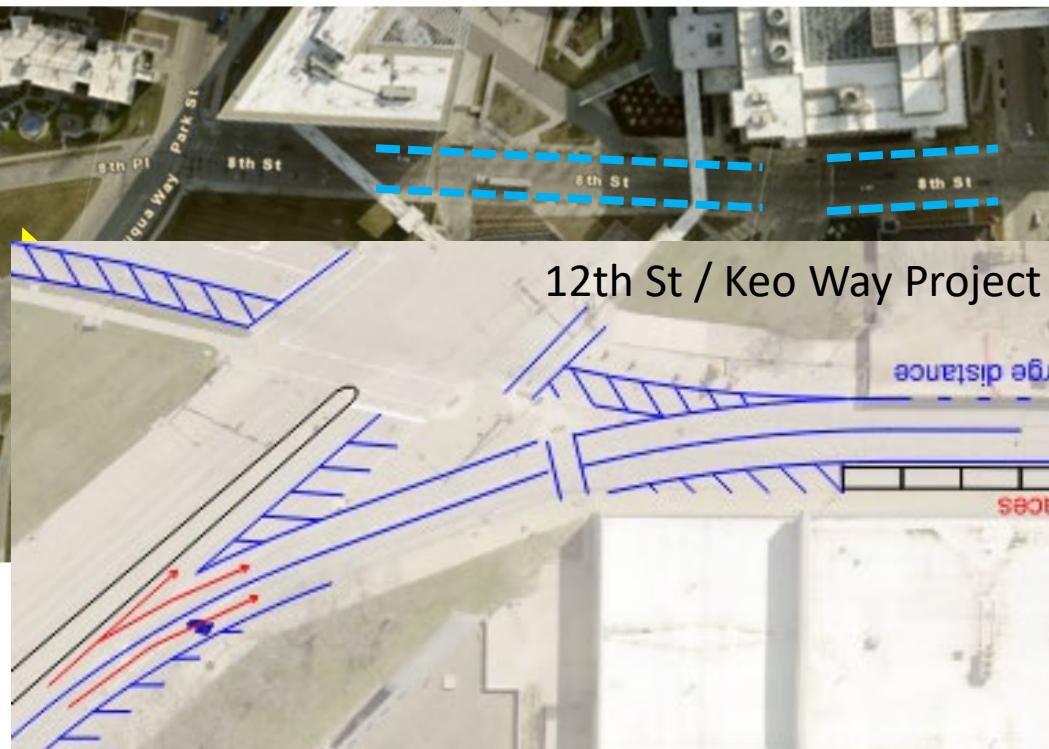


Restriction Elimination Process

- Why don't you just remove the signs and call it a day?
- Some locations have little to no demand for on-street parking
- More complicated than it seems:
 - Pavement Markings – removals and new
 - Traffic signal adjustments
 - Signage review, removal, and new
 - Loading zone review
 - Parking meter review and adjustment
 - Code changes – council action
 - Coordination with Police / Enforcement
 - Typically, too large of project for City Staff

