



Invitation to Bid

Bartow County Parks and Recreation Wilderness Camp Mountain Bike Trail Phase 2

Issue Date: November 19, 2024

Pre-bid Site Visit: December 3, 2024 at 10:00 A.M.

Bids Due: Tuesday, January 7, 2025 BY 2:00 P.M.

Submit bids to:

MAIL	COURIER/BY HAND
Attn: Kathy Gill Certified County Clerk RE: Wilderness Camp Phase 2 Bartow County, Georgia 135 W. Cherokee Avenue, Suite 251 Cartersville, GA 30120	Attn: Kathy Gill Certified County Clerk RE: Wilderness Camp Phase 2 Bartow County, Georgia 135 W. Cherokee Avenue, Suite 251 Cartersville, GA 30120

Due date and time:

Response must be received by or before 2:00 P.M. Eastern Time on
Tuesday, January 7, 2025.

Contact person:

Claire Mulkey

Phone: 770-607-1102

mulkeyc@bartowcountyga.gov

INTRODUCTION

Bartow County Parks and Recreation has been awarded a Recreational Trails Program (RTP) grant to design and construct the second phase of a new multi-use trail system, located on County owned greenspace at 320 Wilderness Camp Rd SE White, GA 30184. Phase 2 will consist of approximately 4 miles of intermediate-level mountain bike optimized trails and a bridge over McKaskey Creek to connect the Phase 2 trails to future phased trails. Bartow County will accept competitive sealed bids for a firm-fixed price contract for the construction of the Phase 2 portion of the trail system.

MTB Atlanta, the Metro Atlanta Chapter of SORBA (Southern Off-Road Bicycle Association), was hired to complete the field design and flagging for all Phase 2 trails. Trail alignments and flagging details are included as an attachment to this solicitation. See directions for accessing trail alignment spatial data files below.

PROJECT LOCATION

Wilderness Camp Trail System (Trailhead)
320 Wilderness Camp Road SE White, GA 30184

PROJECT DESCRIPTION

Bartow County proposes to construct approximately four miles of multi-use, mountain bike optimized trails along with a bridge at the Wilderness Camp Trail System. Existing and proposed site plans are attached to this solicitation. See below for instructions for accessing trail alignment spatial data files below. Davis Bacon Wage rates are present in the project and conformance to such is imperative. The applicable Wage Rate Determination is available as part of the solicitation package.

PROJECT OVERSIGHT:

The bid process will be administered by the Bartow County Grant Writing Department in collaboration with the Bartow County Parks and Recreation Department. Once selected, the chosen contractor will begin working directly with Bartow County staff to complete the scope of work. Grant writing staff will ensure that project activities satisfy all grant award requirements and Parks and Recreation staff will be available as needed to answer on-site questions. The Contractor will establish communication lines with both County departments and provide regular progress reports as well as communicate any challenges as soon as they arise.

SCOPE OF WORK

Bartow County is requesting bids from qualified individuals or firms to provide trail construction services for Phase 2 of the Wilderness Camp Trail System. Phase 2 includes approximately 4 miles of intermediate-level bike trails and a bridge over McKaskey Creek to connect Phase 2 trails to the other side of the property to be developed in future phases.

Trail Construction

- Phase 2 design offers two loops, each one to be built with a distinct character that will provide a more “backcountry” feel and will aid in rider progression. Phase 2 also includes a short connector trail with a bridge crossing that will help connect the Phase 1 and 2 trails to future phased trails.
- Trail segment 301: Trail segment 301 is flagged in orange and at an estimated 2.31 miles or 12,197 feet and designed to be an intermediate directional shared-use loop trail built to Forest Service Class 3 Singletrack specifications with a tread on average of 24-36 inches wide with dirt roller grade reversals added. Trail segment 301 is designed to maximize descents and climbs to highlight the steeper side slopes in this zone and to be directional by day. Natural rock features should be utilized as either alternates or incorporated in the tread with sustainable armoring techniques.
- Trail segment 302: Trail segment 302 is flagged in blue and orange and at an estimated 0.42 miles or 2,264 feet and designed to be an intermediate bi-directional shared-use singletrack trail with tread on average of 36-48 inches wide with dirt roller grade reversals added. Trail segment 302 is designed to accommodate two-way traffic and will require a 15-20 foot bridge crossing (see additional bridge details below).
- Trail segment Wagon Wheel Extension: Wagon Wheel Extension is flagged in pink and at an estimated 1 mile or 5,280 feet and designed to be an intermediate directional shared use loop trail built to Forest Service Class 3 Singletrack specifications with a tread on average of 24-36 inches wide with dirt roller grade reversals added. Natural rock features should be utilized as either alternates or incorporated in the tread with sustainable armoring techniques.

Bridge Crossing

- Trail segment 302 requires a 15-20 foot bridge crossing over McKaskey Creek. The bridge should be built with a deck 8-feet wide with rails appropriate for bikes to accommodate two-way traffic.
- The bridge crossing should be constructed of pressure treated wood or fiberglass. Sills should be set on firm constructed footings. Sills should be rot resistant and rated for ground contact. Sills should be dimensional lumber or fiberglass. The final deck width should be 8-feet in width. Decking should be rough cut timber, to ensure adequate traction during wet weather. All bridge, boardwalk, and wooden feature construction should meet local deck or similar building code.
- No stamped plans for the bridge currently exist. See example bridge design plans attached for suggested specifications

Signage

- Updated trail kiosk map (1) – 36”x 48” panel (posts and frame can remain as is, front panel will just be swapped out) and Hub Map signs (2) - 44” x 60” panels (posts and braces can remain as is, panels will just be swapped out)

- New trail hub signs (2) 24”x 36” panel with posts and frame
- Directional trail signage (5) – 12”x 9”
- Assorted trail signage (11) – 12”x12”
- Cedar recycled plastic posts (16) – 4x4x96”
- Trail blades – brown, 48” out of the ground, will be placed approximately every 0.10 of a mile so ~40 should be adequate for wayfinding signage
- Trail blade decals – 3”x 3” full color symbols. ~4 per side of each blade (~360 in total)

Access/Staging

- There is a paved parking lot at the trailhead with access to a restroom and drinking water. Selected contractor will coordinate with County staff to determine staging area, either on the property or on neighboring County-owned property.
- The undeveloped area where the Phase 2 trails will be constructed is forested and not maintained for passenger vehicle traffic. It is up to the contractor to make a transportation plan based on the conditions they observe on site and the capabilities of their vehicle(s) and equipment.

