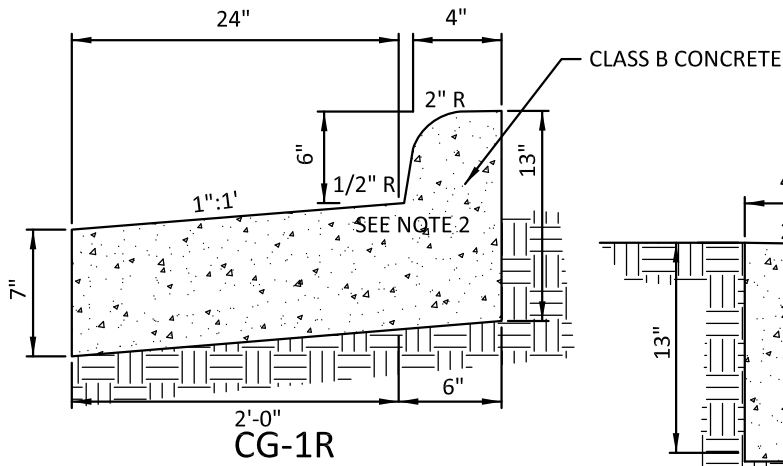
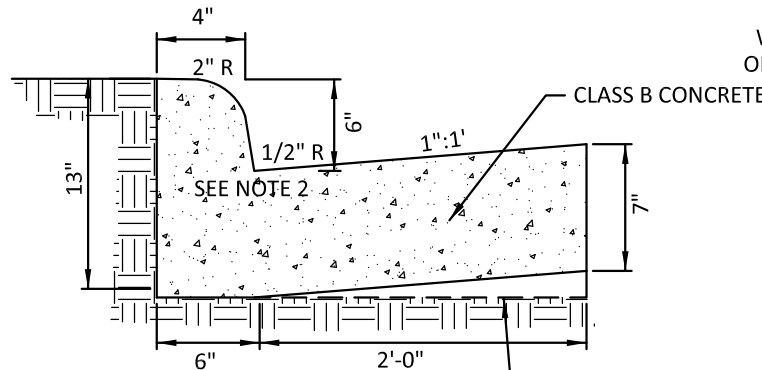


Appendix B

Standard Details

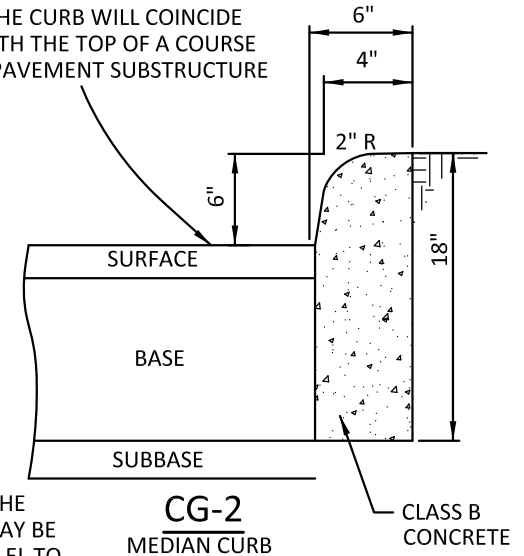


CG-1R
REVERSED CURB AND GUTTER
SECTION VIEW



CG-1
STANDARD CURB AND GUTTER

THE DEPTH OF CURB MAY BE REDUCED AS MUCH AS THREE INCHES OR INCREASED AS MUCH AS THREE INCHES IN ORDER THAT THE BOTTOM OF THE CURB WILL COINCIDE WITH THE TOP OF A COURSE OF PAVEMENT SUBSTRUCTURE



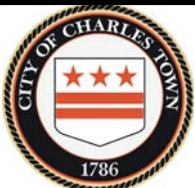
CG-2
MEDIAN CURB

NOTES:

1. THE USE OF CG-1R SHALL NOT BE UTILIZED WITHIN THE LIMITS OF PUBLIC RIGHT OF WAY.
2. A MAXIMUM OF A 2" RADIUS IS ACCEPTABLE WITH CURB AND GUTTER.
3. SUBGRADE FOR ALL CURB AND GUTTER SHALL BE COMPACTED TO 95 PERCENT DENSITY AT OPTIMUM MOISTURE TO THE FULL WIDTH OF RIGHT OF WAY IN ACCORDANCE WITH AASHTO, T99.