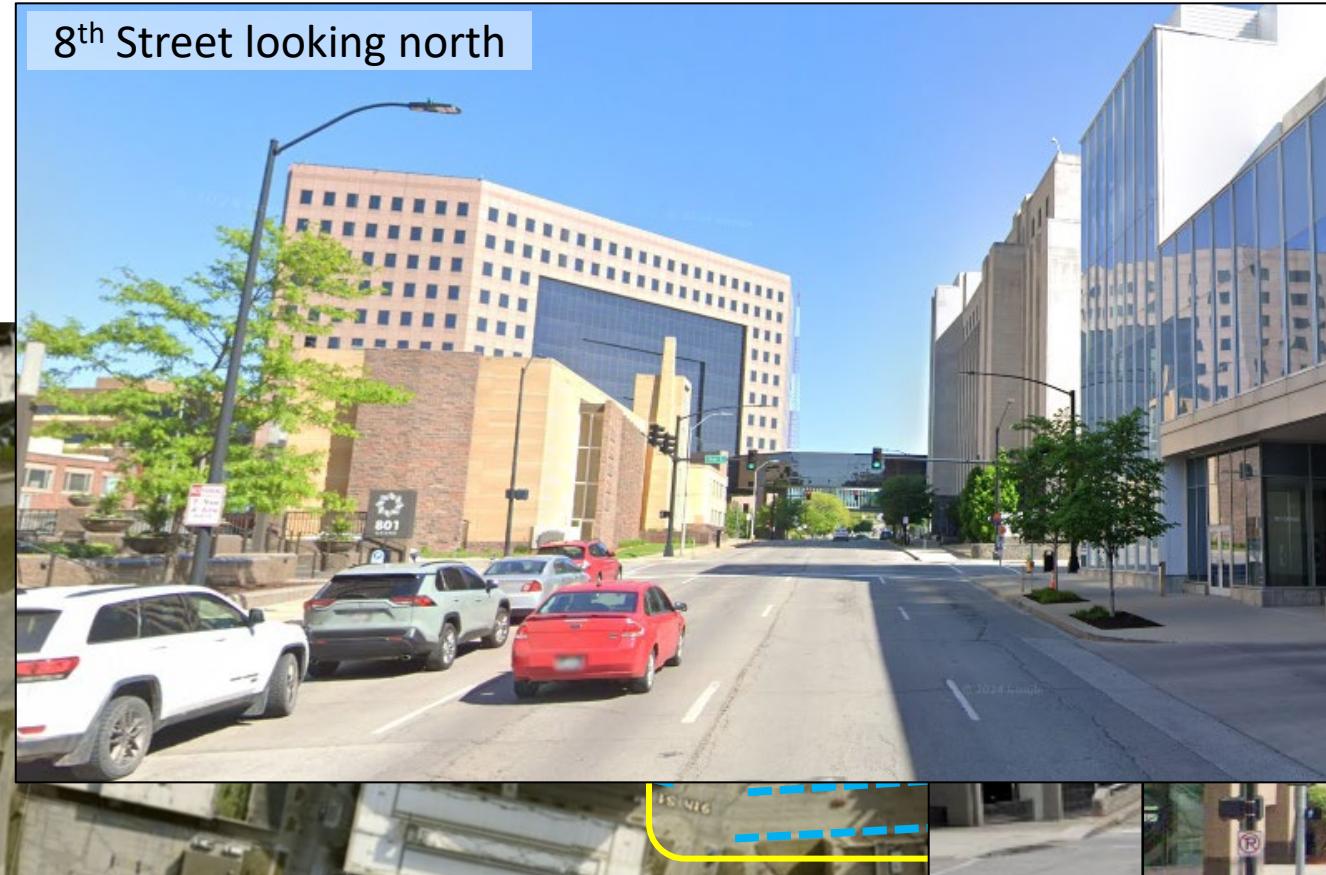


2025 – 8th Street & 9th Street



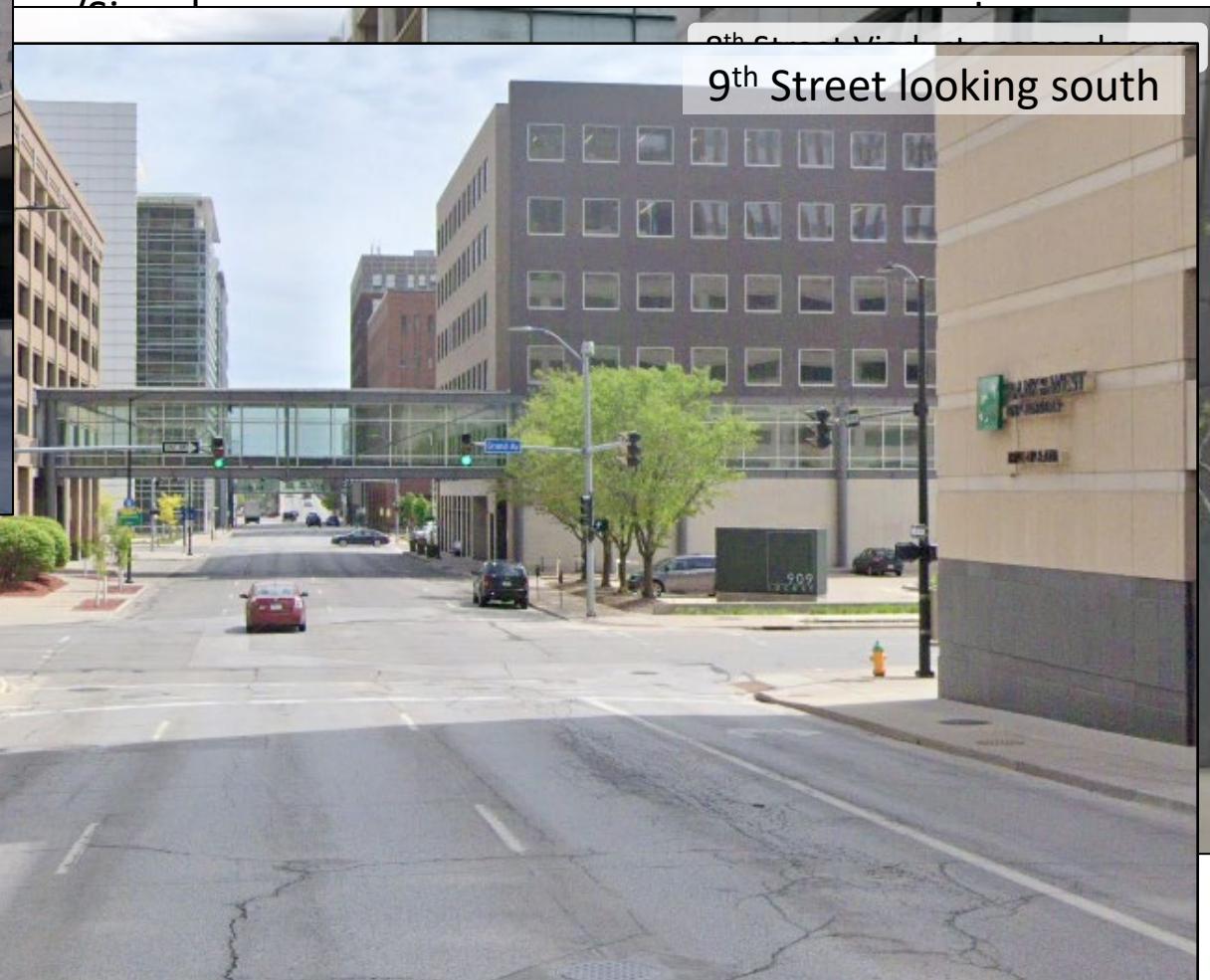
2025 – 8th Street & 9th Street

8th Street looking north



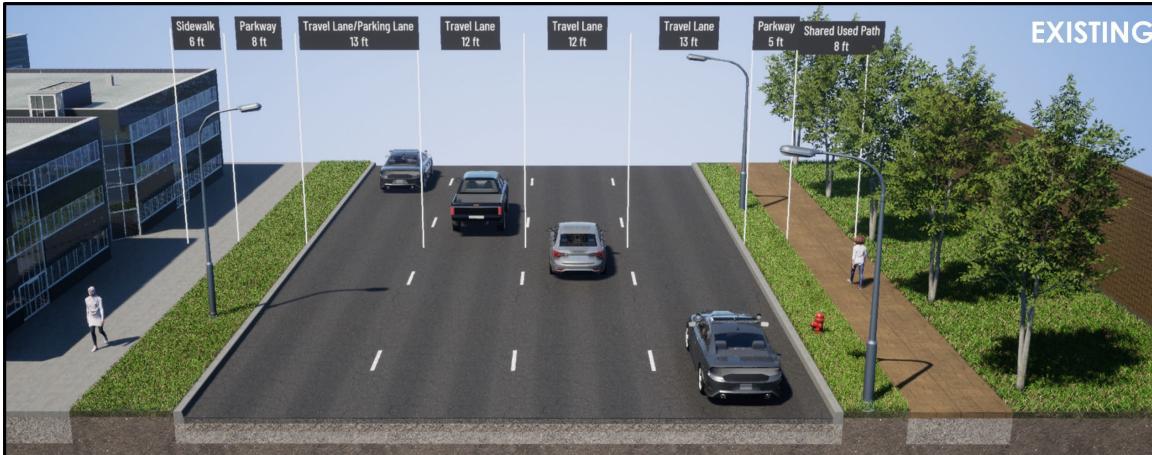
8th Street
Viaduct access

9th Street looking south

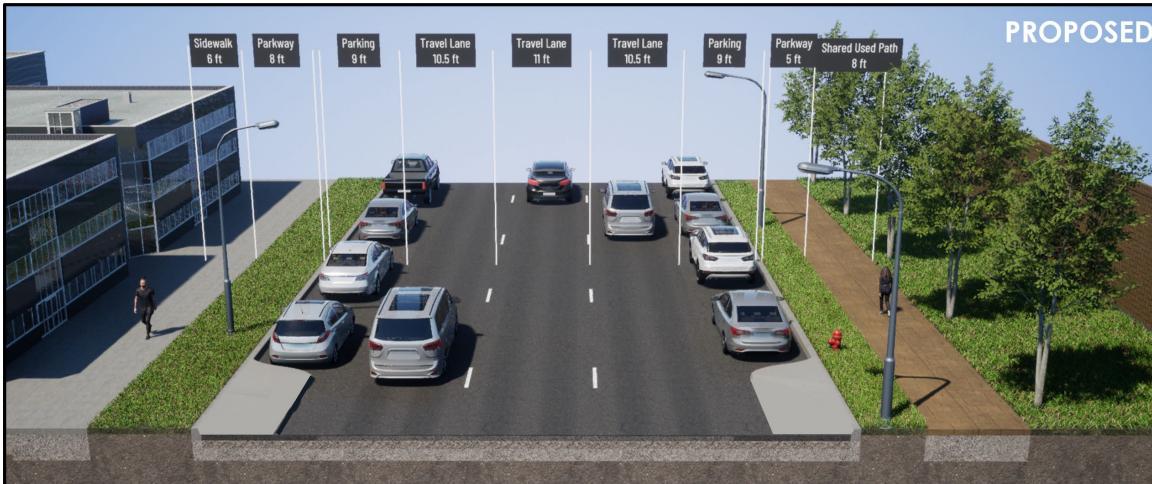
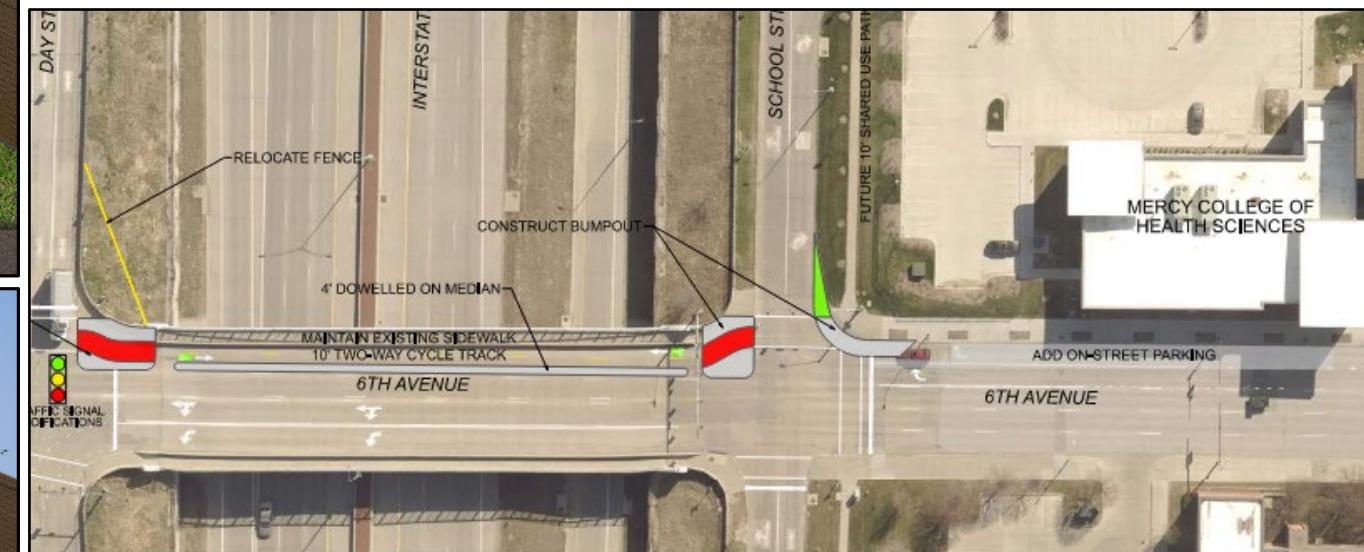