PROJECT TIMELINE:

Critical Dates for this Bid	
ITB Issue Date	November 19, 2024
Pre-bid Site Visit (mandatory)	December 3, 2024 at 10 A.M.
Deadline for Request for Clarification/Questions	December 30, 2024 by 4:00 P.M.
Final Response to Request for Clarification/Questions Published	December 31, 2024
Bids Due, Bid Opening to immediately follow	Tuesday, January 7, 2025, by 2:00 P.M.
Anticipated Award Announcement	January 2025*
Trail Construction Completed	April 2025*

*dates are subject to change

Bids should be submitted to the Bartow County Commissioner’s Office, located in the Frank Moore Administration Building, 135 W. Cherokee Avenue, Suite 251, Cartersville, Georgia 30120. Bids are due to this location no later than 2:00 p.m. on January 7, 2025. Bidders are invited to attend the Public Bid Opening which will be held at 2:00 P.M. on January 7, 2025 in the Bartow County Commissioner’s Office conference room at the above mentioned address. Bids will be publicly opened and read aloud. All bids will be evaluated based on price and bid specifications provided. The contract will be awarded principally based on price to the lowest responsible and responsive bidder whose bid meets the requirements and criteria set forth in the invitation for bids.

PRE-BID SITE VISIT:

A Pre-bid site visit and walk of the proposed flag line will be mandatory for all contractors who submit a proposal. The terrain is rocky and steep and will require advanced logistics. The Pre-bid site visit will be held on December 3, 2024 at 10 A.M. All site visit participants should meet at the trailhead parking lot at 320 Wilderness Camp Rd SE White, GA 30184. Please wear clothing and footwear appropriate to the terrain.

BID SUBMISSION:

Bidders shall follow all submission criteria as outlined in the Invitation to Bid. Bid packages that do not contain signed copies of required grant documentation will be considered incomplete / unresponsive and will be rejected. Grant documentation includes E-Verify documentation and Bonding Requirements. Addendum may be issued during the response period. Contractors are responsible for addenda. Bids submitted without acknowledgement of all addenda will be rejected.

Required Documents for this Bid
1. Bid Form including Firm-Fixed Price for the complete scope of work, requirements and criteria as outlined in the invitation to bid as well as the acknowledgement of addenda
2. A schedule identifying when the contractor is available to begin and complete the work
3. Bid Bond
4. Signed E-Verify Documentation

Bids may not be withdrawn for a period of 60 days after time has been called on the date of bid opening. Bartow County reserves the right to reject any and all bids, to negotiate changes in the scope of work or services to be provided, to negotiate with any firm and to waive any technicalities, informalities or irregularities and to award the bid based on the highest and best interests of the County. Bartow County and its associated municipalities are equal opportunity employers and do not discriminate against any party on the basis of race, creed, color, national origin, gender or handicapping condition. This project may exceed \$100,000 and therefore this project may be subject to the Public Works Construction Law, OCGA Sec. 36-91-1 et seq.

ELIGIBILITY CRITERIA:

Contractors should be aware that the project is funded, in part, by a Recreational Trails Program Grant. All contractors shall be in good standing with Bartow County and shall abide by all Federal Requirements set forth for the project (i.e. Federal Labor Standards, Equal Opportunity, Accessibility, and Build America Buy America Requirements). A brief summary of these requirements is included below.

Individuals or firms responding to this ITB:

- May not appear on any list of service providers who are debarred from participating in federally funded contracts
- Must be a licensed professional in good standing with the State of Georgia

- Must have Errors and Omissions Insurance
- Must have knowledge of state licensure requirements and any other regulatory criteria which apply to the project as is described within this ITB, including the Build America, Buy America Act

RECOMMENDED QUALIFICATIONS:

While this bid will be awarded principally based on price, it is considered to be in the best interest of Bartow County, Federal Highway Administration, Georgia Department of Natural Resources and all future trail users to ensure that the bid is awarded to the lowest, most qualified bidder. Bidders are encouraged to include documentation to demonstrate evidence of the recommended qualifications below:

- Prior shared-use trail construction experience
- Prior experience working on federally funded contracts
- References for similar completed projects
- Professional TrailBuilders Association (PTBA) membership

INSURANCE AND MANDATORY BONDING REQUIREMENTS

All proposals must be accompanied by a Bid Bond or Certified Funds in the amount of 5% of the Bid Amount. Both a Performance and a Payment Bond will be required in an amount equal to 100% of the Contract Price. Proof of General Liability Insurance and Workman's Compensation Insurance valued at \$1 million will be required with Bartow County listed as an additional insured. The primary contractor is responsible for providing bonds, insurance and E-Verify documentation on all sub-contractors.

Contractors must comply with the provisions of O.C.G.A. Sec. 13-10-91, and must register and participate in the federal work authorization program (also known as E-Verify (<http://www.uscis.gov/everify>)). Contractors must submit the required affidavit BEFORE ANY PROPOSAL IS CONSIDERED. A form affidavit shall be provided with the solicitation package.

APPLICABLE REGULATIONS

Bartow County, in accordance with Title VI of the Civil Rights Act of 1964 and related statutes and regulations, hereby notifies all contractors that it will affirmatively ensure that in regards to any contract entered into pursuant to this advertisement, minority business enterprises will be afforded full opportunity to submit proposals in response to this invitation and will not be discriminated against on the grounds of race, color, sex, or national origin in consideration for an award. The General Contractor and all Sub-contractors shall insure that employees and employment applicants are not discriminated against because of race, color, creed, sex, or national origin.

The successful contractor must comply with all applicable local, state and federal regulations, including:

- U.S. Department of Transportation (DOT) Disadvantaged Business Enterprise (DBE) policy
 - DOT's DBE program is designed to remedy ongoing discrimination and the continuing efforts of past discrimination in federally assisted highway, transit, airport, and highway way safety project contracts.
 - The primary remedial goal and objective of the DBE program is to level the playing field by providing small businesses owned and controlled by socially and economically disadvantaged individuals a fair opportunity to compete for federally funded transportation contracts
 - Learn more here: <https://www.transportation.gov/civil-rights/disadvantaged-business-enterprise/dbe-guidance>
- 23 CFR 635.410 Buy America
 - Buy America requirements apply to steel and iron permanently incorporated in a project funded under title 23 (and associated eligible contracts).
 - The Buy America provision in 23 CFR 635.410 (b)(1)(ii) requires that steel/iron materials (including components and subcomponents) be melted and manufactured domestically.
 - In addition, all manufactured products used in the project must be produced in the United States. This means the manufactured product was manufactured in the United States, and the cost of the components of the manufactured product that are mined, produced, or manufactured in the United States is greater than 55 percent of the total cost of all components of the manufactured product.
 - If documentation of domestic steel/iron manufacturing is not available, then a waiver request is necessary.
 - Learn more here: <https://www.fhwa.dot.gov/construction/cqit/buyam.cfm>
- 49 CFR 29, Suspension and Debarment
 - By signing and submitting a bid, the prospective bidder is certifying that they are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded by any Federal department or agency, among other responsibility matters
 - Learn more here: <https://www.fhwa.dot.gov/construction/cqit/suspensi.cfm>
- 29 CFR 3, Anti-Kickback (Copeland Act)
 - Prohibits contractors and subcontractors on federally funded construction projects from forcing workers to give up any part of their wages, essentially preventing "kickbacks" from employees to employers
 - Learn more here: <https://www.dol.gov/agencies/whd/government-contracts/copeland-anti-kickback>
- 23 U.S.C. 113, Prevailing Wage Rate (Davis Bacon Act)
 - Requires the payment of prevailing wage rates to all laborers and mechanics on Federal or Federally assisted construction contracts.