THE BOTTOM OF THE CURB AND GUTTER MAY BE CONSTRUCTED PARALLEL TO THE SLOPE OF THE SUBSURFACE COURSES PROVIDED A MINIMUM OF 7" IS MAINTAINED.

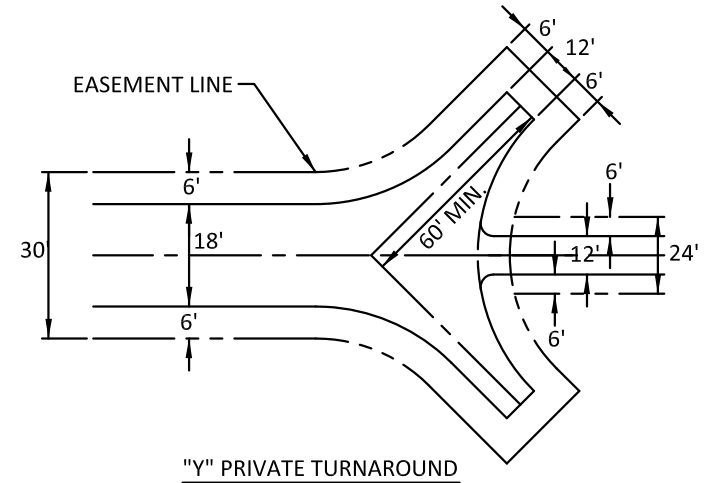
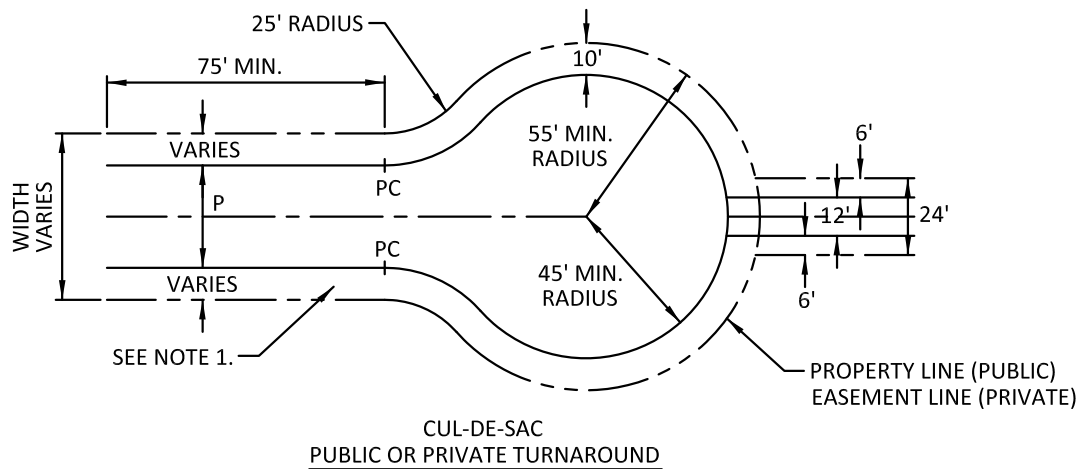
NOT TO SCALE



TYPICAL COMBINATION CURB AND GUTTER

City of Charles Town

CG-1
CG-1R
CG-2



NOTES:

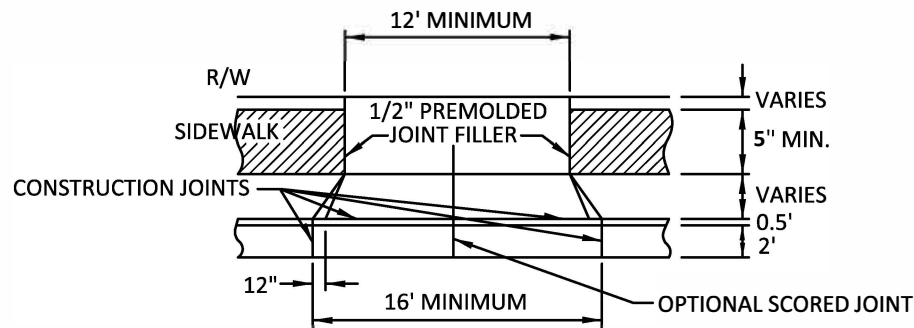
1. SIDEWALKS ON PUBLIC CUL-DE-SAC STREETS SHALL BE PROVIDED WITHIN THE RIGHT-OF-WAY.
2. A MAXIMUM GRADE OF 3% IN ALL DIRECTIONS SHALL BE MAINTAINED ACROSS THE CUL-DE-SAC.
3. THE 45' MINIMUM RADIUS MAY BE REDUCED TO AN ABSOLUTE MINIMUM RADUIS OF 30' ON SHORT LOW VOLUME CUL-DE-SAC'S, WHICH WILL REQUIRE THE APPROVAL OF THE PLANNING COMMISSION.

NOT TO SCALE

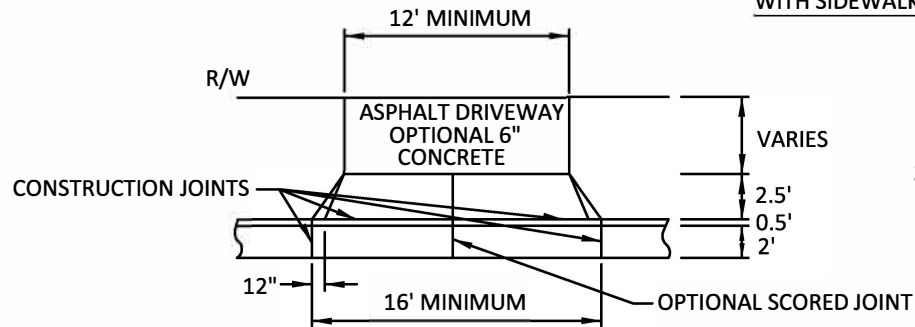


TYPICAL CUL-DE-SAC AND "Y" TURNAROUND

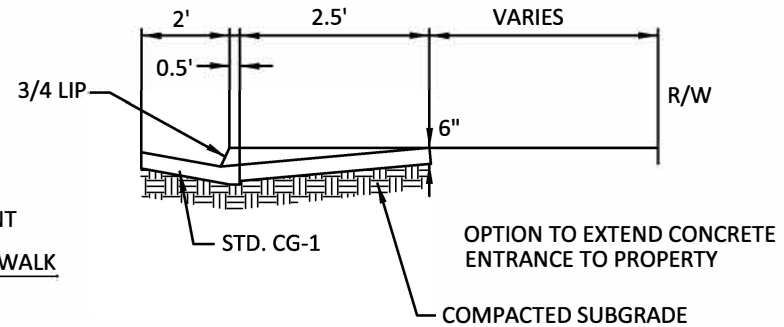
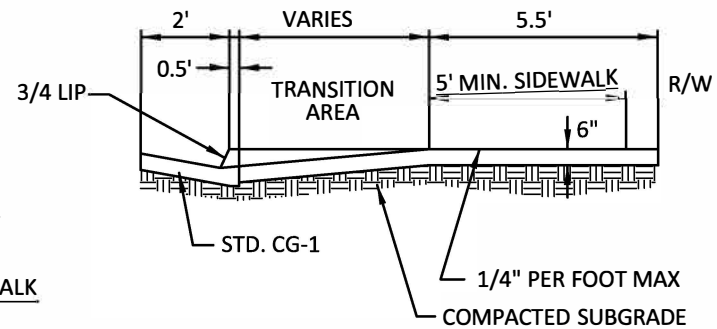
City of Charles Town



WITH SIDEWALK



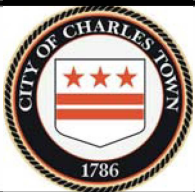
WITHOUT SIDEWALK



NOTES:

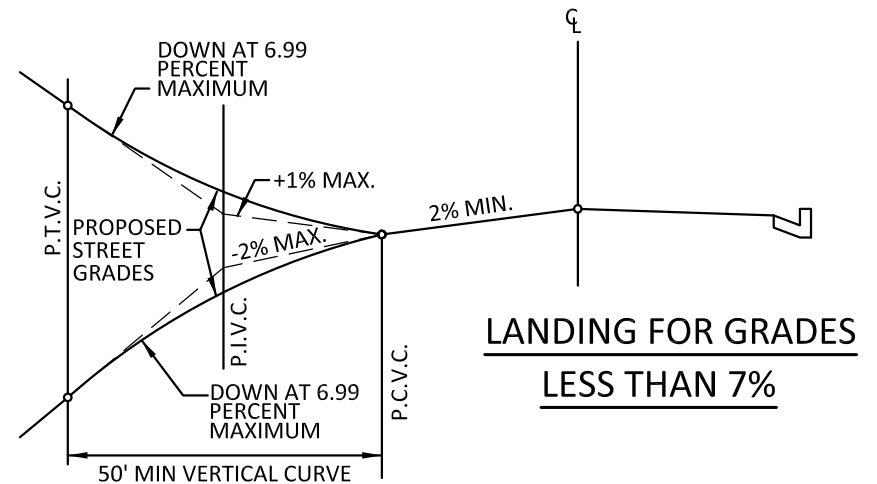
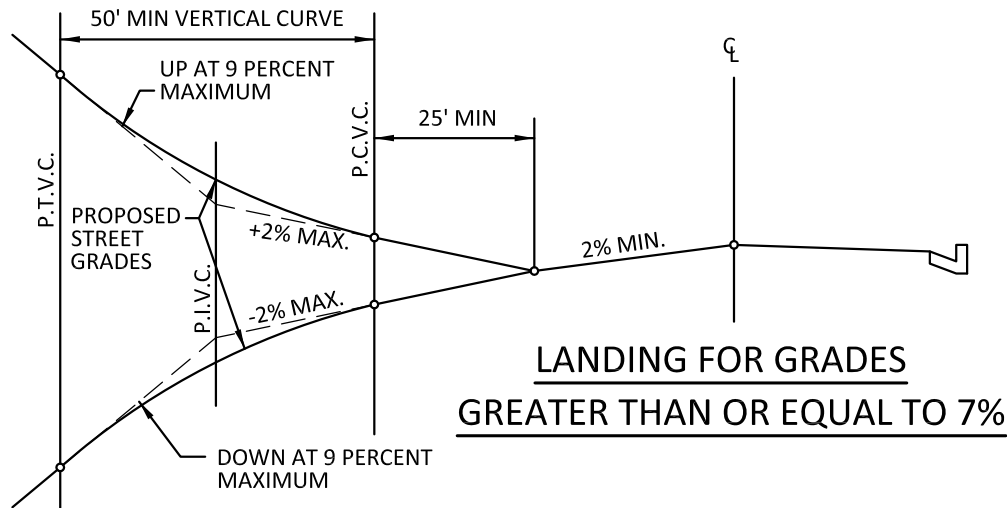
1. GRADING PLANS MUST PROVIDE FOR ADEQUATE VEHICULAR CLEARANCE FOR DRIVEWAY APPROACH, DEPARTURE, AND BREAKOVER TRANSITIONS.

NOT TO SCALE



DRIVEWAY ENTRANCE WITH CURB AND GUTTER City of Charles Town

DE-2



NOTES:

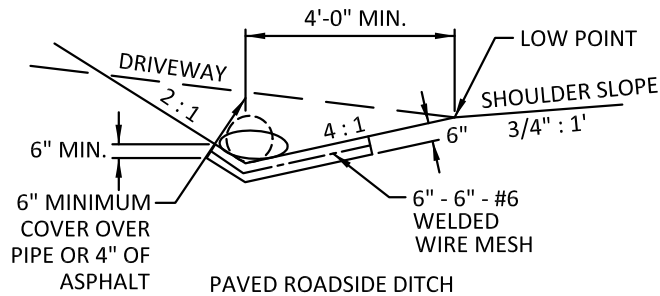
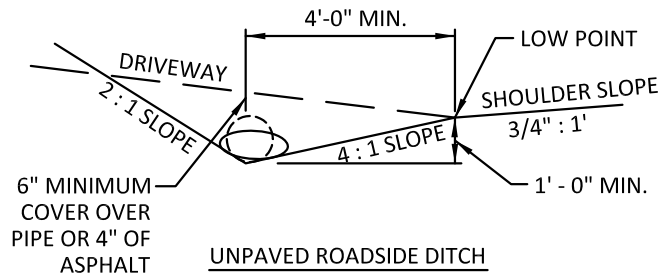
1. THE P.C.V.C. AS SHOWN ON THE DETAIL FOR LANDINGS WITH GRADES LESS THAN 7% SHALL BEGIN AT THE FACE OF CURB ON A CURB AND GUTTER SECTION OR AT THE EDGE OF PAVEMENT ON A SECTION WITHOUT CURB AND GUTTER.
2. THE 25' MINIMUM VERTICAL CURVE AS SHOWN ON THE DETAIL FOR LANDINGS WITH GRADES EQUAL TO OR GREATER THAN 7% SHALL BEGIN AT THE FACE OF CURB ON A CURB AND GUTTER SECTION OR AT THE EDGE OF PAVEMENT ON A SECTION WITHOUT CURB AND GUTTER.

NOT TO SCALE

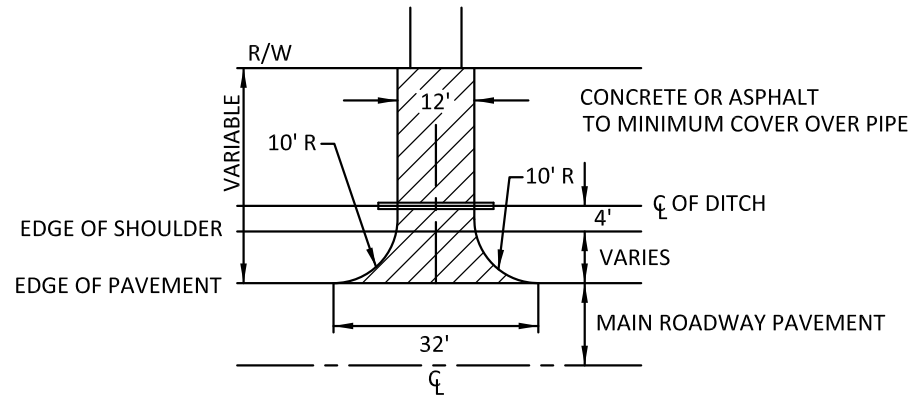


INTERSECTION LANDING REQUIREMENTS

City of Charles Town



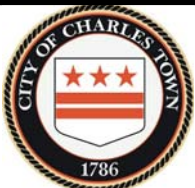
A PAVED DITCH IS REQUIRED WHERE SOIL CONDITIONS AND RUNOFF VELOCITIES WILL CAUSE EROSION.



NOTES:

1. ALL CULVERTS SHALL BE SIZED IN ACCORDANCE WITH THE WVDOH DRAINAGE MANUAL - CURRENT EDITION.
2. DRIVEWAYS SHALL BE SURFACED FROM THE EDGE OF PAVEMENT TO THE PROPERTY LINE WITH THE SAME TYPE OF SURFACING USED ON THE STREET.
3. ALL DRIVEWAY GRADES SHALL START BACK OF THE SHOULDER LINE.
4. IN CUT SECTIONS, SIDES OF THE DRIVEWAY SHALL BE GRADED TO A MAXIMUM 3:1 SLOPE.
5. THE DITCH LINE MAY BE MOVED BACK TO PROVIDE REQUIRED COVER. THE TRANSITION OF THE DITCH LINE SHALL BE SMOOTH WITH A MINIMUM TRANSITION LENGTH OF 10 FEET.