2026 – 2nd Avenue & 6th Avenue

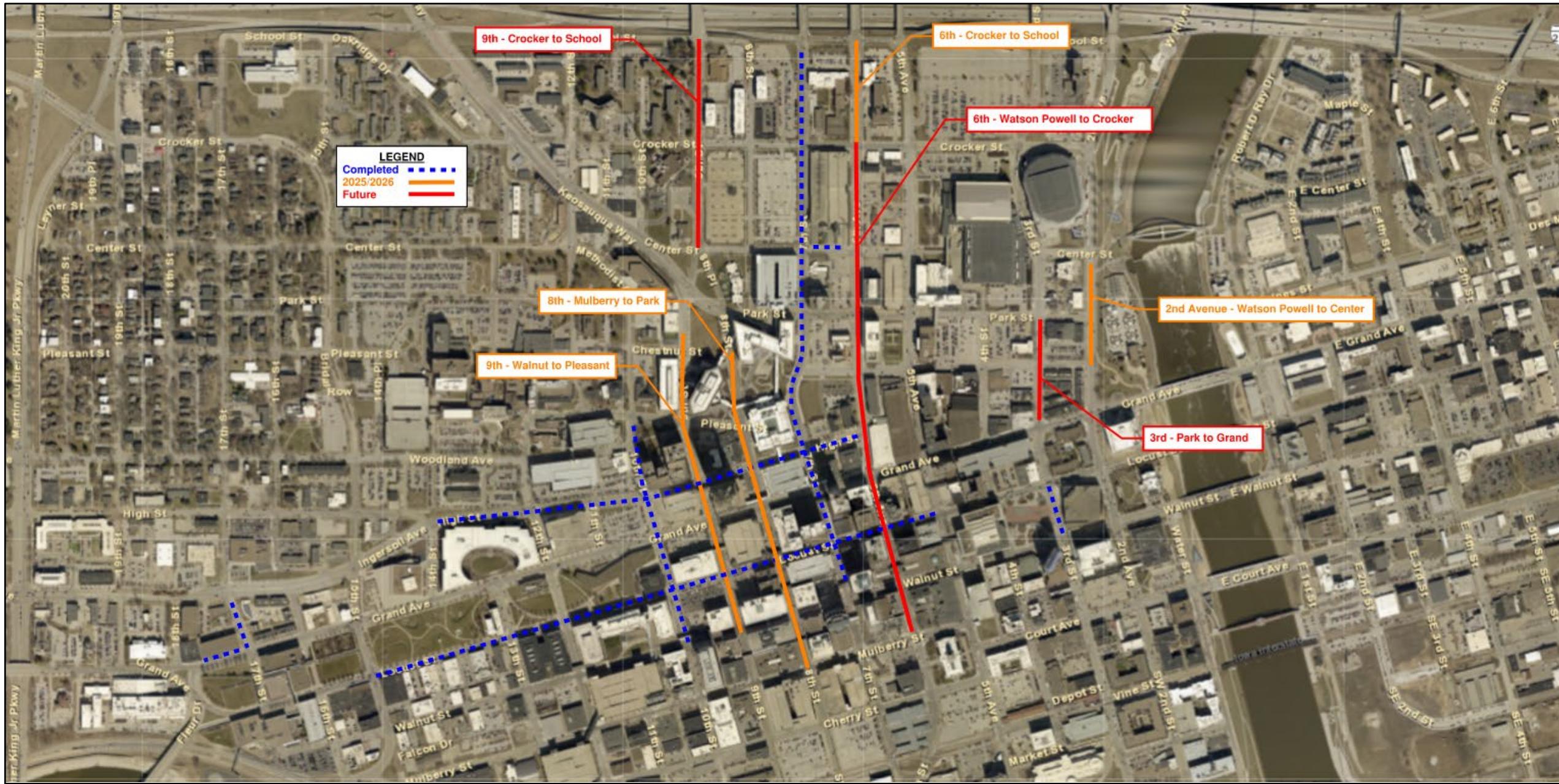
2nd Avenue – Court to I-235



6th Avenue – north of Crocker Street



Peak Hour Parking Restrictions Map



Question?



Vision Zero Transportation Safety Action Plan 2024 Annual Report

Transportation Safety Committee
May 13, 2025

Corey Bogenreif, PE – Principal Traffic Engineer

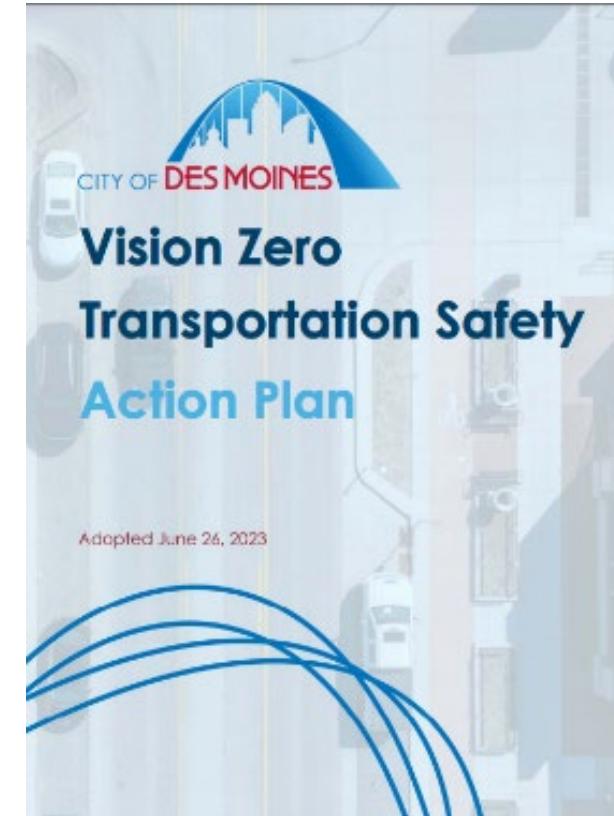


Vision Zero Transportation Safety Action Plan Refresher

Safe System Approach



Action Plan



Adopted June 24, 2023

Action Plan Focus Areas



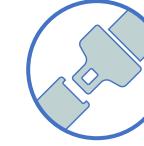
City Policies, Programs, Processes, and Partnerships