- Contractors are required to pay all subcontractors and employees at a rate that is equivalent to or greater than the applicable job classification rates listed in the attached wage determination. Contractors must also submit weekly certified payroll reports demonstrating wage rate compliance. A representative number of employees per trade will be interviewed on the job site and the information supplied in the interviews will be compared to the information reported in the certified payroll reports

Regulations and requirements related to the grant funding associated with this project include:

- Migratory Bird Treaty Act (16 U.S.C. sections 703-711)
 - Actions must be taken to avoid or minimize impacts to migratory bird resources, and to prevent or abate the detrimental alteration of the environment for the benefit of migratory birds, as practicable.
- The agreement that no existing bridge or culvert on the project site will be reconstructed or removed as part of this project unless DNR has been notified and provides approval.
- Required provision of stream buffer variance(s) for any stream crossings.
- The requirement that all construction work be halted immediately in the event that previously unidentified historic properties are discovered within the Area of Potential Effect (APE) during project construction.

QUESTIONS:

All inquiries relating to this project should be submitted to Ms. Claire Mulkey, Grant Writing Specialist, by calling (770) 607-1102 or by email at mulkeyc@bartowcountyga.gov. The deadline for all inquiries is Monday, December 30, 2024 by 4:00 P.M. All updates including addenda will be available on the Bartow County website.

ATTACHMENTS:

Bid Form
Design Brief
E-Verify affidavit form
DBE Participation Certification
Davis Bacon Wage Determination

Link to spatial data (available by request, please email mulkeyc@bartowcountyga.gov for access)

BID FORM

**BARTOW COUNTY GEORGIA
WILDERNESS CAMP MOUNTAIN BIKE TRAIL PHASE 2**

Project Location: Bartow County, GA

The undersigned CONTRACTOR, having examined these documents, and having full knowledge of the condition under which the work described herein must be performed, hereby proposes that she/he will fulfill the obligations contained herein in accordance with all instructions, terms, conditions, and specifications set forth; and that she/he will furnish all required products/services and pay all incidental costs in strict conformity with these documents for the stated prices as payment in full.

Bartow County Wilderness Camp Mountain Bike Trail Phase 2 – Construction Bid Worksheet				
Item	Unit Measure	Estimated Quantity	Price	Total
Mobilization	Lump sum	1		
Trail Construction	Linear foot	19,747.20		
Bridge	Lump sum	1		
Signage	Lump sum	1		
Other (please describe)				
			Total	

Included and attached is a Bid Bond in the amount of not less than 5% of the base bid. Also included in the amount above is the sum of \$_____ for providing a Performance and Payment Bond for 100% of the total base bid. Same shall be provided by a surety corporation licensed in the State of Georgia and a certified Power of attorney shall be attached.

Submitting Firm: _____

Address: _____

Name and Title of Authorized Representative (print/type): _____

Authorized Signature: _____

(Signature-When signed, this bid is legal and binding to Bartow County and acknowledges that ALL Specifications, Terms and Conditions and/or instructions to Bidders have been read and understood).

Date: _____

Email: _____

FIRM PRICING – Prices submitted on this bid form are firm through (minimum 60 days).

Initial below for Acknowledgement of Addenda (if any)

_____ Addendum #1 _____ Addendum #2 _____ Addendum #3 _____ Addendum #4

_____ Addendum #5 _____ Addendum #6 _____ Addendum #7 _____ Addendum #8

Wilderness Camp Trail System

Phase 2 Design Plan



MTB ATLANTA

Building Trails - Connecting Community

Prepared by MTB Atlanta Pro Trails

Project Goals:

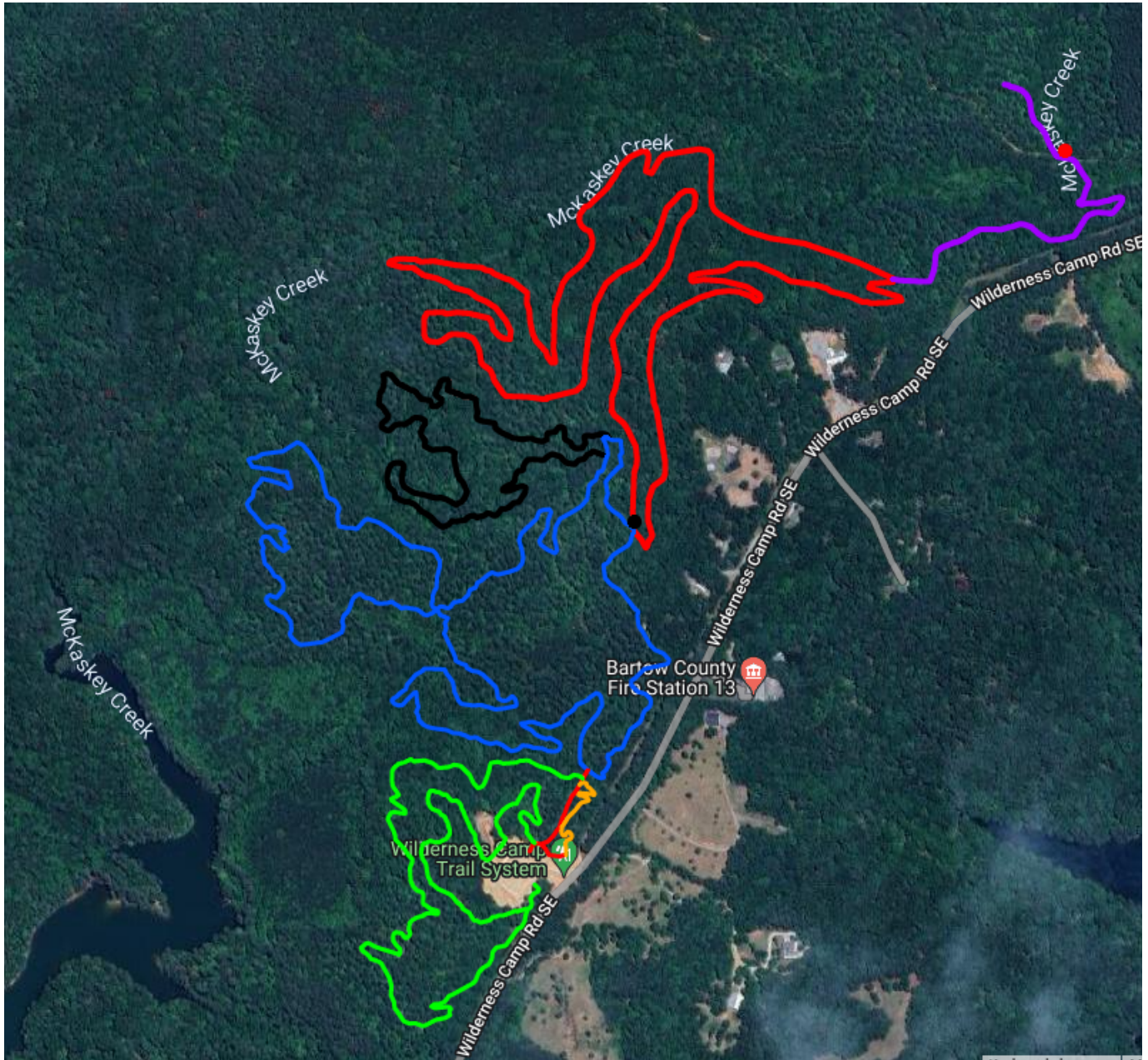
1. The proximity of Wilderness Camp Greenspace (WCG) to Atlanta and the surrounding cities, its proximity to I-75, combined with its semi-remote setting, make it an ideal location for destination-quality trails. WCG has the acreage to offer a half-day of recreation for mountain bikers, which equates to longer experiences for hikers and runners.