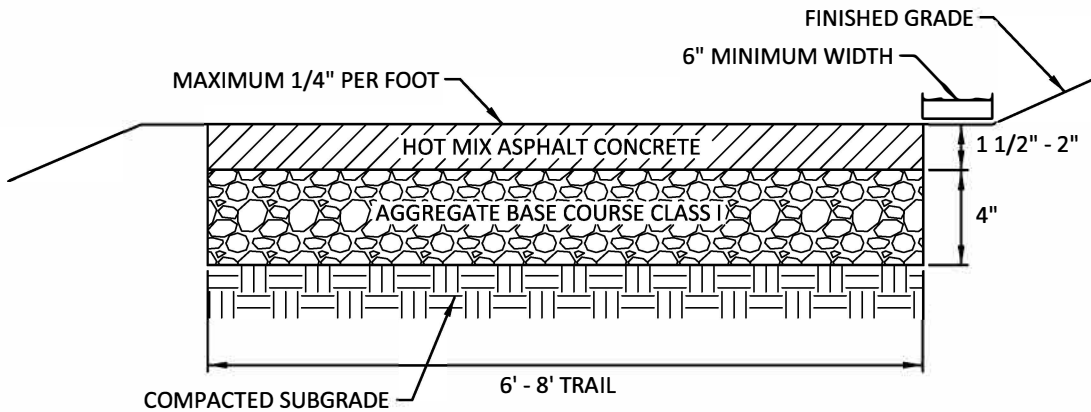
NOT TO SCALE



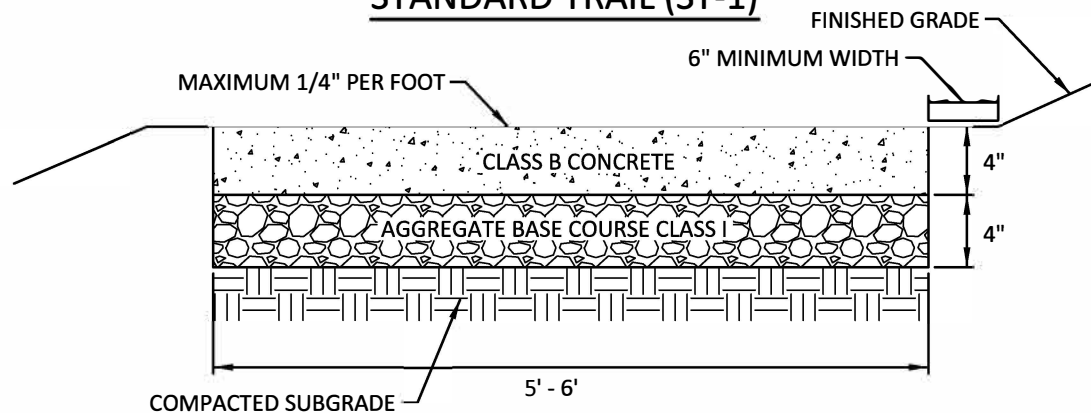
DRIVEWAY ENTRANCE WITH NO CURB AND GUTTER

City of Charles Town

DE-1



STANDARD TRAIL (ST-1)

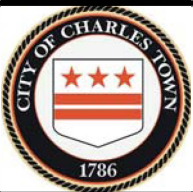


STANDARD SIDEWALK (SW-1)

NOTES:

1. ALL PEDESTRIAN AND BIKE TRAILS LOCATED OUTSIDE OF CITY OF CHARLESTOWN RIGHT OF WAY SHALL BE A MINIMUM OF 5' IN WIDTH FOR SIDEWALKS, 6' FOR ALL PEDESTRIAN TRAILS, AND 8' FOR ALL BIKE TRAILS.
2. A PRIVATE ACCESS EASEMENT SHALL BE PROVIDED FOR ALL SIDEWALKS AND TRAILS OUTSIDE OF CITY OF CHARLES TOWN RIGHT OF WAY AND SHALL INCLUDE A MINIMUM OF 3' OF WIDTH ON BOTH SIDES OF THE TRAIL OR SIDEWALK.
3. THE MAXIMUM GRADE FOR ALL PEDESTRIAN TRAILS SHALL BE 10%.
4. THE MAXIMUM GRADE FOR ALL BIKE TRAILS SHALL BE 8%, EXCEPT UP TO 15% IS ALLOWED FOR DISTANCES OF LESS THAN 150 FEET.
5. APPROPRIATE LIGHTING AND SIGNAGE, LANDSCAPING, BUFFERING AND DRAINAGE MEASURES, ALONG WITH CONSIDERATIONS FOR SECURITY SHALL BE INCORPORATED IN THE DESIGN OF THESE FACILITIES.