Safe Streets for Everyone



Safe Speeds



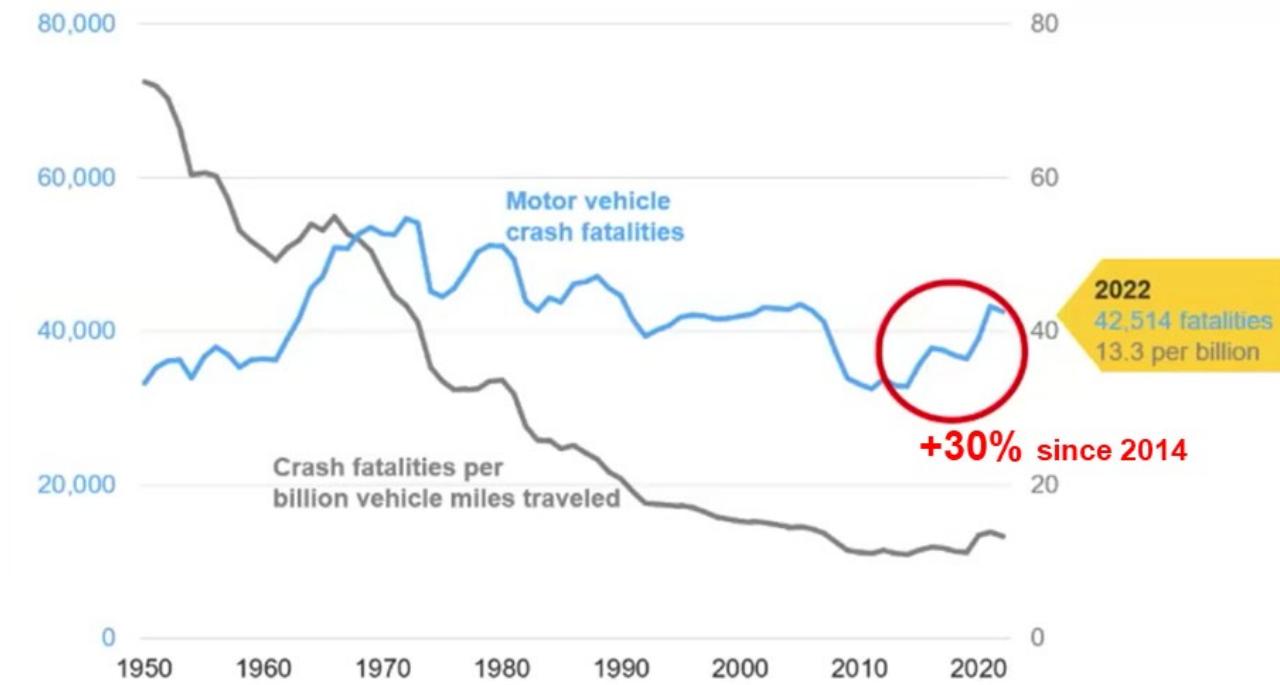
A Culture of Safety and Safe Street Behaviors



Improve Data and Transparency

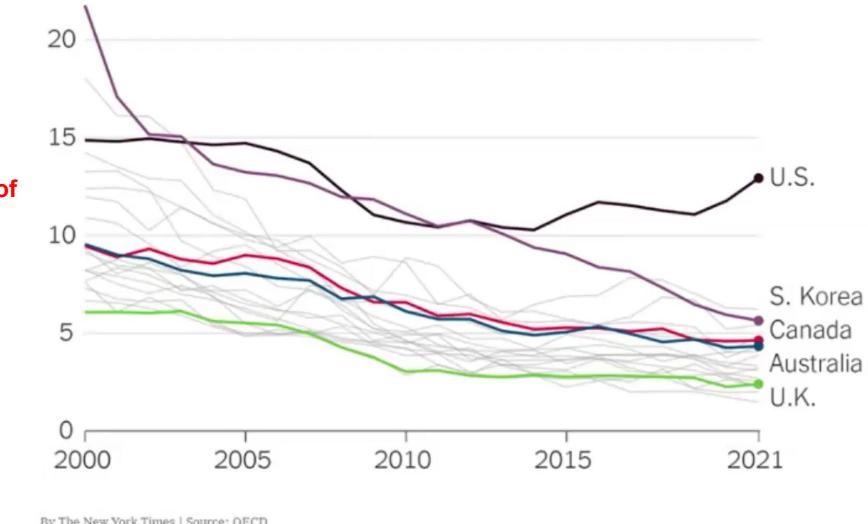
Why Vision Zero?

U.S. motor vehicle crash deaths 1950-2022

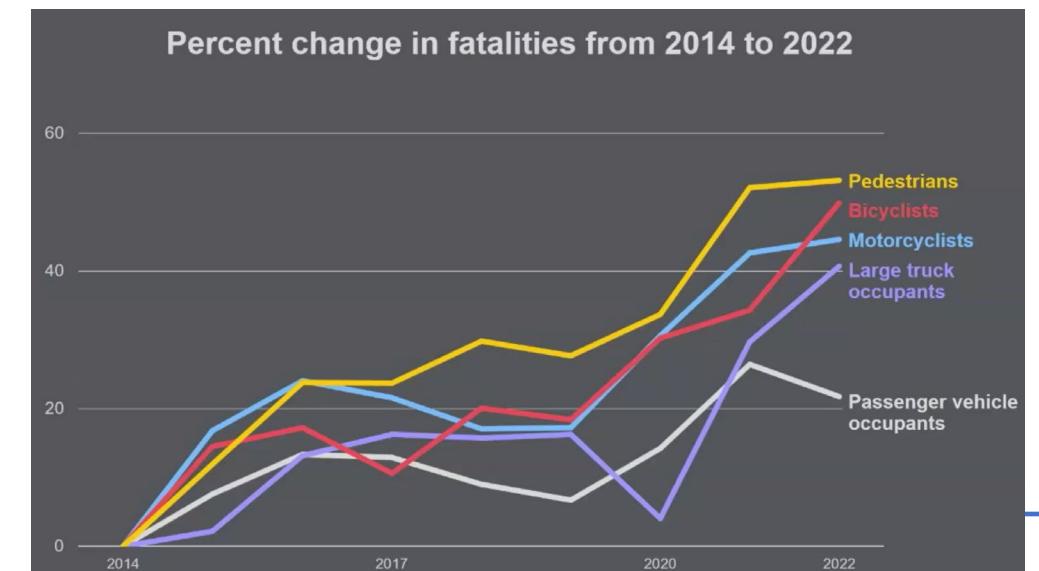


US Fatality rate is
2.3 times higher
than the average of
the next 28 high-
income countries