Combining traditional and bike-optimized singletrack with pockets of progressive gravity or bike-only alternates will help the site stand out as a contemporary, diverse trail system. The abundance and sheer size of the property's rocks and boulders create many unique opportunities. The soil is sandier than many Metro Atlanta riding venues, meaning WCG could provide more wet weather options for the area. Utilizing large rocks in bike specific trails will offer a rare type of technical riding for the region.

The County constructed a unique and welcoming trailhead, complete with a variety of amenities. These additional options will help cater to families and new riders, while also deepening the impact of WCG and guiding it to providing regionally significant recreation.

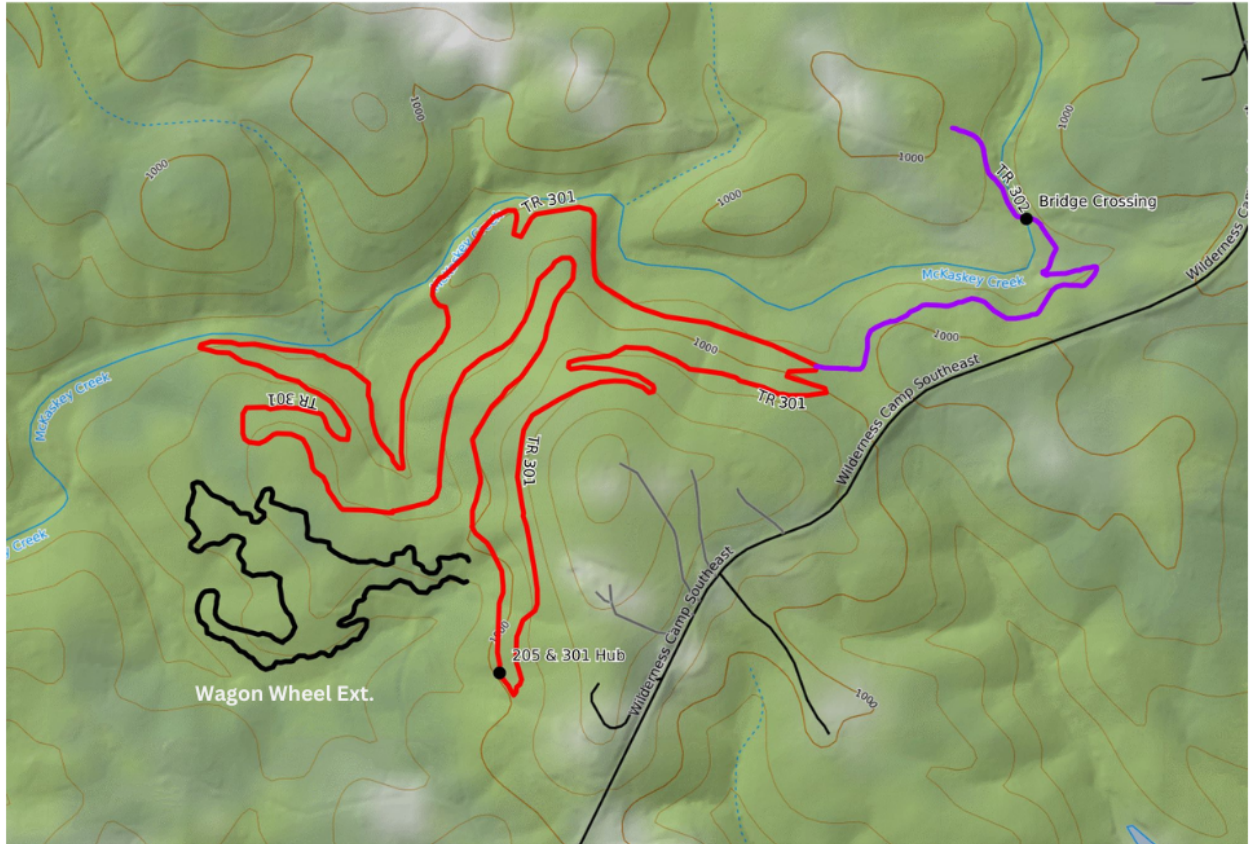
2. Offer diverse trail types to attract regional visitors that meets the needs of riders of all ability levels and ranging styles of riding. Placing mountain bike-specific features and amenities at trailheads provides new and unique trail types for residents and visitors. Phase 2 Design plan offers two loops and one connector trail each to be built with a distinct character that will provide a more "backcountry" feel and will aid in rider progression. The steep and rocky terrain and the drainage of McKaskey Creek in the Phase 2 zone will provide an incredible user experience from the context of natural beauty, skill building with natural features, distance, and the distinct character of the two loops and connector trail that will net approximately four miles of new trail at WCG.

Wilderness Camp Trail System Site Plan

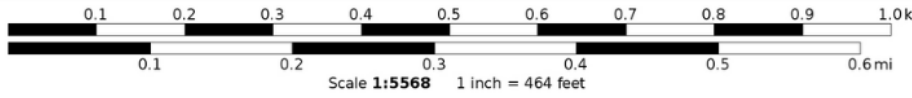


Green, shorter red, orange, and blue trails are existing trails constructed during Phase 1. Black, longer red and purple trails are to be constructed in Phase 2.