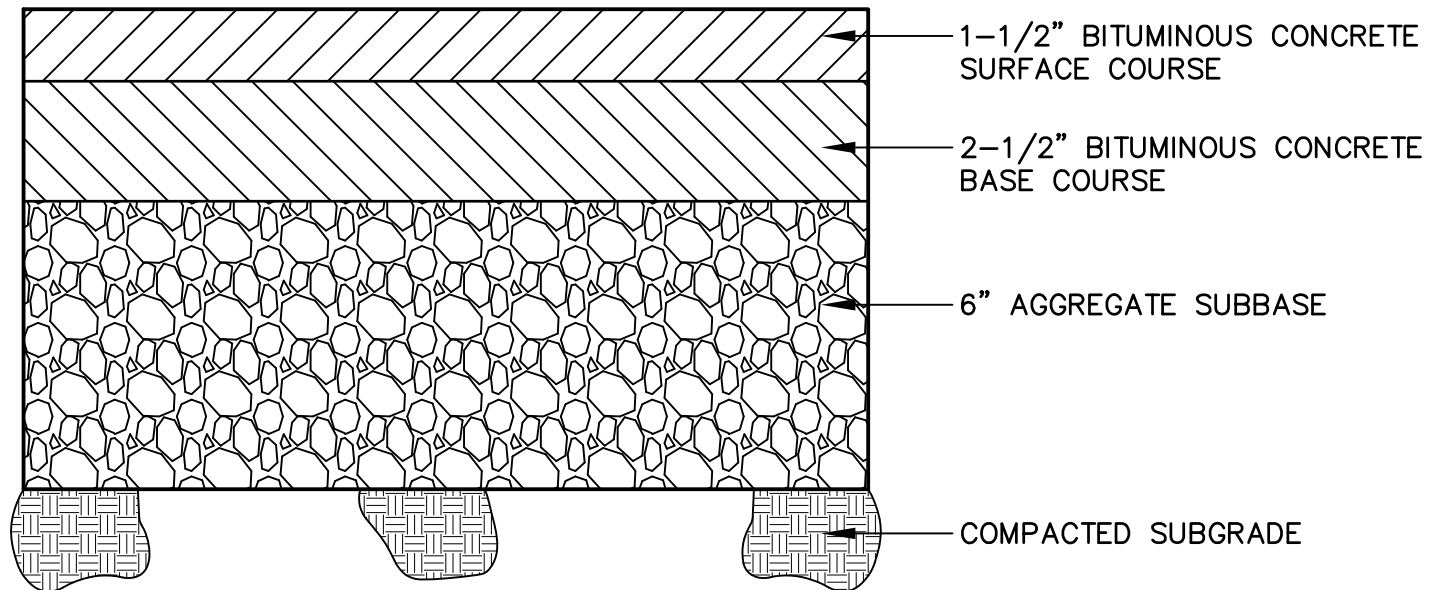
NOT TO SCALE



TRAIL AND TYPICAL SIDEWALK

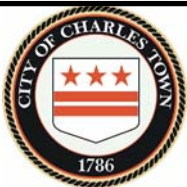
City of Charles Town

ST-1
SW-1



NOTES:

1. ALL BASE AND SUBGRADE MATERIAL SHALL BE COMPACTED TO 95% THEORETICAL MAXIMUM DENSITY AS DETERMINED BY AASHTO T-99 METHOD A, WITHIN A $\pm 2\%$ OF OPTIMUM MOISTURE.



STANDARD DETAIL

TYPICAL DRIVEWAY/PARKING
PAVING SECTION

STANDARD
NUMBER

K-1