Vehicle deaths per 100,000 residents 20 rich countries



Percent change in fatalities from 2014 to 2022

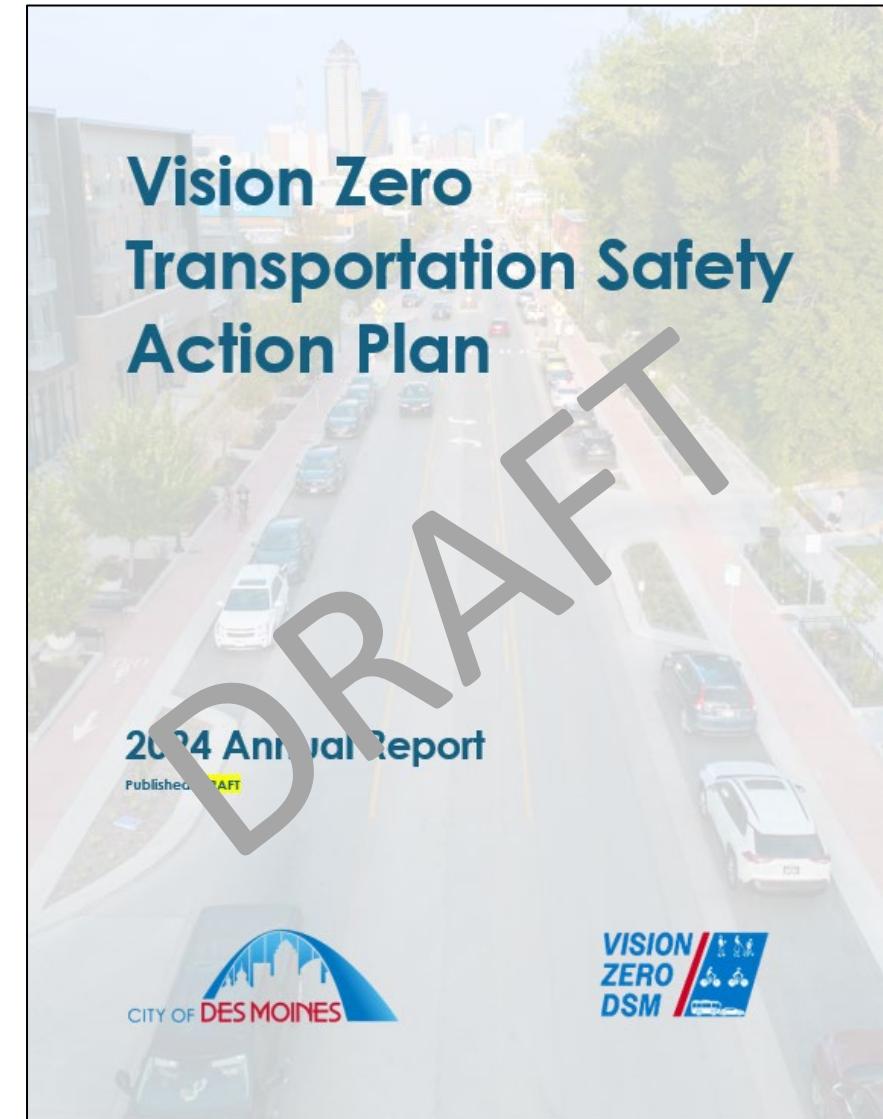


2024 Annual Report

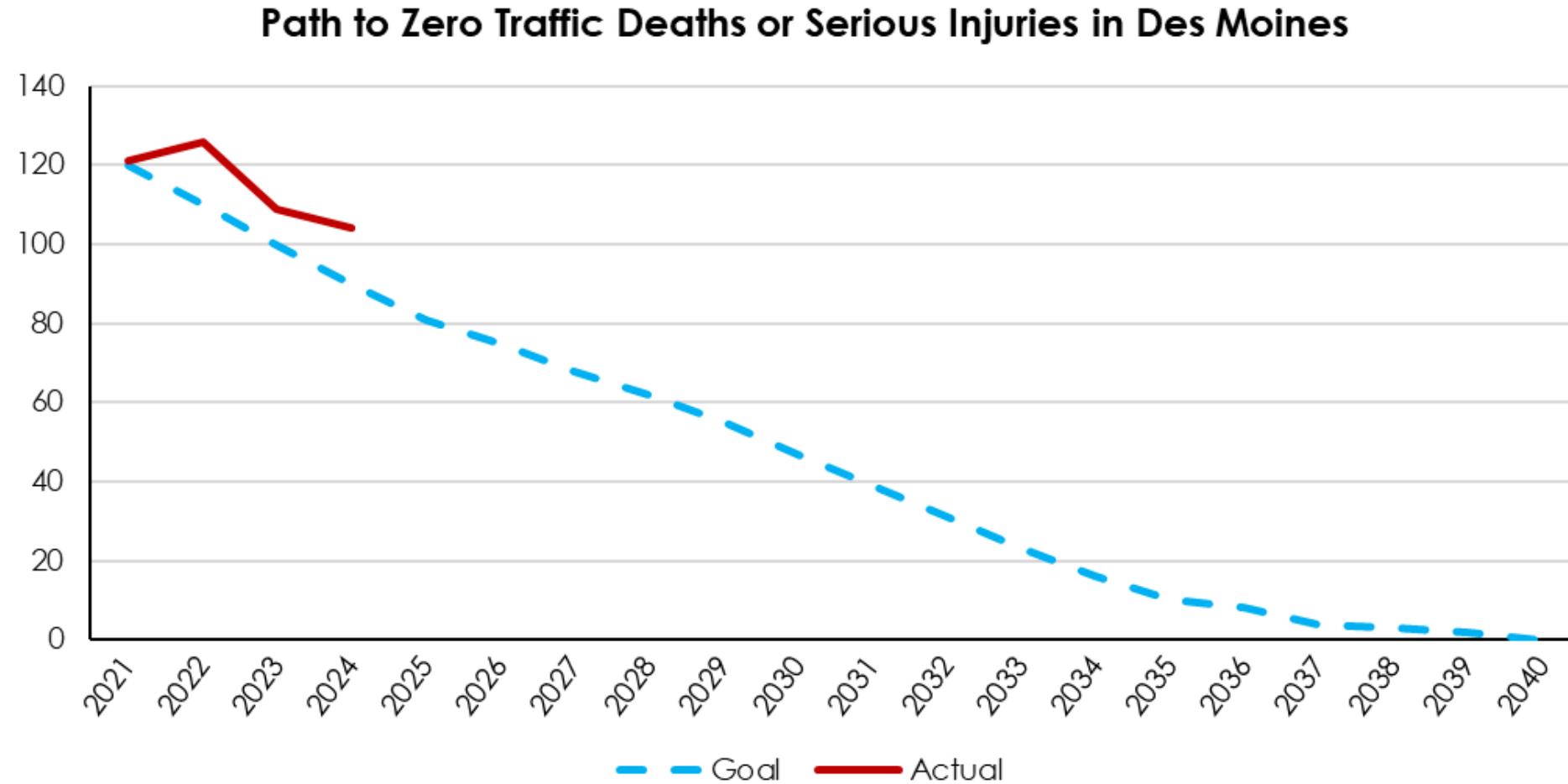
2nd Annual Report

Action Plan includes an annual report to:

- Track progress toward fatality, serious injury, and overall crash reduction
- Summarize the effects of safety interventions
- Detail any adjustments to the Vision Zero program