Phase 2 Site Plan



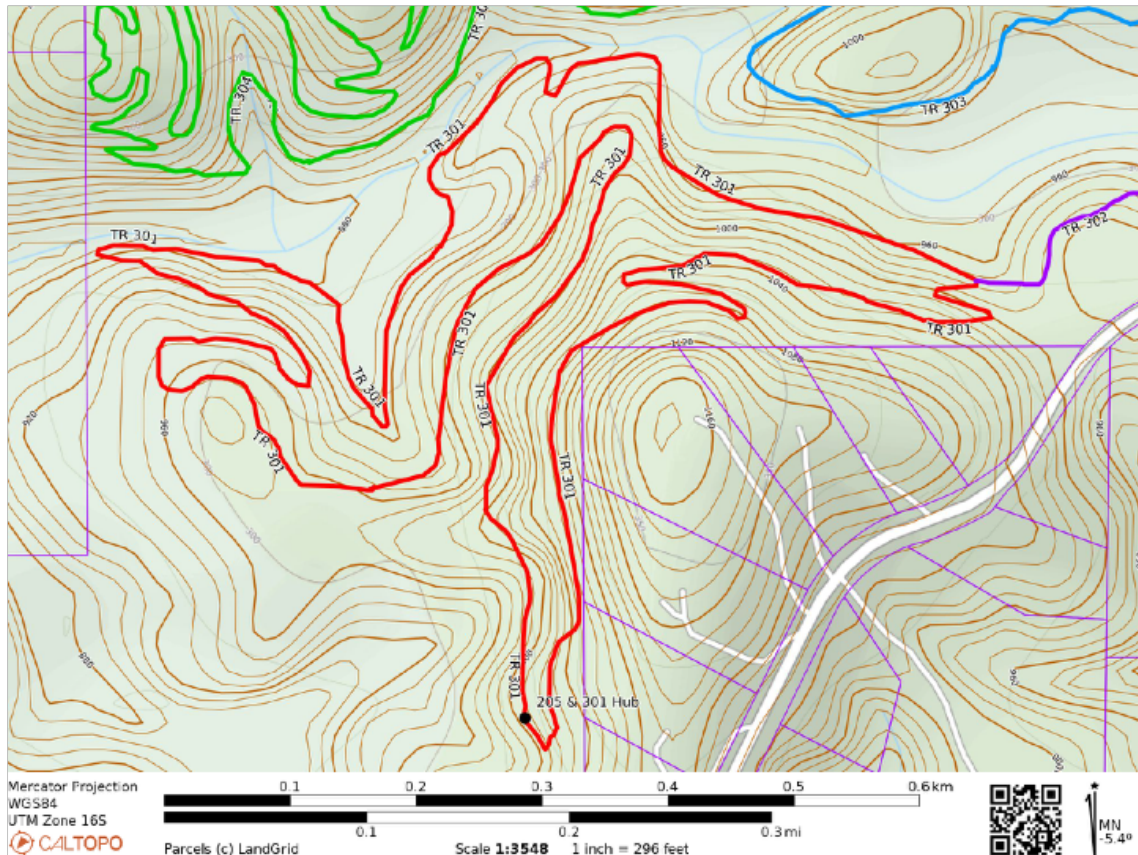
Mercator Projection
NGS84
JTM Zone 16S
CALTOPO



Segment Number/Name	User	Difficulty	Direction	Trail Width	Style	Linear Feet	Mileage	Flag Color
301	Shared Use	Blue	Directional by day (alternating)	24-36"	Traditional Bike Optimized	12,197	2.31	Orange
302	Shared Use	Blue	Bi-directional	36-48"	Traditional Bike Optimized	2,264	0.43	Blue and Orange
Wagon Wheel Extension	Shared Use	Blue	Directional by day (alternating)	24-36"	Traditional Bike Optimized	5,280	1	Pink

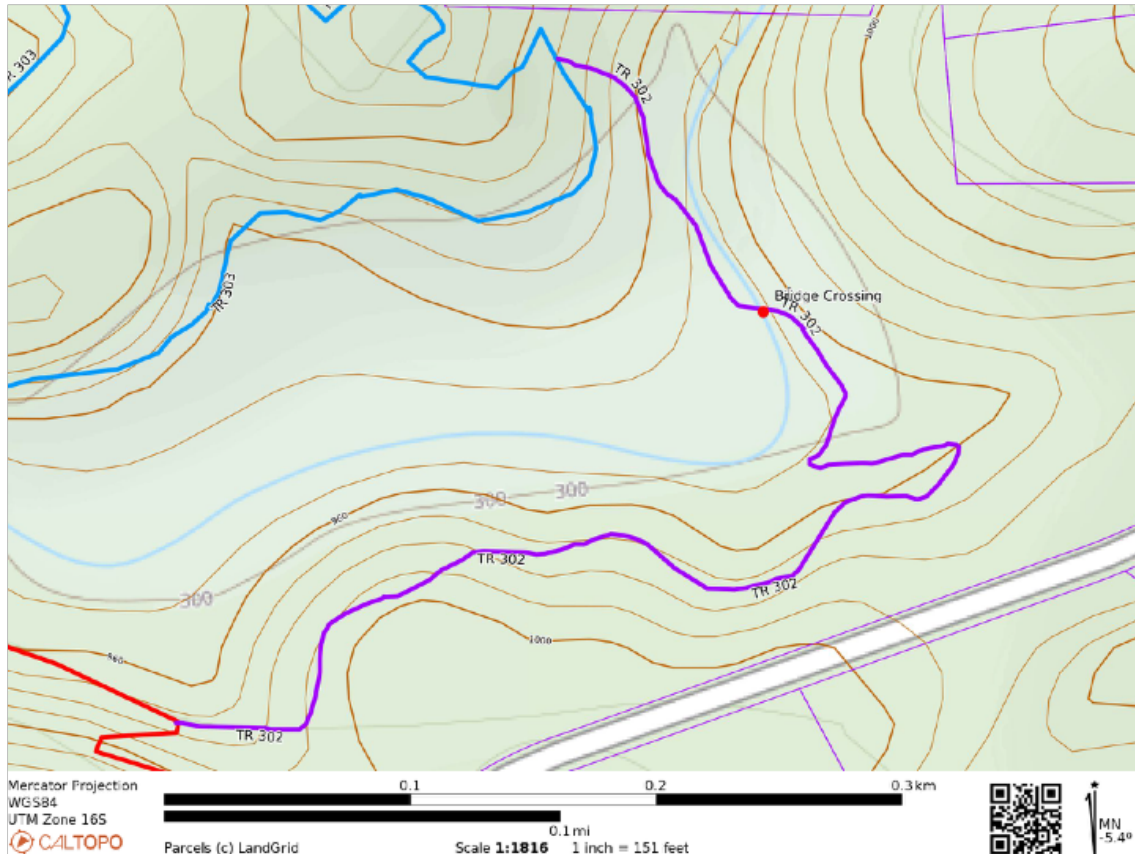
Total miles 3.74

Trail 301



Trail Design: Trail 301 is flagged in Orange and at an estimated 2.31 miles or 12,197 feet and designed to be an Intermediate Directional Shared Use loop trail built to Forest Service Class 3 Singletrack specifications with a tread on average of 24- 36 inches wide with dirt roller grade reversals added. Trail 301 was designed to maximize descents and climbs to highlight the steeper side slopes in this zone and to be directional by day. Natural rock features should be utilized as either alternates or incorporated in the tread with sustainable armoring techniques.

Trail 302



Trail Design: Trail 302 is flagged in Blue and Orange and at an estimated 0.43 miles or 2,264 feet and designed to be an Intermediate Bi Directional Shared Use Singletrack Trail with a tread on average of 36-48 inches wide with dirt roller grade reversals added. Trail 302 is designed to accommodate *two way* traffic and will require a 15- 20 foot bridge crossing. Trail 302 will eventually connect the Phase 1 and 2 trails to future phased trails, but will end in a dead-end turnaround for now.

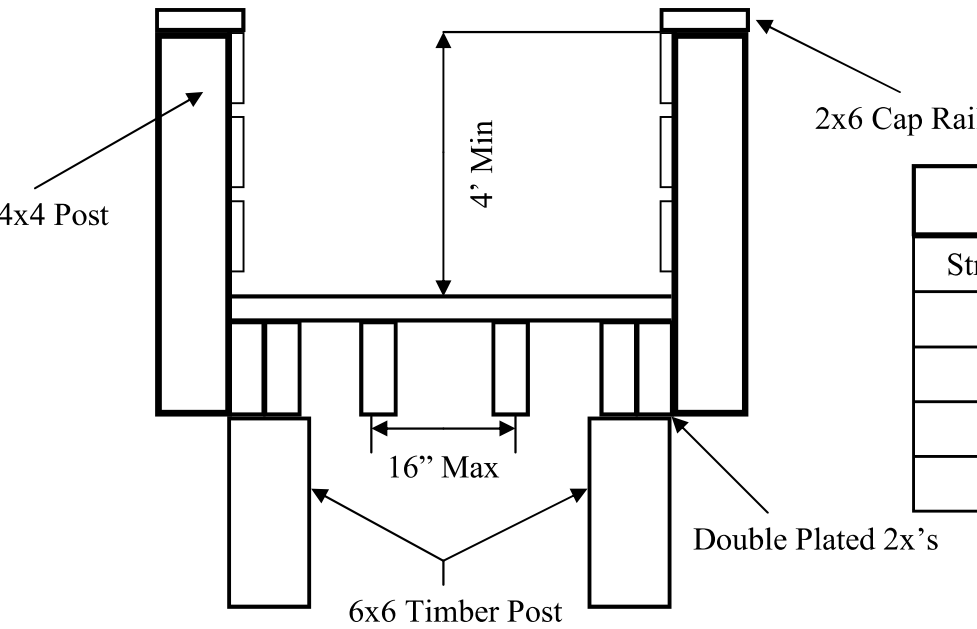
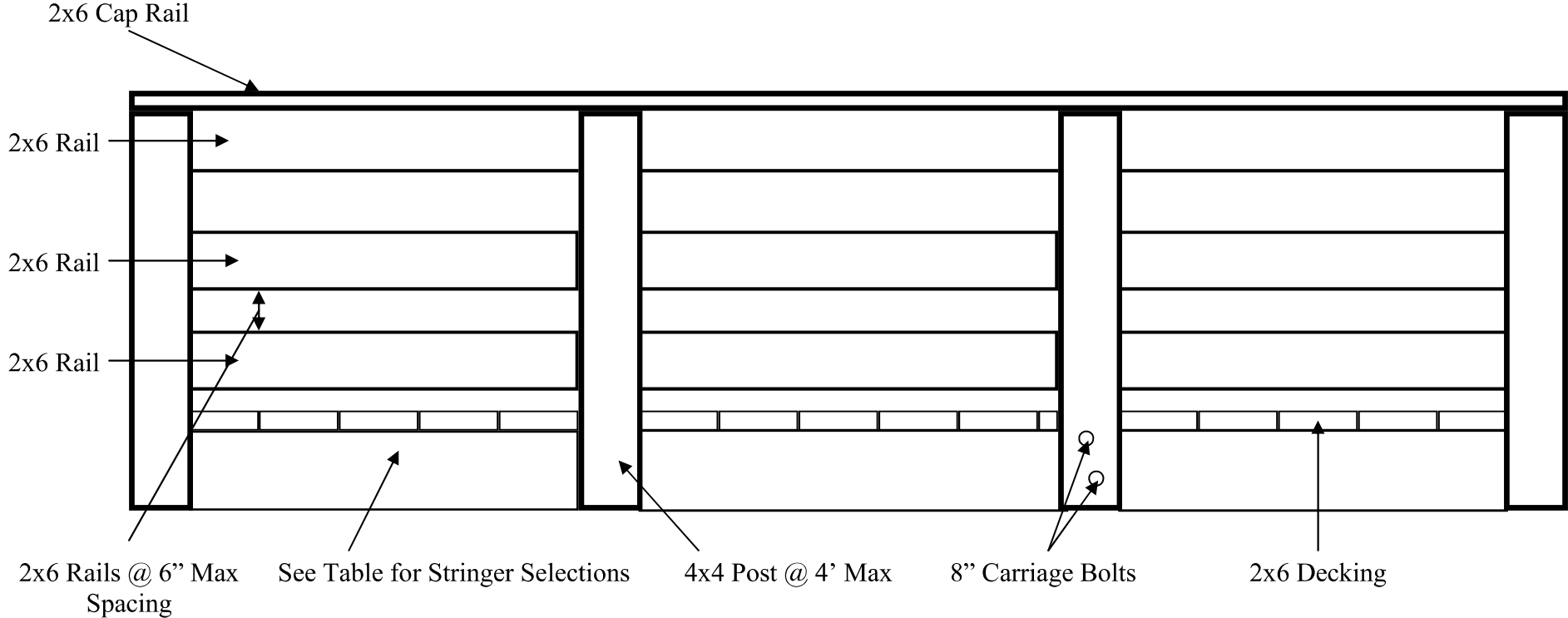
Trail 302 Continued

The Bridge Crossing on Trail 302 is estimated at 15 to 20 feet and should be built with a deck eight feet wide with rails appropriate for bikes to accommodate *two way* traffic. The builder and Bartow County should coordinate on the necessary details for such a bridge.

The Bridge should be constructed of pressure treated wood or fiberglass. Sills should be set on firm constructed footings. Sills should be rot resistant and rated for ground contact. Sills should be dimensional lumber or fiberglass. The final deck width should be eight (8) feet in width. Decking should be rough cut timber, to ensure adequate traction during wet weather. All bridge, boardwalk, and wooden feature construction should meet local deck or similar building code.

No stamped plans for the bridge currently exist. See example bridge design plans on the next two pages for suggested specifications.