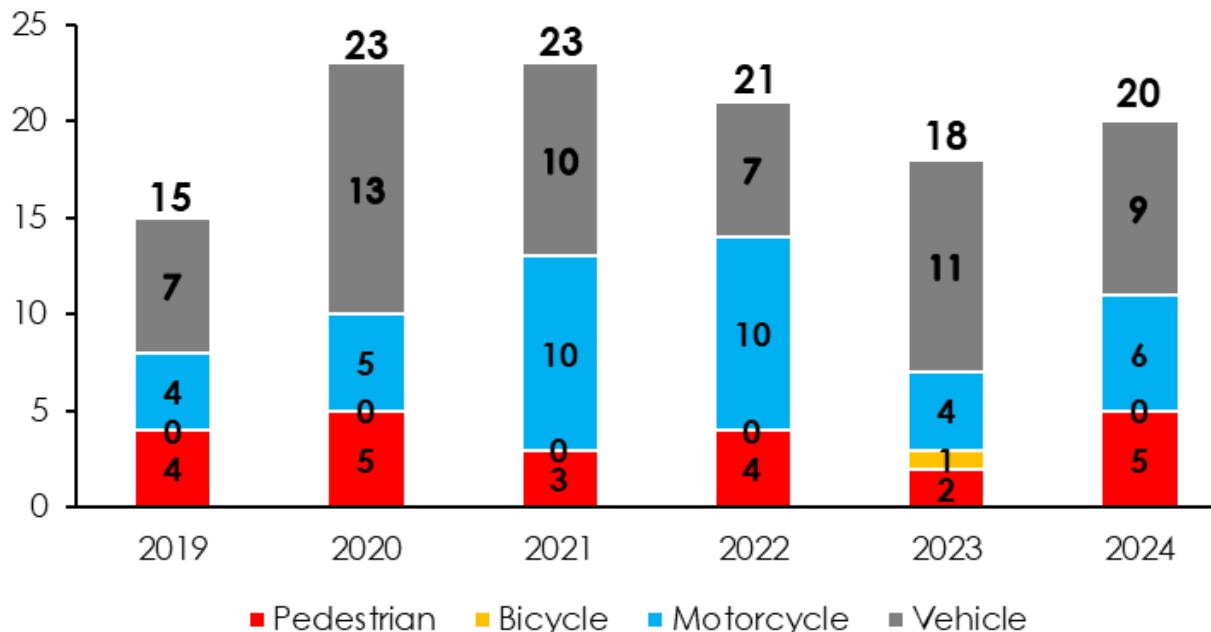


Crash Data and Path to Zero

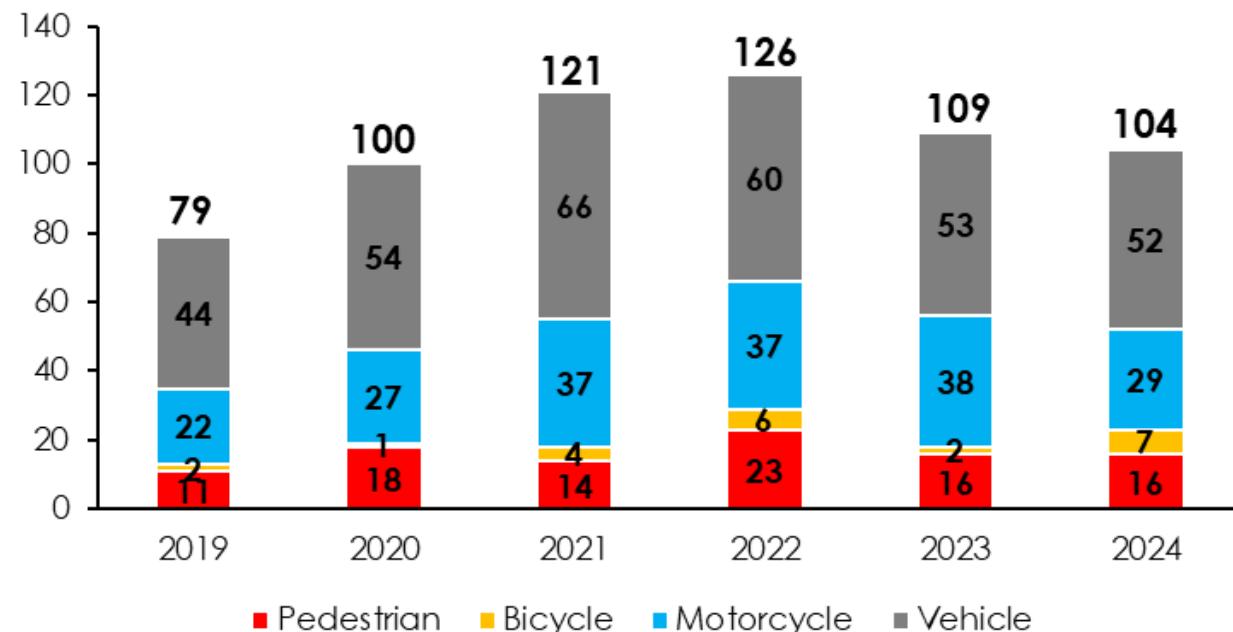


Crash Data and Path to Zero

Fatal Injuries



Fatalities + Serious Injuries



*As of May 12, 3 fatalities & 23 serious injuries in 2025

Progress on Implementation

19 Completed

20 On Track

Highlights:

Action 1.1, Step 3: Establish dedicated funding source (\$500,000 in CIP starting in FY27)

Action 1.3, Step 4: Coordinate with DMAMPO on Comprehensive Safety Action Plan

Action 1.5, Step 1: Apply for USDOT SS4A Grant Funding (\$13 million grant secured)

Action 3.1, Step 1: Citywide Speed Management Study programmed for FY2026

Action 5.2, Step 3: Before and After Studies



Safer Streets DSM:
Prioritizing People
Safe Streets and Roads for All
May 2024



Progress on Implementation

3 Not Completed

Action 1.1, Step 4: Dedicated Vision Zero Staff

Action 1.5, Step 3: Increase operations funding for long-term maintenance of street improvements

Action 5.3, Step 2: Create and maintain Data Dashboard with regular updates

16 Modified

Most of these action steps cannot be done with current staffing. Examples:

Action 2.3, Step 1: Establish interagency crash rapid response team

Action 4.1, Step 7: Create online pledge form and manage program

Action 1.5, Step 2: Host a Roadway Safety Day annually

Action 4.3, Step 2: Develop public education materials

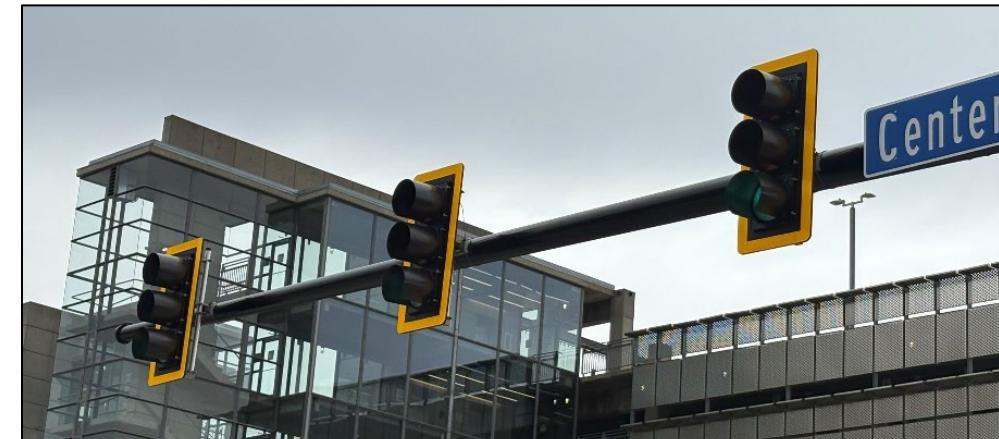
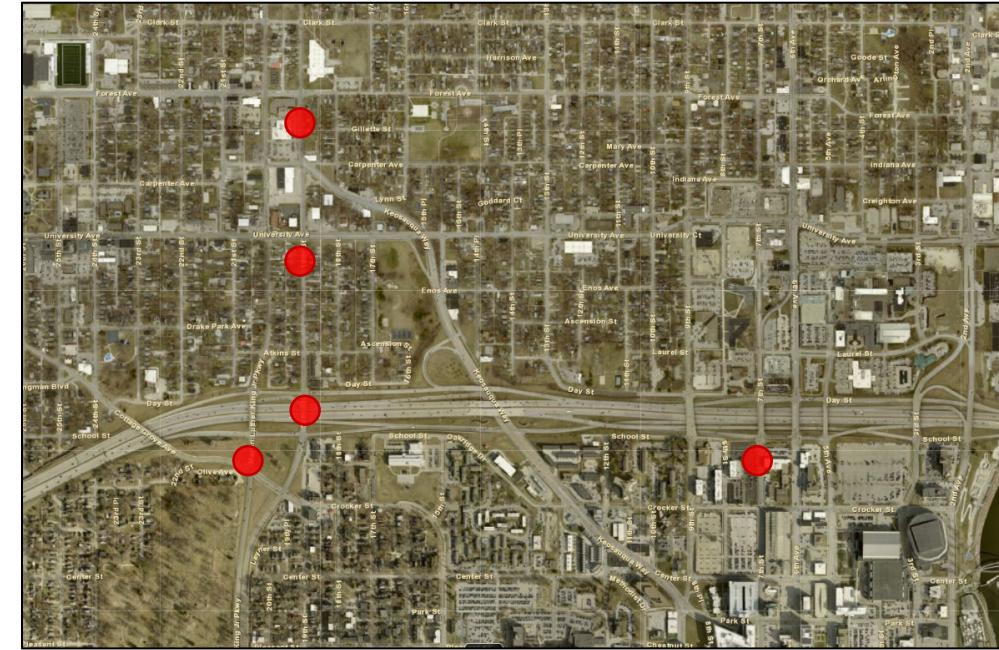
Action 5.3, Step 3: Conduct annual public survey



Figure 10. Pop-up event interaction at DART Central Station on August 25, 2022. Source: Confluence

Safety Intervention Highlights Retroreflective Backplates

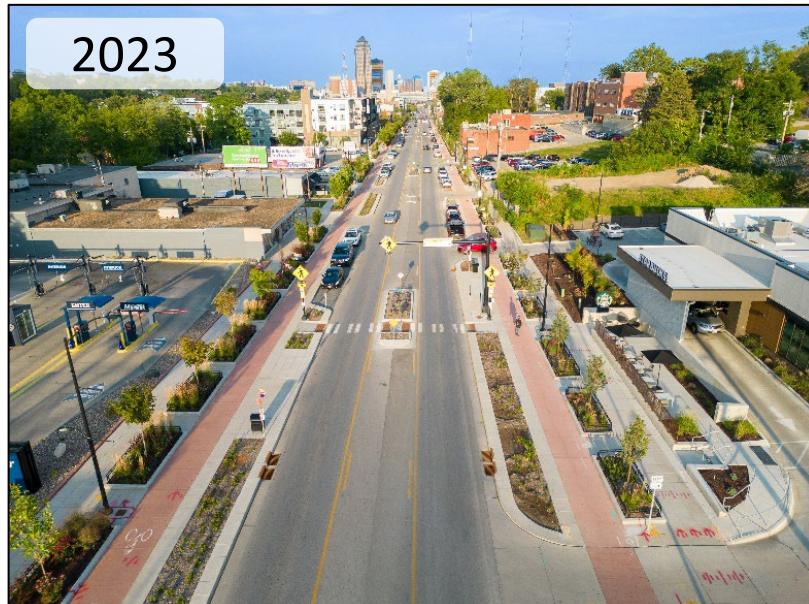
- Pilot implementation at 5 locations in late 2023
- Locations chosen based on history of red-light running crashes
- Before & after data in Des Moines showed a 20% reduction in red light running crashes
- **Standard for all signal projects moving forward**



Ingersoll Avenue Streetscape ML King Jr. Parkway to 28th Street

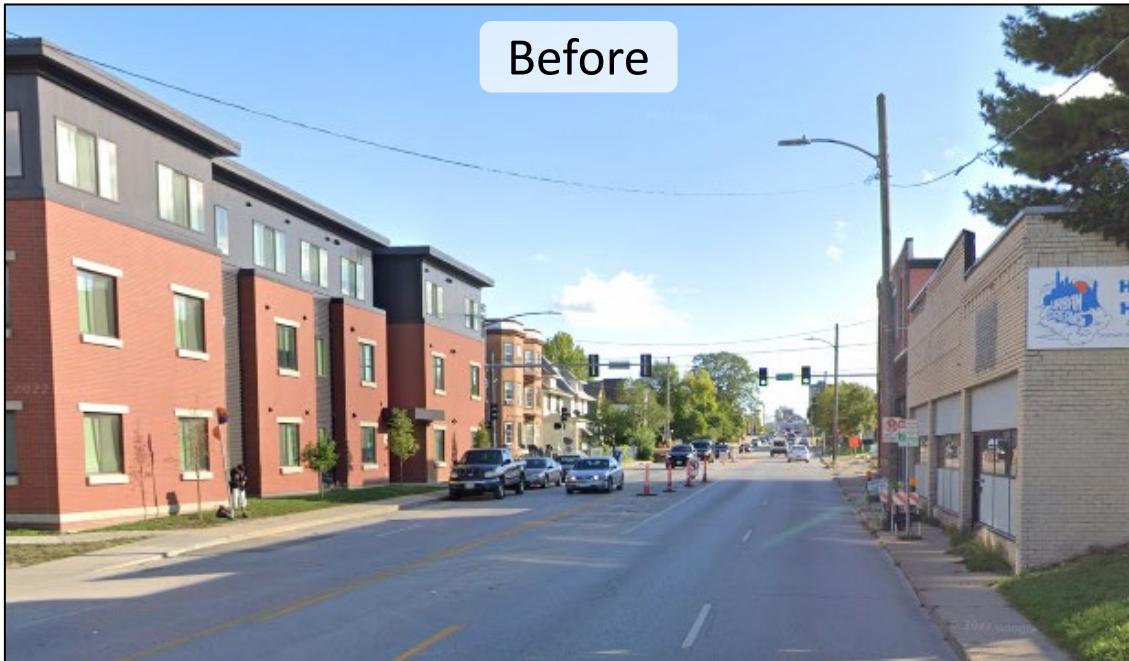
- Example of iterative application of Proven Safety Countermeasures
- No fatal or serious injuries on corridor since streetscape project
- 28th Street to 31st Street completed in 2024
- 31st Street to 35th Street planned for 2026

	Time Period	Crashes/Year	Average Vehicle Speed (mph)
Pre 4-3 Conversion	2006-2009 (3 years)	51	24
Pre Streetscape Project	2015 - 2019 (5 years)	15	23
After Streetscape Project	2024 (1 year)	12	20



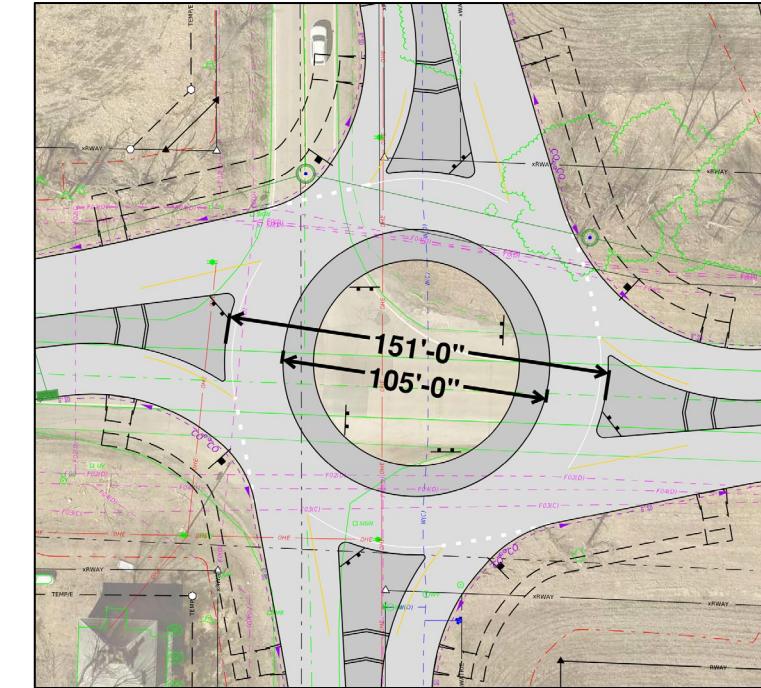
6th Avenue Streetscape University Avenue to Hickman Road

- Completed in 2022
- No fatalities before or after. Number of crashes were not reduced. Injuries (minor and serious) have been reduced.
- Speeds reduced by 5 mph
- University Avenue to School Street (S I-235) planned for 2026.
- Hickman Road to Euclid Avenue planned for 2028.



Roundabout Projects

- Polk County Broadway Avenue Project included 2 roundabouts
- Roundabout at SE 36th Street and E Army Post under construction in 2025
- SE 36th Street and Three Lakes Parkway planned in 2028
- Other locations planned pending funding



High Injury Network Projects Completed in 2024

E University Ave – E 14th St to Easton Blvd



Before



After

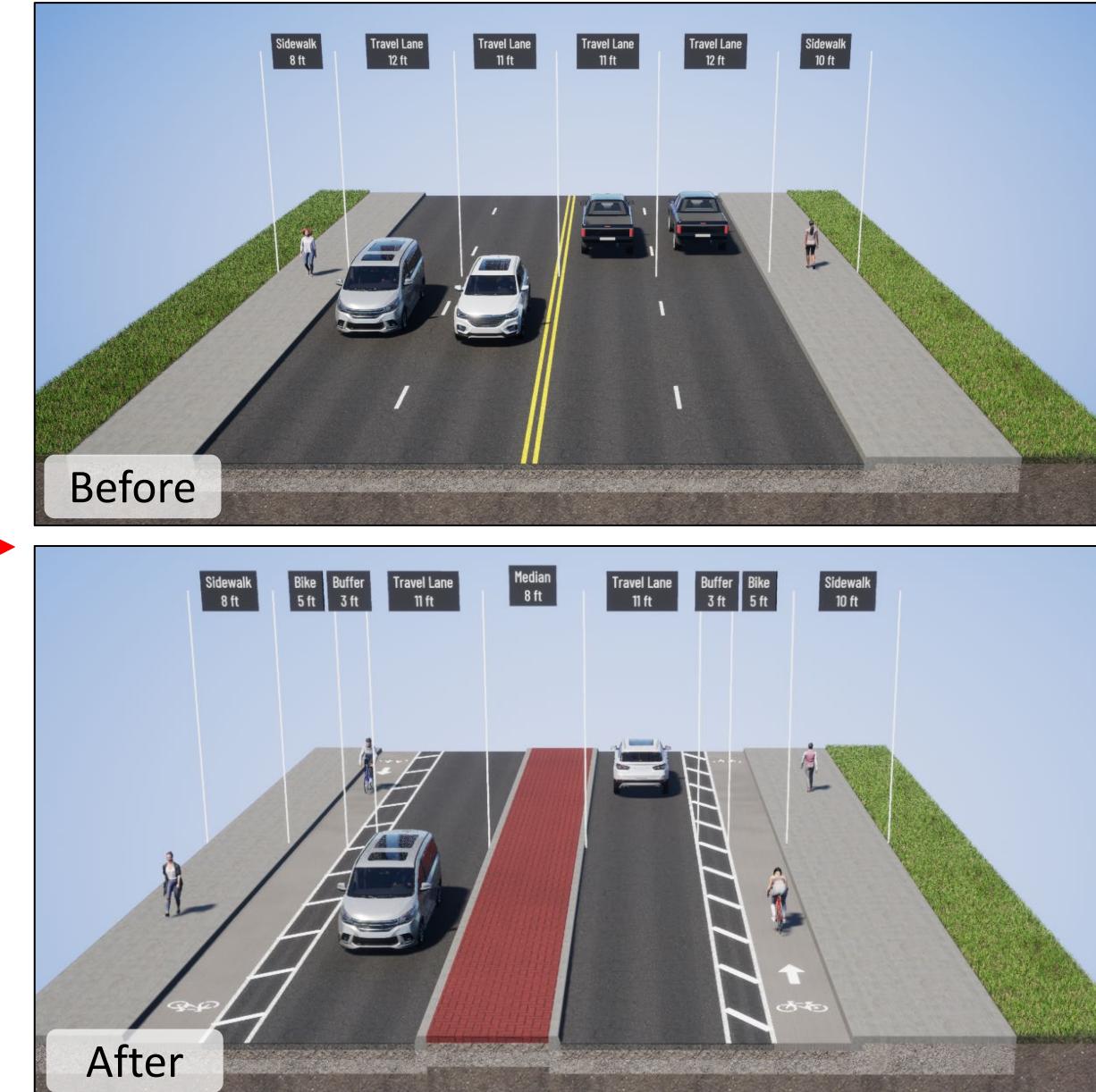
University Ave – 31st St to 48th St



Agreement with US DOT on funding

Implementation Schedule TBD (within 5 years)

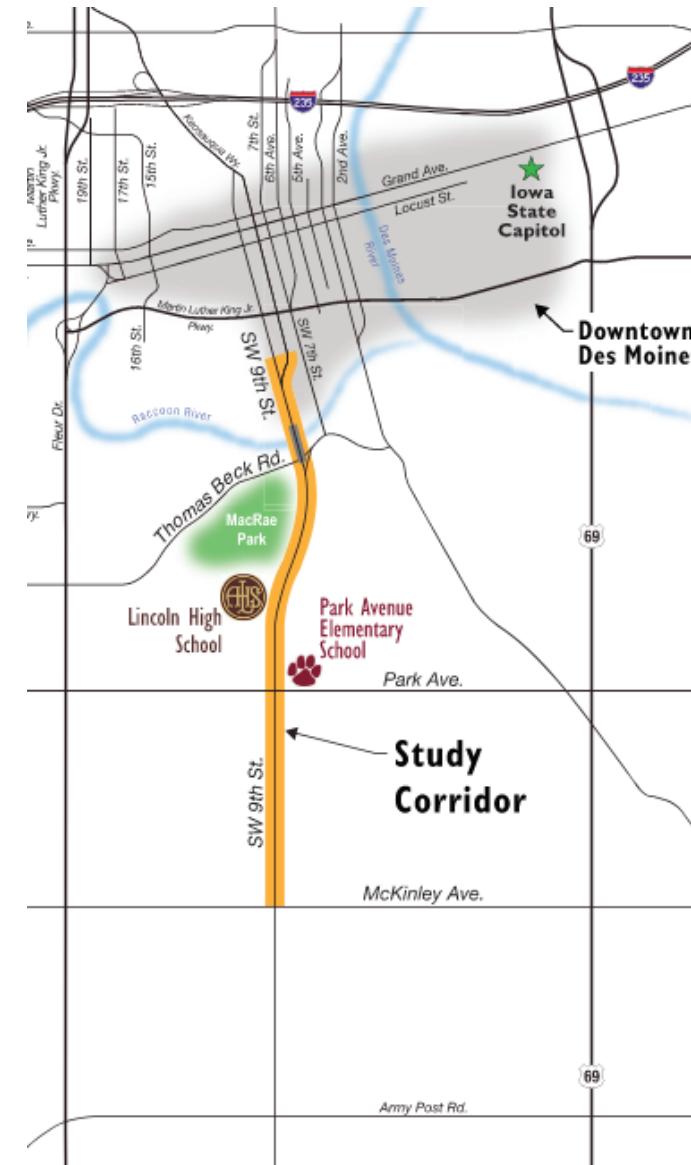
- Upgrading School-Zone Beacons with Speed-Feedback Signs
- Add Retroreflective Backplates to Traffic Signals at 232 intersections on HIN
- Forest Avenue Corridor: Beaver Avenue to 9th Street – road diet and multimodal improvements
- ML King Jr. Parkway & 19th Street Corridors – road diet and multimodal improvements
- SE 14th Street Corridor – intersection improvements and multimodal improvements



Looking Forward

- Develop process for Vision Zero Initiative Funding (\$500,000 FY2027)
 - Focus on HIN and high crash intersections
- Upcoming Studies:
 - SW 9th Street Corridor Study (Summer)
 - Speed Management Study (FY2026)
 - Keo Way Corridor Study (FY2027)
 - Vision Zero Plan Update (FY2028/29)

SW 9th Street Corridor Study



Questions?

Thank you!