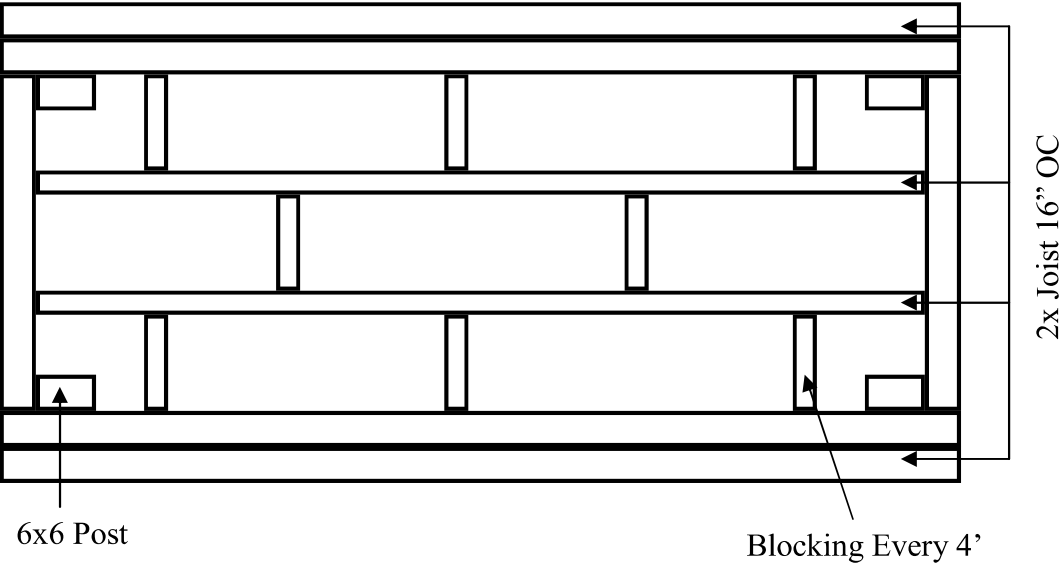
Bridge Plan with Handrails



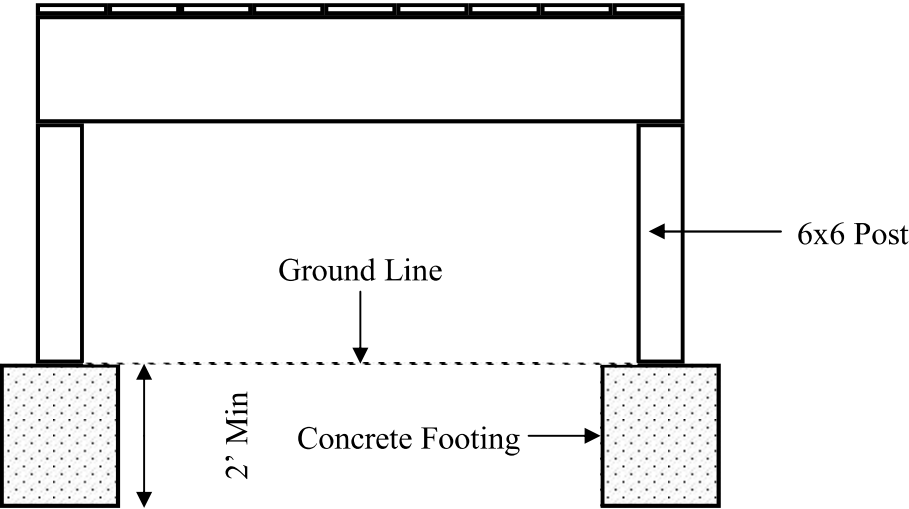
Stringer Selection Table		Bushwacker Trails, LLC Standard Lumber Pedestrian Bridges With Handrails 1' to 16'
Stringer Size	Maximum Span	
2x6	5'	
2x8	8'	
2x10	12'	
2x12	16'	

Bridge Plan with Handrails

Overhead View

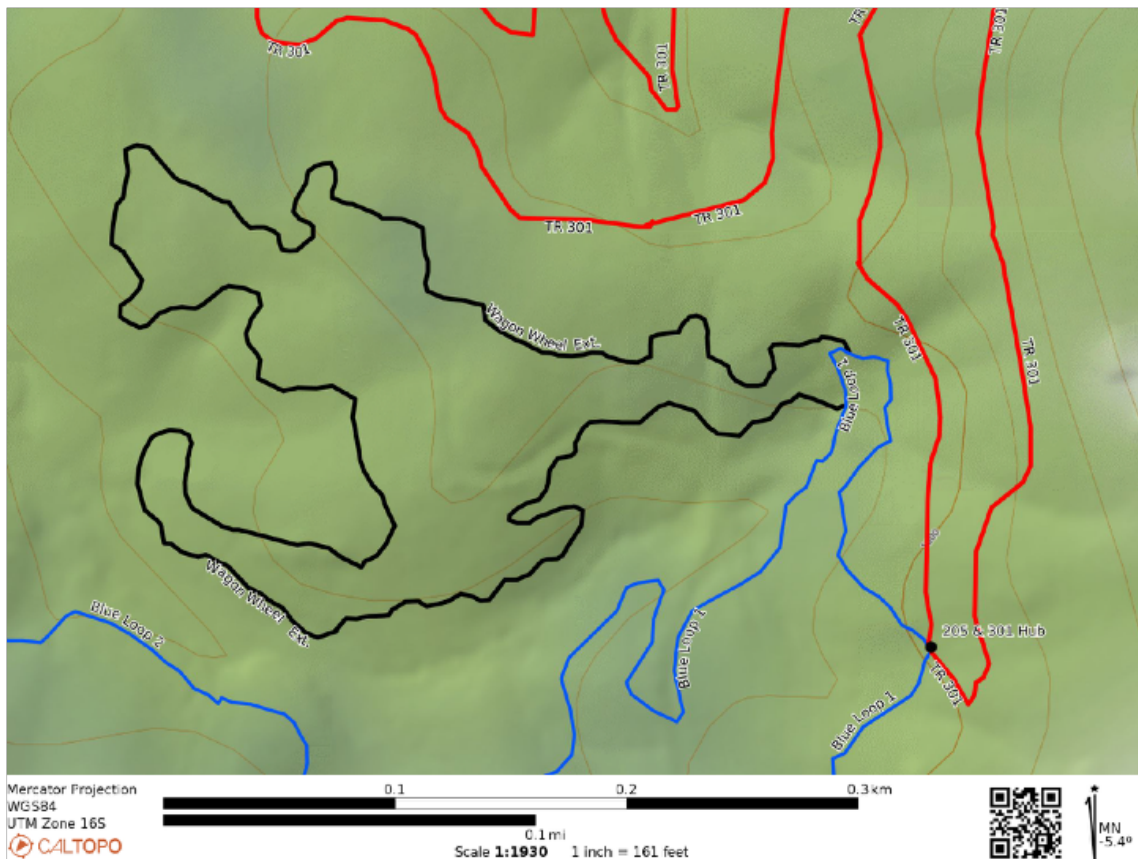


Footing Detail



Bushwacker Trails, LLC
Standard Lumber Pedestrian Bridges With Handrails 1' to 16'

Wagon Wheel Ext Trail



Trail Design: Wagon Wheel Extension is flagged Pink and at an estimated 1 mile or 5,280 feet and designed to be an Intermediate Directional Shared Use loop trail built to Forest Service Class 3 Singletrack specifications with a tread on average of 24- 36 inches wide with dirt roller grade reversals added. Natural rock features should be utilized as either alternates or incorporated in the tread with sustainable armoring techniques.

Contractor Affidavit under O.C.G.A. § 13-10-91(b)(1)

By executing this affidavit, the undersigned contractor verifies its compliance with O.C.G.A. § 13-10-91, stating affirmatively that the individual, firm, organization or corporation which is engaged in a physical performance of services agreement with Bartow County, Georgia has registered with, is authorized to use and uses the federal work authorization program commonly known as E-Verify, or any subsequent replacement program, in accordance with the applicable provisions and deadlines established in O.C.G.A. § 13-10-91. Furthermore, the undersigned contractor will continue to use the federal work authorization program throughout the contract period and the undersigned contractor will contract for the physical performance of services in satisfaction of such contract only with subcontractors who present an affidavit to the contractor with the information required by O.C.G.A. § 13-10-91(b). Contractor hereby attests that its federal work authorization user identification number and date of authorization are as follows:

E-Verify Authorization Number

Date of Authorization

Authorized Signature

Name of Contractor

Address

Address

Name of Project

Name of Public Employer

I hereby declare under penalty of perjury that the foregoing is true and correct.

Executed on _____, 20__ in _____ (city), _____ (state).

Signature of Authorized Officer or Agent

Printed Name and Title of Authorized Officer or Agent

SUBSCRIBED AND SWORN BEFORE ME
ON THIS THE ____ DAY OF _____, 20__.

NOTARY PUBLIC

My Commission Expires:

Disadvantaged Business Enterprise (DBE) Participation Certification

Bidder Certifies that:

- They do not intend to subcontract work.

Bidder certifies that:

- It has taken affirmative action to seek out and consider Disadvantaged Business Enterprises (DBEs) as potential subcontractors. DBE is a certification program under the U.S. Department of Transportation for minority-or women-owned-operated and controlled firms that are involved in highway and airport construction.
- It has taken affirmative action to seek out and consider DBEs as potential suppliers.

Further, the bidder shall:

- Specifically describe, on separate lines of this form, each component of work of the contract to be subcontracted to each DBE firm is being used as a supplier of materials and for other subcontract work (e.g. furnish sign materials as a supplier and traffic control as a subcontractor) the firm must be shown twice, once as a supplier and once performing subcontract work.
- List all contacts and follow-up contacts made with the potential DBE subcontractors and DBE material suppliers. (If necessary, use additional sheets).
- List the dollar amount quoted by each responding DBE subcontractor for the work described in accordance with the scope listed above and the dollar amount quoted by each DBE material supplier for the materials described in accordance with A. above.
- Only those DBEs possessing current certification by GADOT will be eligible to meet the requirements of the DBE program.
- Indicate responding DBEs that will be used, and those that will not be used.

Signature: _____

Date: _____

"General Decision Number: GA20240003 09/20/2024

State: Georgia

Construction Type: Highway

Counties: Barrow, Bartow, Butts, Carroll, Cherokee, Clayton, Cobb, Coweta, Dawson, Dekalb, Douglas, Fayette, Forsyth, Greene, Gwinnett, Haralson, Heard, Henry, Jasper, Lamar, Meriwether, Morgan, Newton, Paulding, Pickens, Pike, Rockdale, Spalding and Walton Counties in Georgia.

HIGHWAY CONSTRUCTION PROJECTS

Note: Contracts subject to the Davis-Bacon Act are generally required to pay at least the applicable minimum wage rate required under Executive Order 14026 or Executive Order 13658. Please note that these Executive Orders apply to covered contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but do not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(1).

<p>If the contract is entered into on or after January 30, 2022, or the contract is renewed or extended (e.g., an option is exercised) on or after January 30, 2022:</p>	<ul style="list-style-type: none"> ◆ Executive Order 14026 generally applies to the contract. ◆ The contractor must pay all covered workers at least \$17.20 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2024.
<p>If the contract was awarded on or between January 1, 2015 and January 29, 2022, and the contract is not renewed or extended on or after January 30, 2022:</p>	<ul style="list-style-type: none"> ◆ Executive Order 13658 generally applies to the contract. ◆ The contractor must pay all covered workers at least \$12.90 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours performing on that contract in 2024.

The applicable Executive Order minimum wage rate will be adjusted annually. If this contract is covered by one of the Executive Orders and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must still submit a conformance request.

Additional information on contractor requirements and worker protections under the Executive Orders is available at <http://www.dol.gov/whd/govcontracts>.

Modification Number Publication Date

0

09/20/2024

SUGA2022-003 07/12/2024

	Rates	Fringes
CARPENTER.....	\$ 32.68	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 30.37	0.00
LABORER: Asphalt, Includes Raker, Shoveler, Spreader and Distributor.....	\$ 17.23	2.23
LABORER: Common or General.....	\$ 18.70	0.00
LABORER: Pipelayer.....	\$ 22.42	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 23.53	3.63
OPERATOR: Bulldozer.....	\$ 24.68	2.59
OPERATOR: Crane.....	\$ 39.31	0.00
OPERATOR: Grader/Blade.....	\$ 26.18	4.28
OPERATOR: Loader.....	\$ 22.37	1.40
OPERATOR: Milling Machine.....	\$ 29.67	6.29
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 23.62	4.10
OPERATOR: Roller.....	\$ 18.89	3.99
OPERATOR: Screed.....	\$ 21.00	4.22
TRAFFIC CONTROL: Flagger.....	\$ 14.80 **	2.76
TRUCK DRIVER: Dump Truck.....	\$ 22.21	3.58
TRUCK DRIVER: Lowboy Truck.....	\$ 25.43	4.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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** Workers in this classification may be entitled to a higher minimum wage under Executive Order 14026 (\$17.20) or 13658 (\$12.90). Please see the Note at the top of the wage determination for more information. Please also note that the minimum wage requirements of Executive Order 14026 are not currently being enforced as to any contract or subcontract to which the states of Texas, Louisiana, or Mississippi, including their agencies, are a party.

Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours

they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classification and wage rates that have been found to be prevailing for the cited type(s) of construction in the area covered by the wage determination. The classifications are listed in alphabetical order of ""identifiers"" that indicate whether the particular rate is a union rate (current union negotiated rate for local), a survey rate (weighted average rate) or a union average rate (weighted union average rate).

Union Rate Identifiers

A four letter classification abbreviation identifier enclosed in dotted lines beginning with characters other than ""SU"" or ""UAVG"" denotes that the union classification and rate were prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2014. PLUM is an abbreviation identifier of the union which prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

Union prevailing wage rates are updated to reflect all rate changes in the collective bargaining agreement (CBA) governing this classification and rate.

Survey Rate Identifiers

Classifications listed under the ""SU"" identifier indicate that no one rate prevailed for this classification in the survey and the published rate is derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As this weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SULA2012-007 5/13/2014. SU indicates the rates are survey rates based on a weighted average calculation of rates and are not majority rates. LA indicates the State of Louisiana. 2012 is the year of survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the

wage determination. 5/13/2014 indicates the survey completion date for the classifications and rates under that identifier.

Survey wage rates are not updated and remain in effect until a new survey is conducted.

Union Average Rate Identifiers

Classification(s) listed under the UAVG identifier indicate that no single majority rate prevailed for those classifications; however, 100% of the data reported for the classifications was union data. EXAMPLE: UAVG-OH-0010 08/29/2014. UAVG indicates that the rate is a weighted union average rate. OH indicates the state. The next number, 0010 in the example, is an internal number used in producing the wage determination. 08/29/2014 indicates the survey completion date for the classifications and rates under that identifier.

A UAVG rate will be updated once a year, usually in January of each year, to reflect a weighted average of the current negotiated/CBA rate of the union locals from which the rate is based.

State Adopted Rate Identifiers

Classifications listed under the "SA" identifier indicate that the prevailing wage rate set by a state (or local) government was adopted under 29 C.F.R. 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. 01/03/2024 reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour National Office because National Office has responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.

Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION"